

City Council
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Planning Director
Kari Svanstrom
Assistant Planner
Dana Morrison
Senior Administrative Assistant
Rebecca Mansour

City of Sebastopol Design Review Board Staff Report

Meeting Date: Meeting of May 15, 2019
To: Design Review Board
From: Dana Morrison, Assistant Planner
Kari Svanstrom, Planning Director 
Subject: Design Review – Davis Townhomes
Recommendation: Approval with Conditions
Applicant/Owner: Kathy Austin / Dan Davis
File Number: 2018-82
Address: 6737 Sebastopol Avenue
CEQA Status: Mitigated Negative Declaration
General Plan: CC – Central Core
Zoning: CD – Downtown Core

Introduction:

In August of 2018 the City received an application for the Davis Townhomes project, proposing development of eighteen (18); 1,180 square foot, 2-bedroom, 2.5 (revised from 1.5) bathroom, 2-story townhomes. The proposed development would occur on a vacant lot located at the end of Morris Street, south of Sebastopol Avenue/Hwy 12. The Design Review of the project is the final step in the entitlement process.

Project History/Prior Review:

The Planning Commission conducted a Preliminary Review of the proposed project at their March 13, 2018 meeting prior to the formal submittal of the project. The Commission at that time was supportive of the project, including the development as a residential-only project, and reductions to the Environmental and Scenic Open Space (ESOS) requirements given the prior development of the site. On October 23, 2018 the Planning Commission approved the reduction of the required ESOS to require only visual analysis per SMC 17.46.090. The Commission also approved the reduction of the required 100' setback buffer to the sensitive habitat (Railroad Forest) to 50' per SMC 17.46.050.B(1).

On November 14, 2018 the Design Review Board (DRB) conducted an initial review of the Planned Community project and the DRB was unanimously in support of the project as solely residential, and with the lower FAR and reduced lot sizes. The DRB discussed the proposed height of the project and while some members thought that the height of the development could go as high as the base zoning district allows (40 feet), the applicant adjusted the submitted Policy Statement Table to show 40 feet as the proposed height and a maximum of two stories for the structures. The proposed elevations in the plans continue to show the height to be ~30

feet from finish floor to ridge and 2-stories. It should be noted that the final height may vary due to flood plain requirements and the site's topography.

The DRB also provided general comments regarding the overall design and landscaping of the project but limited this discussion as the item will be returning for formal Design Review at a later date. Minutes from the November 14, 2018 DRB meeting are located in the Attachments section of this Staff Report.

On April 9, 2019 the Planning Commission held a public hearing to review the proposed project's Use Permit (for solely residential and to rezone the property as a Planned Community), Tentative Map, ESOS Visual Analysis, and Initial Study/Mitigated Negative Declaration (MND). The Planning Commission felt that the Use Permit for solely residential and the Planned Community Rezone were appropriate given the nature of the development and its location, and recommended approval. The Commission reviewed and recommend approval of the Visual Resource Analysis and Tentative Map, they had no comments or issues and recommended approval to the City Council. Finally, the Planning Commission reviewed the Initial Study/MND and recommended that City Council adopt the MND.

Other items of note discussed:

- Planning Commission discussed the possibility of the Project's sidewalk that runs east-west at the front of the project (from the existing bike/pedestrian connector path to across the front of the property and ending at the shared fence with Park Village) to be a public sidewalk in the future, to connect all the way through to Tomodachi Park. One suggestion was to create an irrevocable offer of easement to be required for this extended connector path, if Park Village is ever closed and the site becomes an extension of Tomodachi Park.
- Additions to the Townhomes are not permitted; expansion of exterior features such as decks, porches or small garden shed/accessory structures (up to 80 sq. ft.) are allowed, subject to setback requirements.
- The Planning Commission felt the applicant should be encouraged to use the 50' setback from the Railroad Forest property for passive recreation (benches, minor trails, etc.). They modified this Condition of Approval (COA #6b). They felt a Use Permit reviewed by the Planning Director was the appropriate level of review.

At their May 7, 2019 meeting the City Council reviewed and approved the Project. The final Conditions include the requirement for an Irrevocable Offer of Dedication for the sidewalk as noted above, but without the requirement that Park Village become an extension of Tomodachi Park. The Council concurred with the restrictions on additions and the ability for the applicant to use the 50' ESOS buffer setback for passive recreation. Additionally, the Council added a Condition of Approval that, once the DRB has approved the exterior colors for the project, future changes would require approval by the Planning Director to ensure continued compatibility with the ESOS Visual Analysis as approved (See Final Conditions of Approval attached to this report).

Site and Surrounding Uses:

The site is located at the end of Morris Street. The site itself is relatively sheltered from view, this is due to the physical separation from Sebastopol Avenue, in combination with commercial, retail and residential development along Sebastopol Avenue and the mobile home park to the

east. The proposed development would be 2-stories, comparable to the single-family house along the northeastern boundary and the Sebastopol Inn located to the west. As such, the size would not be out of character with the surrounding uses. Given the site location, the proposed development, would only be visible from the public right-of-way at the intersection of Morris Street and Sebastopol Avenue.

Project Description:

The proposed town homes are clustered around a central open space, in three clusters. Lot sizes vary from approximately 1,250 to 1,800 square feet. The town homes are proposed to have 6-foot-deep front and rear porches (which will have a storage closet). In addition, there will be private rear yards of a minimum 20 feet deep by 20 feet wide with rear gates.

The project will include 18 carport spaces along with 18 assigned surface parking spaces and two (2) visitor parking spaces, for a total of 38 parking spaces. Four (4) of the spaces are proposed to be electric car charging stations.

The proposed development also includes gated vehicle access to Park Village (to be used in case of emergency), fire and emergency vehicle access, storm water retention areas, and new landscaping including the addition of 56 new trees, a trash and recycling enclosure, and one ganged mail box.

The applicant indicated that the exact structure heights from grade cannot be confirmed until a grading plan is completed. Depending on the finish grade the homes are proposed to be approximately 30½' high at the ridge from the finish floor. Due to the fact that the project is located in a flood plain all homes are required to have a finish floor at 80' elevation, which is 2' above the 78' Base Flood Elevation (BFE, often called the 100-year flood level).

Townhome Design

The style of the homes are proposed to be farmhouse/late craftsman to compliment much of the architecture found throughout Sebastopol. Roof pitches and front and rear porch roofs will be varied; and all porches will have decorative railings which will be painted to coordinate with the individual color trim of the home. Colors proposed are shown in the color rendering sheet located in the attachments. Colors proposed are greens, tans, greys, browns and reds.

Landscaping

The landscaping plan includes a variety of trees, shrubs, vines and ground covering plants. The majority of the plants selected are classified for low or moderate water use. 9% of the proposed plantings have a very low water use, 55% of the proposed plantings have a low water use, 18% have a moderate water use, and 18% have a high water use. As such, 82% of the plantings are classified as moderate or lower water use. Only two of the proposed planting species are classified for high water use; the 6 Heritage River Birch trees and the grass lawn, which are all proposed to be located in the central common area. The landscape also includes the placement of benches, boulders and local field stones placed throughout the common area. A minimum layer of 3" of mulch is also proposed to be applied on all exposed soil surfaces of plantings areas, barring in the "NoMow" grass areas. For details regarding irrigation see Sheet L-2.

The 50' setback from the Railroad Forest property is required to be restored and replanted. As per the Conditions of Approval the applicant is required to work with a native plan consultant to design a planting plan for this area. Passive recreation, such as walking paths will be permitted in this area, but no structures such as playgrounds shall be permitted within this setback. The

applicant has provided a concept of the pathways in this area for preliminary review by the DRB. The final approval of this area will require Use Permit approval by the Planning Director in accordance with the Conditions of Approval. The required review for conformance with the Conditions of Approval, including the City's adopted Laguna Wetlands Preserve Restoration and Management Plan and CEQA requirements for biologist review of the removal and replanting plan.

Paths for the complex are proposed to be made of new concrete (for the main and common area paths, pervious pavement (for the pathway along the eastern edge, abutting Park Village), and granite-crete for the proposed path in the 50' setback.

Carport/Trash/Mail Boxes:

There are two carports proposed as part of the development; each unit will have 1 space of covered parking and one uncovered. The carports are proposed to be made of steel columns and supports with a corrugated metal roof and simple trim. The color proposed is grey with a light underside. The photograph included as an example in the Attachments section is from an apartment project in Santa Rosa, and according to the applicant shows the style of carport they propose.

Also included in the project is a trash/recycle enclosure located near the northern property line, near the neighboring residential dwelling. The trash/recycling enclosure will be roofed (concept plan attached). A solid masonry wall is proposed to be located along the walls opposite the trash enclosure that will provide privacy to the residential property located to the north and east of Morris St. next to part of the proposed parking. The trash enclosure will be internally lit, as well. The trash hauler will have final approval as to the exact location of the enclosure, however the applicant has noted they have had preliminary conversations with Recology.

A ganged mailbox is proposed for the development, the mail box will be bronze in color (the exact location is subject to post office approval).

Lighting:

The lighting plan involves two types of light fixtures; standard (shielded) pole-top luminaires for the parking and bollards for the pathways. There are 3 pole-top luminaires and 19 bollards proposed. See cut-sheets and lighting site plan, in Attachments section, for details.

As noted in the Project Description, the porch lighting will consist of soffit lighting located underneath the porch roofs and illuminating downwards to ensure compliance with the City's dark sky goals. These lights will be controlled individually by the residents.

General Plan and Zoning Ordinance Consistency:

As noted earlier, the City Council adopted Resolutions approving the project and finding it consistent with the General Plan and Zoning Requirements at its May 7, 2019 meeting (see attached staff report). The proposed location for the Davis Townhomes project is on a parcel located at the edge of the Downtown Core District (CD), but is being rezoned to Planned Community. The City Council is in the process of adopting Ordinance 1124 (second reading/adoption scheduled for May 21, 2019), subject to the below zoning standards:

Allowed Uses Table

Allowed Uses	Downtown Core Requirements	Planned Community Proposed
Permanent residential uses that are allowed in the R7 District when not part of a mixed-use development	Conditional Use Permit	Conditional Use Permit
Nonresidential uses except for office uses	35,000 sf	Home office uses, subject to SMC 17.210
Common Area Accessory buildings		Conditional Use Permit
Residential Accessory buildings	-	Permitted, subject to 17.08.030, but limited to 80 sq. ft. in size

Development Standards Table

Development Standards	Downtown Core Requirements	Planned Community Proposed
Minimum lot area	6,000	1,260 – 1,801 sq. ft.
Maximum building height: Buildings and other facilities	40 ft., 3 stories/ 50 ft. 4 stories (1)	2 stories, and 40 ft. However per plans ~30 ft. (total may vary due to flood plain restrictions).
Front Yard	0 ft.	5 ft.
Interior Side Yard (except for structures sharing a common wall)	0 ft.	5 ft. ¹
Rear Yard, Main Building	0 ft.	5 ft.
Rear Yard, Main Building when abutting residential district	20 ft.	5 ft.
Maximum residential density	1 DU/1,743 SF lot area (CD) 1 DU/ 1,000 SF lot area (RM-H)	18 residential units on 75,794.4 SF lot = 1 DU/4,210.8 SF lot
Maximum Floor area ratio, not including residential except for purposes of calculating minimum FAR	Minimum (new buildings); 1.0 Maximum; 2.5	Net FAR 0.39 FAR ²
Minimum residential density	1 DU/ 3,600 SF lot area	Net Density: 18 residential units on 54,103 SF lot = 1 DU/3,005.72 SF lot
Minimum usable open space	50 sq. ft. per DU	280 sq. ft. per DU in rear yard + front and rear porches and common area
Parking - Residential	2 parking spaces per 2 or 3 bedroom units. 18 units x 2 = 36	2 spaces per unit (36 spaces), one of which is in a carport, and 2 visitor spaces. 38 spaces total

		(8 spaces to be provided for Electric Vehicles ³)
Bicycle Parking Spaces	20% required vehicle parking requirement. 36 x 0.20 = 8 bicycle parking spaces.	Hook in rear closet space for a bike for each unit, 18 bike spaces.

¹ Lot 18 which has an angled rear property line: angled lot line shall be considered a side yard.

² No additions to habitable space shall be permitted.

³ Vehicle parking to comply with Ordinance 1111 (Section 17.110.040 Electrical Vehicles).

Generally, the proposed project is consistent with the base zoning requirements of the Downtown Core District. The Planned Community overlay is being requested as the project does not meet the minimum required FAR and minimum lot sizes. However, the Planned Community overlay allows for consideration of the unique site characteristics (discussed in the Analysis section of this Staff Report); and, with the approval by the City Council on May 7, 2019, the application is consistent with the Zoning Ordinance.

The Use Permit to permit a solely residential development was approved by City Council May 7, 2019, consistent with the Zoning Ordinance (both Downtown Core and the Planned Community standards).

As noted above, an ESOS visual analysis is required due to the overlay, this is included in the Analysis section of this Report. This was reviewed by the Planning Commission at their April 9, 2019 meeting, and approved by City Council on May 7, 2019. As such, the proposed project is consistent with ESOS section of the Zoning Ordinance.

Inclusionary Housing:

The project is subject to the city's Inclusionary Housing requirements, which requires a certain percentage of units be reserved as Affordable housing in perpetuity for projects of more than five dwelling units.

For this project, the number of units to be provided are as follows (note, the fraction of a unit requirement can be fulfilled through either paying the corresponding Inclusionary Housing In-lieu fee, or by rounding up the number of units provided):

120% of AMI (Moderate Income) requires 15% of units, or 2.7 units (2 units plus .7 of a unit, or 3 units provided)

80% of AMI (Low Income) requires 10% of units, or 1.8 units

50% of AMI (Very Low Income) requires 5% of units (Very Low Income), or .9 units

These units will be reserved as affordable through a deed-restriction and affordable housing agreement, with the number of units/level of affordability and the specification of which of the units/lots will be Inclusionary will be determined at the time the Final Map of the subdivision is approved by Council.

The Inclusionary Housing further requires market-rate and Inclusionary to have the same external appearance. The applicant is not proposing any differences between units based on the affordability.

Public Comment:

Pursuant to CEQA Guidelines §15070, staff prepared a Draft Mitigated Negative Declaration (MND) which was filed with the State Clearinghouse and the Sonoma County Clerk/Recorder (Attachment 3). The Draft Initial Study/Mitigated Negative Declaration (CEQA) was circulated for public review March 7, 2019 through April 8, 2019.

The Planning Department has not received any comments from the public as of writing this report. The State Clearinghouse confirmed that no state agencies (including CalTrans) had any comments on the CEQA Initial Study/Mitigated Negative Declaration. At their May 7, 2019 meeting the City Council certified the Mitigated Negative Declaration, and the subsequent required CEQA Notice of Determination filing was submitted to the County Clerk's office on May 8, 2019.

City Departmental Comment:

The application was routed to the Building, Engineering, Fire, Police and Public Works Department, along with the City Manager's office for comments and conditions. Comments and Conditions from various departments have been integrated into the Conditions of Approval.

Required Findings:

Section 17.450.030.B of the Zoning Ordinance establishes the following Design Review Procedure: In considering an application for design review, the Design Review Board, or the Planning Director, as the case may be, shall determine whether:

1. The design of the proposal would be compatible with the neighborhood and with the general visual character of Sebastopol.
2. The design provides appropriate transitions and relationships to adjacent properties and the public right of way.
3. It would not impair the desirability of investment or occupation in the neighborhood.
4. The design is internally consistent and harmonious.
5. The design is in conformity with any guidelines and standards adopted pursuant to this Chapter.

Analysis:

Overall, the project site is unique as it is located on the edge of an urbanized area and is considered part of the City's Downtown but abuts an environmentally sensitive habitat. The project will result in the removal of 1 existing tree and result in the planting of an additional 50+ trees, as currently proposed. The proposed site is located within walking distance of existing commercial, and recreational uses, and will provide additional housing opportunities for locals. The DRB should review the proposed lighting, landscaping and colors to ensure that they are sensitive to the surrounding commercial and residential developments

The site itself is relatively sheltered from view, due to the physical separation from Sebastopol Avenue, in combination with commercial, retail and residential development along Sebastopol

Avenue and the mobile home park to the east. The proposed development would be 2-stories, comparable to the single-family house along the northeastern boundary and the Sebastopol Inn located to the west. As such, the size would not be out of character with the surrounding uses, and is consistent with the Planned Community development standards and the ESOS Visual Analysis Conditions of Approval.

The development will be subject to a number of Conditions of Approval regarding height, color selection and screening which will reduce potential impacts on any scenic vistas. The project lighting is in compliance with the City's General Plan policies related to light pollution. This includes the use of downcast ("dark sky compliant") lighting and conscientious placement of any proposed lighting. The lighting is proposed at 3000 Kelvin; a member of the Council expressed the desire for 2700 K if possible; the applicant noted they thought they could accommodate that change. The applicant also noted they were looking at having the parking area lights dimmable so they could be softened further later in the evening. If feasible, this would further comply with the City's dark-sky policies in the General Plan. Fences and walls are used to provide security, visual privacy, and/or define a space.

The DRB could find the project to be compatible with the neighborhood and greater Sebastopol in that the buildings will have materials, colors, and architectural features, which are present throughout the City in general. Furthermore, the project maximizes the development potential of the site by siting, while still being sensitive to the existing surrounding uses and the environmentally sensitive habitat located to the south. The project will contain trees and plantings, which provides a natural aesthetic element to the overall urban character of the proposed development. The project promotes the community desire for Sebastopol to remain a small town in that it increases the amount of residential housing in the community through urban infill development, renewing a previously-developed and then abandoned site.

The design provides appropriate transitions to adjacent properties in that the townhomes are not disproportionately shorter or taller than nearby buildings, and existing trees (barring one) will be retained and vegetation added to create a buffer. The design does not impair the desirability of investment or occupation in the neighborhood in that it revitalizes a vacant, unmaintained lot and creates 18 townhomes while leaving ample space to create an outdoor area with permeable surfaces, vegetation, trees and open space that softens the visual appearance of the existing site. Finally, the design is internally consistent and harmonious in that it utilizes the same patterns, siding, windows, doors, and materials throughout the development.

Recommendation:

Staff recommends that the Design Review Board receive a presentation from the applicant, conduct a public hearing and consider any public comments, deliberate, and approve the project based on the facts, findings, and analysis set forth in this staff report, and subject to the conditions of approval outlined.

Alternatively, the DRB may find that revisions are necessary and a continuance is appropriate. Staff recommends that the DRB provide direction for redesign to the applicant in the event of a continuance or rationale in the event of a denial.

Attachments:

Master Planning Application Form and Environmental Assessment

Application Materials, including: Policy Statement/Policy Statement Table, Project Description, Color Palette/Color rendering sample, Location, Aerial and Parcel Map, Cross Site Section, Lighting Plan and Cutout, Smart Vent Wet and Dry Flooring Info, Carport, Trash Enclosure and Mailbox Info, and Flood Plain Grading Summary and Initial Storm Water Management Plan

Initial Study / Mitigated Negative Declaration and attachments, including: Traffic Study and Cultural Resources Evaluation

Minutes from 10/23/2018 PC meeting

Minutes from 11/14/2018 DRB meeting

Minutes from 4/9/2019 PC meeting

City Council Staff report and Approved Resolutions (including Exhibit A, Tentative Map and Conditions of Approval, and proposed Ordinance)

Plan Submittal: Tentative Map, Grading Plan, Utility Plan, Floor Plan, Elevations, Landscape/Irrigation Plan

EXHIBIT A
RECOMMENDED FINDINGS OF APPROVAL

Design Review
6737 Sebastopol Avenue
APN 004-063-036, File 2018-82

1. The project was the subject of an Initial Study and Mitigated Negative Declaration (MND) prepared in compliance with the California Environmental Quality Act, which was certified by the City Council on May 7, 2019 via Resolution 6239-2019; the MND did not identify any significant environmental issues or impacts from the design or use of the residential projects, and the IS found the project will have a less-than-significant impact as it relates to community risk cause by construction activities, after mitigation measure are implemented.
2. The Planning Commission (Resolution 19-04) and the City Council adopted resolutions finding that the Project, as conditioned, is consistent with the Central Core Land Use designation of the General Plan and General Plan policies, in that:
 - a) The proposal is consistent with Policy LU 1-7 as the site is located at the edge of the Downtown Core District and is surrounded on three sides by existing residential and commercial developments.
 - b) The proposal is consistent with Policy LU 6-2 as it provides much needed housing located in close proximity to various community services and transit.
 - c) The proposal is consistent with Policy COS 12-9 as only one tree is slated for removal. Additionally, Condition of Approvals have been added in regards to the 50' Railroad Forest setback which require this area to be maintained in its natural state, and which requiring buffer plantings / mitigation planting in said 50' setback.
 - d) The proposal is consistent with Policy COS 12-12 as the development is clustered in the center of the project site, with the townhomes arranged in the three clusters with open space located in the center, and around the perimeter of the property. The southern edge of the site will have the largest open space, with a 50' setback from the Railroad Forest property. As conditioned, an open space easement shall be required on this section of the property which would prevent disturbance of this area with the exception of restoration and removal of invasive species.
 - e) The proposal is consistent with Policy CD 1-7 as it is an infill development which provides much needed housing located in close proximity to pedestrian and bicycle trails and has convenient access to local amenities and transit. The project includes the requirement to construct a bicycle/pedestrian connection to the existing connector trail located at the west side of the site.
 - f) The proposal is consistent with Policy CD 3-4 as the project was subject to a Cultural Resource Evaluation which found that the site did not contain any known or evident cultural, archeological or historic resources. In the event that any unanticipated artifacts or cultural features are discovered during grading or underground excavations, all work in the vicinity of the find shall be stopped until the discovery area can be evaluated by an archaeologist and appropriate actions are taken.

- g) The proposal is consistent with the City's Housing Element, in that the site is identified as a site in the Site Inventory as land suitable for residential development.
 - h) The proposal is consistent with the City's Housing Element, in that, as conditioned, the proposal would meet the City's Inclusionary Housing requirements by providing on-site Inclusionary Unit(s) and providing housing opportunities at a variety of income levels.
3. That the project is consistent with the Zoning Ordinance in that it complies with the PC: Planned Community District and the applicable development standards as approved and conditioned.
4. That the design of the Project, as conditioned, will be compatible with the neighborhood and the general visual character of Sebastopol as well as the Environmental and Scenic Open Space overlay Visual Analysis requirements in that:
- a) The application proposes only two stories (as opposed to the 4 stories permitted in the district) and is designed with colors that would blend into the natural landscaping.
 - b) Considerable open space and an extensive planting plan (including the addition of 50+ trees, along with other shrubs and vines) is included as part of the proposed development.
 - c) The site is predominantly blocked from the main view corridor of Bodega Ave/Highway 12 by a residence and a mobile home park, as well as commercial and retail facilities; and
 - d) The Project would not result in the introduction of features that would significantly detract from or contrast with the visual character of the surrounding community by conflicting with visual elements or quality of an existing area (i.e., through conflicting style, size, coverage, scale, building materials, etc.) given that the proposed project is of a similar size and scale to the surrounding development.
 - e) The Project would not result in the removal of or substantial adverse change to one or more features that contribute to the valued visual character or image of the Project area, including but not limited to designated landmarks, historic resources, trees, or rock outcroppings in that there are no designated landmarks, historic resources or rock outcroppings located on the property; the Project only proposed the removal of one (1) tree and includes a planting plan for 56 more trees (this is not including any plantings that will be required in the 50' Railroad Forest setback).
 - f) The Project does not substantially obstruct, interrupt, or detract from a valued focal and/or panoramic vista from a public road, trails within an adopted County or State trail system, scenic vista or highway, or recreational area.
 - g) The Project as designed and Conditioned would also not result in an inconsistency with any goals, standards, or policies related to visual resources as given in the General Plan.
 - h) While affecting the visual character of the Project area on a short-term basis, Project construction activities would not substantially alter or degrade the existing visual character or quality of the Project Site and surrounding area, for the following reasons:
 - i. views of construction would be limited in duration and locations;
 - ii. the Project Site appearance would be typical of construction sites in urban areas;

- iii. construction fencing would be placed along the periphery of the Project Site to screen much of the construction from view at street and bike path level.
-
- 5. That the project will not impair the desirability of investment or occupation in the neighborhood in that it will redevelop a vacant site that was formerly used for industrial purposes.
 - 6. That the project provides appropriate transitions and relationships to adjacent properties and the public right-of-way in that the building is appropriately setback from the property lines and adjacent sites and buildings; utilizes existing roadways and driveways where feasible, including maintaining emergency access to and from the Park Village site; provides for both pedestrian and bicycle access to the existing trail connection; and, provides privacy fencing, downcast lighting, and landscaping to provide screening to adjacent properties.

EXHIBIT B
RECOMMENDED CONDITIONS OF APPROVAL

Design Review
6737 Sebastopol Avenue
APN 004-063-036, File 2018-82

1. Approval is granted for the Davis Townhomes submittal described in the Design Review application and the approved plans. Plans and elevations shall be in substantial conformance with plans prepared by Katherine Austin, and date-stamped received on December 19, 2018, and amended April 25, 2019 and on file at the City of Sebastopol Planning Department, except as modified herein.
2. All conditions of approval for the Use Permit, ESOS Study, and Tentative Map approved by the City Council Resolution 6240-2019 remain applicable.
3. Except as otherwise noted in these conditions of approval, and the conditions of approval adopted by the City Council on May 7, 2019, the plans submitted for permitting shall be in substantial conformance to those approved by the City Council and Design Review Board. If any changes are made to the approved Design Review plans, the applicant is responsible for clearly identifying all such changes and reviewing them with the Planning Department prior to submitting a Building Permit application or a revision to an approved Building, Improvement, or Grading Permit. All changes to the Design Review Plans approved by the Design Review Board on the Building Permit submittal shall be clearly highlighted with a revision "Bubble" or "Cloud" on the plans and marked with a "Delta P". A list describing in detail all such changes shall be submitted in a transmittal memo and attached to these plans. Any changes that have not been clouded on the plans and noted in a transmittal memo and explicitly approved by Planning Staff are not approved, and may be subject to removal.
4. Approval of a Use Permit shall be required for the passive recreation features within the 50' ESOS buffer setback in accordance with City Council Resolution 6240-2019.
5. The applicant shall submit documentation to the Building Official, demonstrating compliance with the Water Efficient Landscape requirements per Chapter 15.36 of the Sebastopol Municipal Code.
6. Any tree removals shall be consistent with the Tree Protection Ordinance. The applicant is advised that this Design Review approval does not constitute authorization to remove any protected trees, which would be subject to a separate permit process.
7. The project site include protected trees intended to remain. Protective measures are required. All final tree protection measures shall be submitted reviewed and approved by the City Arborist prior to issuance of Improvement Plans.
8. Applicant is advised that under specified circumstances, the City of Sebastopol requires new commercial or residential buildings, and specific alterations, additions and remodels to the install a photovoltaic energy generation system. For specific requirements, see Municipal Code Chapter 15.72.

9. Rooftop equipment and backflow devices shall be screened. Any final screening shall be depicted in building permit plans and shall be to the satisfaction of the Planning Director.
10. This approval does not include any signs. Any new signs that will identify the use of this property are subject to the prior approval of the Design Review Board or City staff, as appropriate.
11. This approval is valid for two (2) years, except that the applicant may request a one (1) year extension of this approval from the Planning Director, pursuant to Section 17.250.050 of the Zoning Ordinance.
12. The Planning Director shall interpret applicable requirements in the event of any redundancy or conflict in conditions of approval.
13. The City of Sebastopol and its agents, officers and employees shall be defended, indemnified, and held harmless from any claim, action or proceedings against the City, or its agents, officers and employees to attach, set aside, void, or annul the approval of this application or the environmental determination which accompanies it, or which otherwise arises out of or in connection with the City's action on this application, including but not limited to, damages, costs, expenses, attorney's fees, or expert witness fees.



City of Sebastopol

Planning Department
7120 Bodega Avenue
Sebastopol, CA 95472
(707) 823-6167 (Phone) or (707) 823-1135 (Fax)
www.ci.sebastopol.ca.us

Major DR
+
Major UP

MASTER PLANNING APPLICATION FORM

PROJECT INFORMATION: * 6737 Sebastopol Avenue

ADDRESS:	NO ADDRESS ASSIGNED @ THIS TIME END of MON'S St. So. of Seb. Rd.
PARCEL #:	004-063-036
PARCEL AREA:	Approx. 1.74 AC

FOR CITY USE ONLY	
PLANNING FILE #:	2018 / 82
DATE FILED:	08-15-18
TOTAL FEES PAID: \$	3,390 + 5485
RECEIVED BY:	[Signature]
DATE APPLICATION DEEMED COMPLETE:	

Major + Subdivision Tentative

APPLICANT OR AGENT:

Name: DAN DAVIS
 Email Address: dodavis@comcast.net
 Mailing Address: 1051 Todd Rd.
 City/State/Zip: Santa Rosa 95407
 Phone: (707) 975-0794
 Fax: 707-585-6877
 Business License #: _____
 Signature: [Signature]
 Date: 8-15-18

OWNER OF PROPERTY IF OTHER THAN APPLICANT:

Name: _____
 Email Address: _____
 Mailing Address: _____
 City/State/Zip: _____
 Phone: _____
 Fax: _____
 Business License #: _____
 Signature: _____
 Date: _____

I certify that this application is being made with my consent.

OTHER PERSONS TO BE NOTIFIED: (Include Agents, Architects, Engineers, etc.).

Name: Katherine Austin
 Email Address: kaustin@pacbell.net
 Mailing Address: 179 SE Rice Way
 City/State/Zip: Bend OR 97702
 Phone: (707) 529-5565
 Fax: NONE

Name: _____
 Email Address: _____
 Mailing Address: _____
 City/State/Zip: _____
 Phone: _____
 Fax: _____

Bus. License AUS 0001

AUG 15 2018

[Signature]

PROJECT DESCRIPTION:

DESCRIBE IN DETAIL, the proposed project and permit request. (Attach additional pages, if needed):

Please See ATTACHED FOR FULL Description. 18 TOWN HOMES
(2) story, 2 BR 1 1/2 Ba. 1180 sq. ft. 39 parking spaces. New
Driveway to MOBILE HOME park. Full landscaping proposed.
FIRE TRUCK/EVA ACCESS INTO site.

This application includes the checklist for the type of application requested: Yes No

Please indicate the type(s) of application that is being requested (example: Use Permit, Design Review, Variance, Planned Community Rezone, etc.):

INITIAL STUDY FOR USE PERMIT, TENTATIVE MAP,
FINAL DESIGN REVIEW

Please describe existing uses (businesses, residences, etc.) and other structures on the property:

VACANT LOT

DEVELOPMENT DATA:

SQUARE FEET BUILDING EXISTING:		<input checked="" type="checkbox"/> N/A
SQUARE FEET BUILDING DEMOLISHED:		<input checked="" type="checkbox"/> N/A
SQUARE FEET BUILDING NEW:	21,258	<input type="checkbox"/> N/A
NET CHANGE IN BUILDING SQUARE FEET:	+ 21,258	<input type="checkbox"/> N/A
NUMBER OF DWELLING UNITS EXISTING:	<input type="checkbox"/> 0 Bedrooms <input type="checkbox"/> 2 Bedrooms <input type="checkbox"/> 4+ Bedrooms	<input type="checkbox"/> 1 Bedrooms <input type="checkbox"/> 3 Bedrooms <input checked="" type="checkbox"/> N/A
NUMBER OF DWELLING UNITS PROPOSED:	<input type="checkbox"/> 0 Bedrooms <input checked="" type="checkbox"/> 2 Bedrooms 18 <input type="checkbox"/> 4+ Bedrooms	<input type="checkbox"/> 1 Bedrooms <input type="checkbox"/> 3 Bedrooms <input type="checkbox"/> N/A
NET CHANGE IN DWELLING UNITS:	+ 18	<input type="checkbox"/> N/A
SETBACKS:	Existing: <input type="checkbox"/> Front Yard _____ <input type="checkbox"/> Side Yard _____ <input type="checkbox"/> Rear Yard _____ <input checked="" type="checkbox"/> N/A	Proposed: <i>Planned Community</i> <input type="checkbox"/> Front Yard _____ <input type="checkbox"/> Side Yard _____ <input type="checkbox"/> Rear Yard _____ <input checked="" type="checkbox"/> N/A

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EXISTING LOT DIMENSIONS: <i>Parcel is NOT rectangular</i>	Front: <u>173.70</u> + 104.73 Left: <u>255.79±</u>	Rear: <u>283.75</u> Right: <u>273.76+</u> <u>67.59</u>	<input type="checkbox"/> N/A
PROPOSED LOT DIMENSIONS:	Front: <u>SAME</u>	Rear: _____	<input type="checkbox"/> N/A
EXISTING LOT AREA:	<u>1.74 AC ±</u> Square Feet		<input type="checkbox"/> N/A
PROPOSED LOT AREA:	<u>1.74 AC ±</u> Square Feet		<input type="checkbox"/> N/A
BUILDING HEIGHT:	Existing: <u>NONE</u>	Proposed: <u>20'±</u>	<input type="checkbox"/> N/A
NUMBER OF STORIES:	Existing: <u>NONE</u>	Proposed: <u>two</u>	<input type="checkbox"/> N/A
PARKING SPACE (S):	Existing: <u>3</u>	Proposed: <u>39</u>	<input type="checkbox"/> N/A
ZONING	Existing: <u>CD</u> <u>ESOS</u>	Proposed: <u>PC</u>	<input type="checkbox"/> N/A

Will the project involve a new curb cut or driveway? Yes No

Are there existing easements on the property? Yes No

Will Trees be removed? Yes No

If yes, please describe (Example: Type, Size, Location on property, etc.)

(1) 9" Live OAK SW top of slope

Will Existing Landscaping be revised? Yes No

If yes, what is square footage of new or revised landscaping?

Please See Landscape PLAN.

Will Signs be Changed or Added? Yes No

Business: Hours of Operation? Open: _____ Close: _____

Is alcohol service proposed? Yes No

If yes, what type of State alcohol license is proposed? _____

If yes, have you applied to the State Alcoholic Beverage Control for a license? Yes No

If this is a restaurant, café or other food service, bar, or nightclub, please indicate total number of seats: _____

Is any live entertainment proposed? Yes No **REC 19 2018**

If yes, please describe: _____

INDEMNIFICATION AGREEMENT

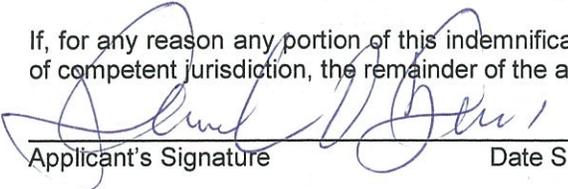
As part of this application, applicant agrees to defend, indemnify, release and hold harmless the City, its agents, officers, attorneys, employees, boards and commissions from any claim, action or proceeding brought against any of the foregoing individuals or entities, the purpose of which is to attack, set aside, void or annul the approval of this application or the adoption of the environmental document which accompanies it or otherwise arises out of or in connection with the City's action on this application. This indemnification shall include, but not be limited to, damages, costs, expenses, attorney fees or expert witness fees that may be asserted by any person or entity, including the applicant, arising out of or in connection with the City's action on this application, whether or not there is concurrent passive or active negligence on the part of the City.

If, for any reason any portion of this indemnification agreement is held to be void or unenforceable by a court of competent jurisdiction, the remainder of the agreement shall remain in full force and effect.

Applicant's Signature

Date Signed

Planning File Number

 8-15-18 2018-82

NOTE: The purpose of the indemnification agreement is to allow the City to be held harmless in terms of potential legal costs and liabilities in conjunction with permit processing and approval.

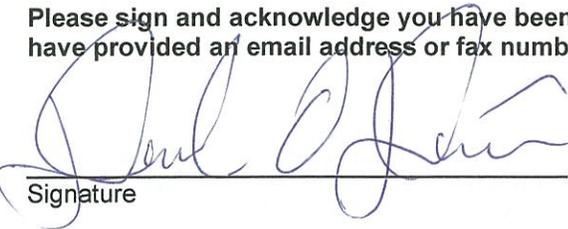
NOTICE OF MAILING:

Email addresses or facsimiles will be used for sending out staff reports and agendas to applicants, their representatives, property owners, and others to be notified.

Please sign and acknowledge you have been notified of the Notice of Mailing for applications and have provided an email address or fax number.

Signature

Printed Name

 Daniel O. Davis

NOTE: It is the responsibility of the applicant and their representative to be aware of and abide by City laws and policies. City staff, Boards, Commissions, and the City Council will review applications as required by law; however the applicant has responsibility for determining and following applicable regulations.



City of Sebastopol

ENVIRONMENTAL INFORMATION/ASSESSMENT FORM

(To be completed by applicant)

The submittal information shall be provided to the Planning Department.

Date Filed: 8/15/18

General Information:

- Name of developer or project sponsor: DAN DAVIS
Address of developer or project sponsor: Architect KATHERINE AUSTIN
- Address of project: Parcel 004-063-036 (no address assigned)
Assessor's Block and Lot Number: 004-063-036
- Name of person to be contacted concerning this project: KATHERINE AUSTIN
Address of person to be contacted concerning this project: 179 SE Rice Way Bend, OR 97702
Telephone Number of person to be contacted concerning this project: 707-529-5565
- Indicate number of the permit application for the project to which this form pertains:

- List and describe any other related permits and other public approvals required for this project, including those required by City, Regional, State and Federal Agencies:

Application for use permit for P.C. and Prelim. Design Review

- Existing Zoning District: CD/ESOS Existing General Plan Designation: CC (Central Core)
- Propose Use of Site (Project for which this form is filed): Planned Community of 18 Town Homes. Please see attached Project Des. for additional information

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PROJECT DESCRIPTION:

8. Site Size: 1.74 AC ±
9. Square Footage: 21,258 in townhouses
10. Number of floors of construction: two
11. Amount of off-street parking: 18 carports + 20 ADD. Surface (38)
12. Attach plans
13. Proposed scheduling summer 2019 construction if approved
14. Associated project NONE
15. Anticipated incremental development: NONE
16. If residential, include the number of units, schedule of unit sizes, range of sale prices or rents, and type of household size expected. ALL 2BR 1½ BA 1180 SF UNITS. UNKNOWN RENTS/sales price at this time. 1+3 people per unit
17. If commercial, indicate the type, whether neighborhood, city or regionally oriented, square footage of sales area, and loading facilities. N/A
18. If industrial, indicate type, estimated employment per shift, and loading facilities. N/A
19. If institutional, indicate the major function, estimated employment per shift, estimated occupancy, loading facilities, and community benefits to be derived from the project. N/A
20. If the project involves a variance, conditional use or rezoning application, state this and indicate clearly why the application is required. CURRENT ZONING DOES NOT INCLUDE TOWNHOUSE DEVELOPMENT STANDARDS. 100% RES. IN CD DIST. SEE PROJ. DESC.
- Are the following items applicable to the project or its effects? Discuss below all items checked yes (attach additional sheets as necessary).*

21.	Change in existing features of any bays, tidelands, beaches or hills, or substantial alternation of ground contour.	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
22.	Change in scenic views or vistas from existing residential areas or public lands or roads. <u>Potentially viewed from adjacent area</u>	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>
23.	Change in pattern, scale or character of general area of project.	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
24.	Significant amounts of solid waste or litter.	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
25.	Change in dust, ash, smoke, fumes or odors in vicinity.	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
26.	Change in ocean, bay, lake, stream or ground water quality or	Yes	No

	quantity, or alteration of existing drainage patterns.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
	<i>See SUSMP Documents</i>		
27.	Substantial change in existing noise or vibration levels in the vicinity.	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
28.	Site on filled land or on slope of 10 percent or more.	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
30.	Substantial change in demand for municipal services (police, fire, water, sewage, etc).	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
31.	Substantially increase fossil fuel consumption (electricity, oil, natural gas, etc).	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
32.	Relationship to a larger project or series of projects.	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>

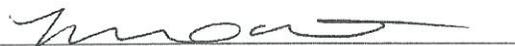
Environmental Setting:

33. Describe the project site as it exists before the project, including information on topography, soil stability, plants and animals, and any cultural, historical or scenic aspects. Describe any existing structures on the site, and the use of the structures. Attach photographs of the site. Snapshots or Polaroid photos will be accepted.
please see submittal package for detailed info + photos. No Bldg on site now.
34. Describe the surrounding properties, including information on plant and animals and any cultural historical, or scenic aspects. Indicate the type of land use (residential, commercial, etc), intensity of land use (one-family, apartment houses, shops, department stores, etc), and scale of development (height, frontage, set-back, rear yard, etc). Attach photographs of the site. Snapshots or Polaroid photos will be accepted.
please see Project Description + submittal package

<i>NO EXISTING BUILDING</i>	YES	NO
A. Does the Project involve any of the following?		
1. No change in the square footage to the existing structure?		N/A
2. An addition of more than 50% of square footage to the existing structure?		N/A
3. An addition of more than 2500 square feet to the existing structure?		N/A
4. An addition of more than 10,000 square feet to the existing structure?		N/A
5. Demolition of the existing structure?		✓
	YES	NO
B. Does the Project involve the replacement or reconstruction of existing structures or facilities at the site which:		
1. Will have substantially the same purpose and capacity as existing structures at the site?		N/A
2. Will result in an increase in square footage or capacity as compared to the existing structure?		N/A

	YES	NO
C. Does the Project involve new construction of:		
1. 35 or more dwelling units? <i>18 UNITS</i>		✓
2. More than 15,000 square feet of commercial, industrial, governmental, or institutional floor area?		✓
3. Stores, motels, offices, restaurants, and similar structures designed for an occupant load of more than 30 persons?		✓
	YES	NO
D. Does the Project involve division of property into more than four parcels or consolidation of more than four parcels?	✓	
<i>TOWN HOUSE SUBDIVISION</i>	YES	NO
E. Will the Project require issuance of a Variance, <u>Use Permit</u> , Zoning Ordinance Amendment, Zoning Map Amendment, or General Plan Amendment?	✓	
<i>FOR ESOS overlay and 100% residential</i>		
	YES	NO
F. Will the Project result in a change in use at the site (for example: from residential to commercial or from office to restaurant?)	✓	
<i>Empty to Residential</i>	YES	NO
G. Is this Project:		
1. Similar to the other projects for which you have received permits in the last two years in the City of Sebastopol?		✓
2. Similar to other projects, which you are planning to develop within two years in the City of Sebastopol?		✓
	YES	NO
H. Does the Project involve changes to an official City landmark?		✓
	YES	NO
I. Does the Project involve use of disposal of potentially hazardous materials, such as toxic substances, flammables, or explosives?		✓
	YES	NO
J. If the Project is located within 500 feet of a residential zone or noise-sensitive land uses, will the construction of the project involve the use of pile driving, night time track hauling, blasting, 24 hour pumping, or other equipment that creates high noise levels and or vibrations?		✓
	YES	NO
K. Does the Project involve the construction, substantial remodel, or 50% or more addition to the following types of uses?		
Mobile home, amphitheater, concert hall, auditorium, meeting hall, hospital, church, library, school classrooms, or day care?		✓

I certify that the information in this form is correct to the best of my knowledge.


Applicant Signature

8.13.2018
Date

Certification:

I hereby certify that the statements furnished above and in the attached exhibits present the data and information required for this initial evaluation to the best of my ability, and that the facts, statements, and information represented are true and correct to the best of my knowledge and belief.

Date: 8.13.2018

Signature: 

Printed Name: Katherine Austin

For: DAN DAVIS

December 19, 2018

**Planned Community ESOS Review
Use Permit and Planned Community**

Honorable Chair, Zachary Douch
Planning Commissioners



**Design Review Application
Tree Board Review**

Chair Ted Luthin
Design Review Board Members

Project Description

Dan Davis Town Homes APN004-063-036
6737 Sebastopol Avenue, Sebastopol

This Project Description is provided for the review of both the Planning Commission and The Design Review Board. Information for both bodies are combined in this one description for simplicity and coordination.

The location of the subject property is at the south terminus of Morris Street behind and to the south of commercial properties that face onto Sebastopol Avenue and an existing parking lot that contains approximately 32 parking spaces. A bike path and the Sebastopol Inn are located to the west. The railroad forest is located to the south. The Village Park and Tomadachi Park are located to the east.

The project proposed consists of the following:

- Eighteen (18) 1180 SF 2 bedroom 1 & ½ bathroom two story town homes with 6' deep front and rear porches.
- The rear porches provide a storage closet which is proposed to include secure bike storage with hooks for that purpose. Access from rear yard by a gate leads to full site and bike path.
- Private rear yards of average 20' deep x 20' wide with some at an angle to the property line which create varied yard areas. Lot 18 has both a larger side yard and angled rear yard to remain out of the 50' setback.
- 18 carport spaces with 20 additional surface parking spaces for a total of 38, (a photograph of proposed carport style is attached. The design is intended to minimize the visual impact to and from the lots adjacent to them.)
- 4 spaces are proposed to be electric car charging stations
- One trash/recycle enclosure roofed and internally lit (concept plan attached) Note: the trash hauler will have the final say as to the exact location of the enclosure.
- A solid masonry wall is proposed to be located along the walls opposite the trash enclosure that will provide privacy to the residential property located to the north and east of Morris St. next to part of our proposed parking. We propose to work with the owner of that property to assure a design that is mutually acceptable.
- One ganged mail box location (bronze color ganged box attached) Note: The Post office will have the final say on the location of the ganged mail boxes.

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- Newly provided driveway access to Village Park to the east, the current gate will remain but emergency access will be provided.
- Fire truck and emergency vehicle access into the site on a combination of concrete and grasspave2 material that supports the weight of such vehicles pre-reviewed by the Fire Chief for compliance.
- Storm water retention areas per best management practices as shown on the Civil Plans and detailed in the SUSMP documentation. Note: retention areas have been revised to be out of the 50' setback area.
- Fully landscaped area, please see the Landscape Plan for details.
 - Full Plant Legend is included with General Landscape Notes
 - Four large Oaks within the buildable area are proposed to remain
 - One small Oak is proposed to be removed
 - At least 56 trees are proposed to be planted
 - All existing trees within the 50' setback from the southerly PL are proposed to remain and be undisturbed
 - The Civil Engineering Plans take precedence with regard to removing all bioswales from the 50' setback and the orientation of the rear stairs in the rear yards.

Changes from Preliminary Plans

- All bioswales have been removed from the 50' setback area
- Homes on lots 17 & 18 have been moved easterly 5' to allow a rear yard and new side yard for lot 18 that is entirely outside of the 50' setback area. Both were moved to stagger the fence line along the bike path for a more attractive appearance and to keep the two homes pared as shown before.

Flood Plain Considerations

Kevin Doble, of Doble Thomas/LACO our Civil Engineer has provided documentation on the 100 year base floor elevation and our grading to address those issues. We meet all requirements of the City of Sebastopol with regard to building in the flood plain. As to the architecture, all finish floors are at the 80' elevation which is two feet above the 100' year floor level of 78'. This is consistent with all the proposed units and is reflected in the building elevations provided. The foundations will be provided with flood vents. An attachment showing how those vents work is provided. Essentially in case of a flood the vents open to allow free flow of water through them to prevent pressure build up and destruction of foundations. In addition the underfloor will have what is commonly referred to as rat-proofing. This is a combination of a waterproof membrane covered with approximately 1/2" concrete slurry to keep it in place. This allows us to dramatically reduce the amount of ventilation required and prevents saturation of the soil in case of flood.

Style, Material and Color Information

Style of Homes

Each town home is proposed to be distinct from its neighbor by using the following:

- The style of the homes are traditional or vernacular that compliments much of the architecture already existing in Sebastopol. Homes will be distinct from each other with six (6) styles and colors that repeat with none identical next to each other.

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- Roof pitches are either 6:12 or 8:12 with end units in some locations being a simple gable, hip or a Dutch Gable. Some roofs also have a dormer detail.
- Front and rear porch roofs have variety with some simple shed roofs, small gables and large gables.
- All porches have decorative railings which will be painted to coordinate with the individual trim color of the home.
- Note: Home heights from grade vary due to the gradual slope of the site and roof pitches. The intent is to provide variety and style while remaining as low as possible given the constraints of the flood plain requirements.
 - Flood plain requires that all homes have a finish floor at 80' elevation to be 2' above the 100 year flood level of 78'. The high point of the site is at 78' and gently slopes down to 75' in the building area.
 - This will cause all homes to have steps up into them and the rendered elevations shown are approximate. Some balancing of the site will allow a small amount of the spoils to fill under the homes and stairs.

Materials & Colors

- A mix of cement board horizontal lap siding & board and bat siding are used
- Wide rough sawn wood belly bands are at mid plate height and can delineate two tone color palettes on some homes
- 2x4 and 2x6 rough sawn trim are used around each window and door with a water table detail on all windows.
- Windows are proposed to be colored dual pane vinyl in either almond or bronze
- Paint colors proposed are a palette of colors compatible with the wooded setting. Warm tones of green, brown, grey, and tan are proposed for the body colors. Some homes will be two tone using the belly band to divide the colors. Trim colors are proposed to be off white or various shades of light tan. Color chips will be presented at the hearing.
- Roof colors will vary with a minimum of three colors: weathered wood, slate and dark grey. Note LP Tech Shield or similar is used under the roofing to reflect heat and keep the roofs cool.

Lighting

- Porch lighting will be in can lights in soffits to provide dark sky compliance and will be controlled individually by the residents.
- Individual internally illuminated address signs will be required by the Fire Dept. for each home.
- A lighting plan is provided using a combination of light bollards and standards as appropriate for illuminating parking and paths and will be energy efficient and dark sky compliant. Final height of the standards will be determined by the lighting contractor but should be in the range of 8' to 10' in height. The intention is to provide lighting for safety while minimizing any light pollution. The standards have baffles to prevent light from falling on adjacent properties. We propose to use photocells and or timers to dim the lights as appropriate in the late evening hours.

Inclusionary Housing

Once the project is approved the Owner will work with City Staff to determine the most appropriate path to meeting the inclusionary requirement. At this time it has not yet been

DEC 19 2018

determined at what level of affordability vs the number of units provided makes the most financial sense. The Owner will coordinate any rental agreements with the City or it's assigns to meet this requirement.

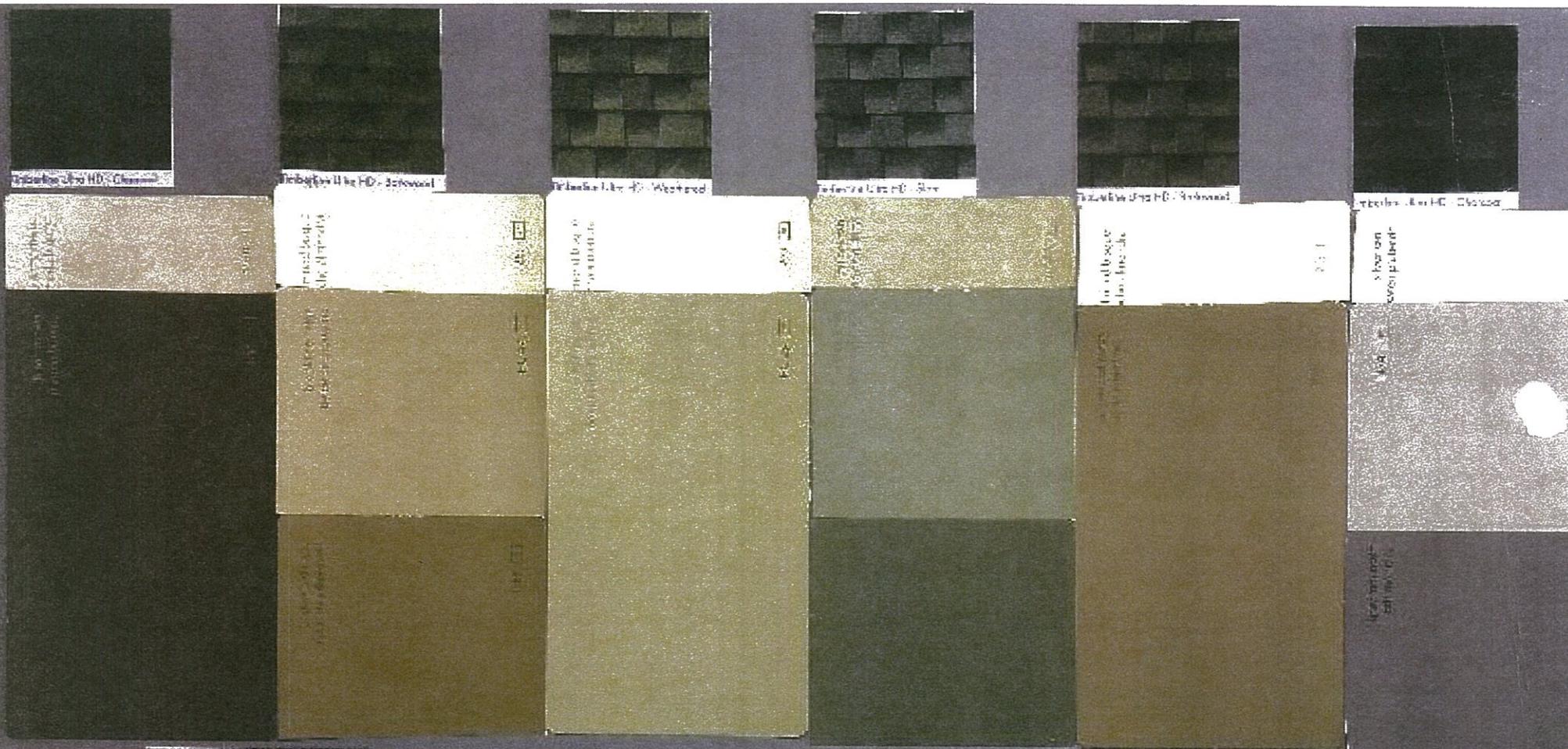
This concludes our Project Description

Katherine Austin, AIA, Project Architect

179 SE Rice Way, Bend OR 97702 and 524 So. Main Street, Sebastopol, CA 95472

707-529-5565 kaaustin@pacbell.net

DEC 19 2018





Lot 15

Lot 14

Lot 13

Lot 12

Lot 11

Lot 10

Davis Town Homes Color Rendering of Typical Color Combinations

Katherine Austin, AIA, Architect
1/1/2019

Davis Town Homes Site Location Map

AP 004-063-036



AUG 15 2018

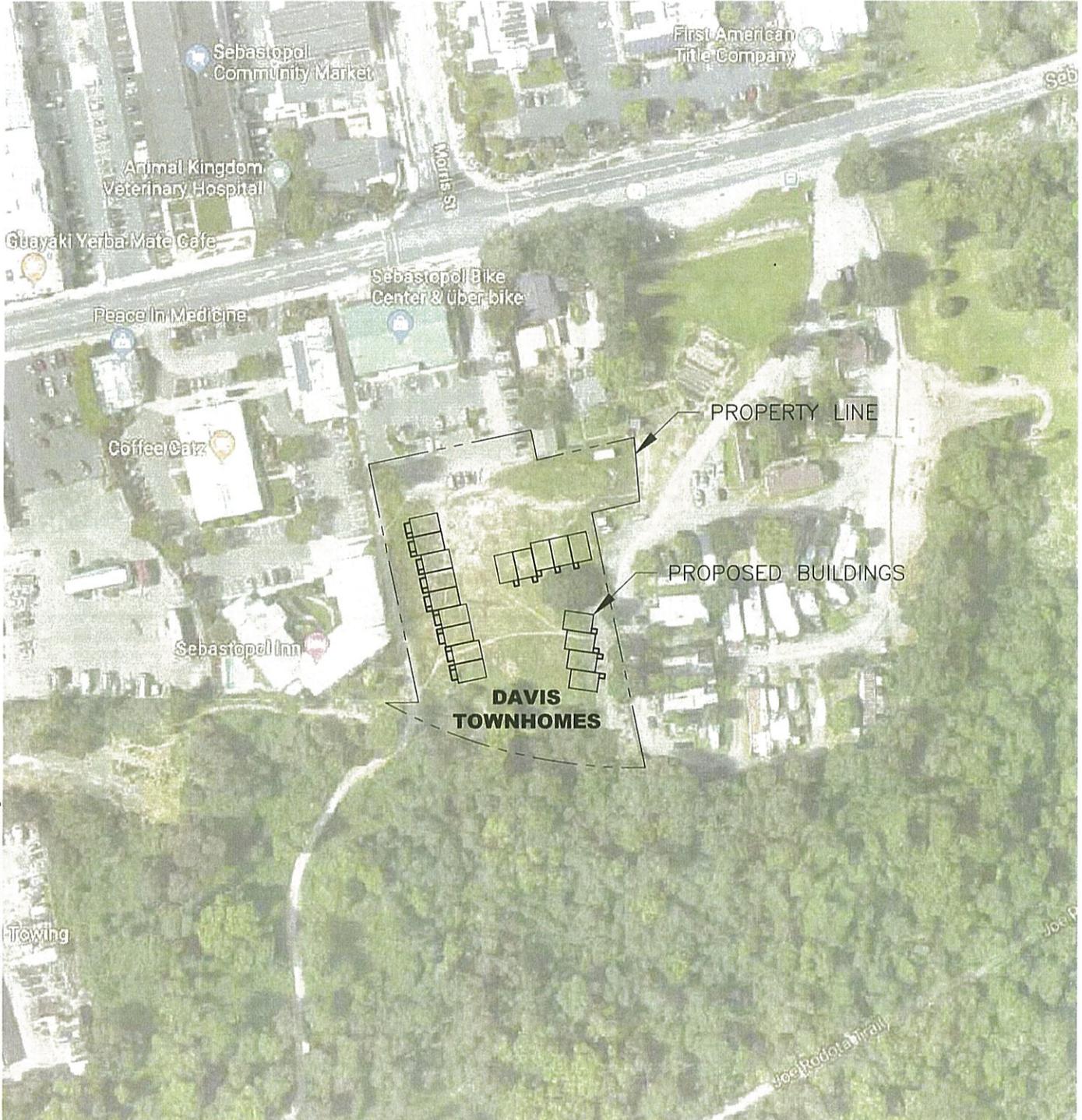
LACO

EUREKA • UKIAH • SANTA ROSA

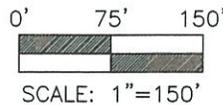
1-800-515-5054 www.lacoassociates.com

PROJECT	DAVIS TOWNHOMES	BY	GG	FIGURE	1
CLIENT	DANIEL DAVIS	DATE	08-13-18		
LOCATION	SEBASTOPOL, CALIFORNIA	CHECK	KD	JOB NO.	9036.00
	AERIAL SITE MAP	SCALE	1"=150'		

REUSE OF DOCUMENTS: This document and the ideas and design incorporated herein, as an instrument of professional service, is the property of LACO Associates and shall not be reused in whole or part for any other project without LACO Associates express written authorization.



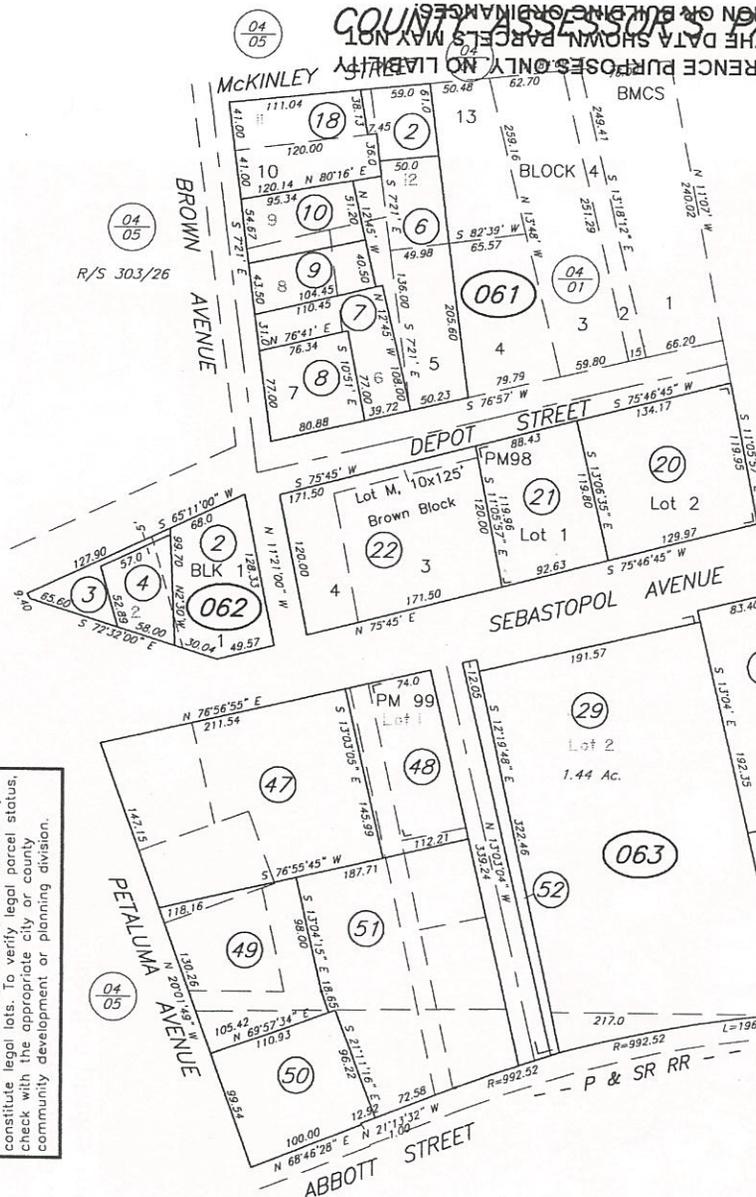
NOTE: ALL LOCATIONS ARE APPROXIMATE.



AUG 15 2018

Aug 14, 2018 - 2:33pm
T:\Codfiles\9000\9054.00 DAVIS TOWN HOMES\DWG\ 2017-061 AERIAL.dwg

THIS MAP SHOULD BE USED FOR REFERENCE PURPOSES ONLY. NO LIABILITY IS ASSUMED FOR THE ACCURACY OF THE DATA SHOWN. PARCELS MAY NOT COMPLY WITH LOCAL SUBDIVISION OR BUILDING ORDINANCES.



- Ptn of BIRDI MILLER CNOPIUS
REC. 08-28-1918 IN BK. 35 , MAPS, PGS. 21-23
- Parcel Map No. 15
REC. 10-01-1971 IN BK. 161 , MAPS, PGS. 43
- Parcel Map No. 58
REC. 11-29-1977 IN BK. 262 , MAPS, PGS. 11
- Parcel Map No. 98
REC. 12-13-1983 IN BK. 353 , MAPS, PGS. 10
- Parcel Map No. 99
REC. 05-22-1984 IN BK. 358 , MAPS, PGS. 11

SCALE: 1"=100'



NOTE: This map was prepared for Assessment purposes only and does not indicate either parcel legality or a valid building site. No liability is assumed for the accuracy of the data delineated. The map is based on the information supplied by the Assessor's office, maps, recorded deeds, prior assessment maps, etc.

NOTE: Assessor's parcels do not necessarily constitute legal lots. To verify legal parcel status, check with the appropriate city or county community development or planning division.

- REVISED
- 10-27-04=38(063)-LSL
 - 12-08-06=R/S-MH
 - 07-20-15=39(063)-KB
 - 08-03-16=45(063)-KB
 - 01-12-17=46(063)-KB
 - 01-26-17=52(063)-KB

Assessor's Map Bk.004, Pg.06
Sonoma County, Calif. (ACAD)

KEY 7-17-15 KB

Important: This plat is not a survey. It is furnished as a convenience to locate the land in relation to adjoining streets and other lands and not to guarantee any dimensions, distances, bearings or acreage.

Davis Town Homes Site Photos

Overall Aerial Photo of site, north is up



View from end of Morris St. & parking lot south into site

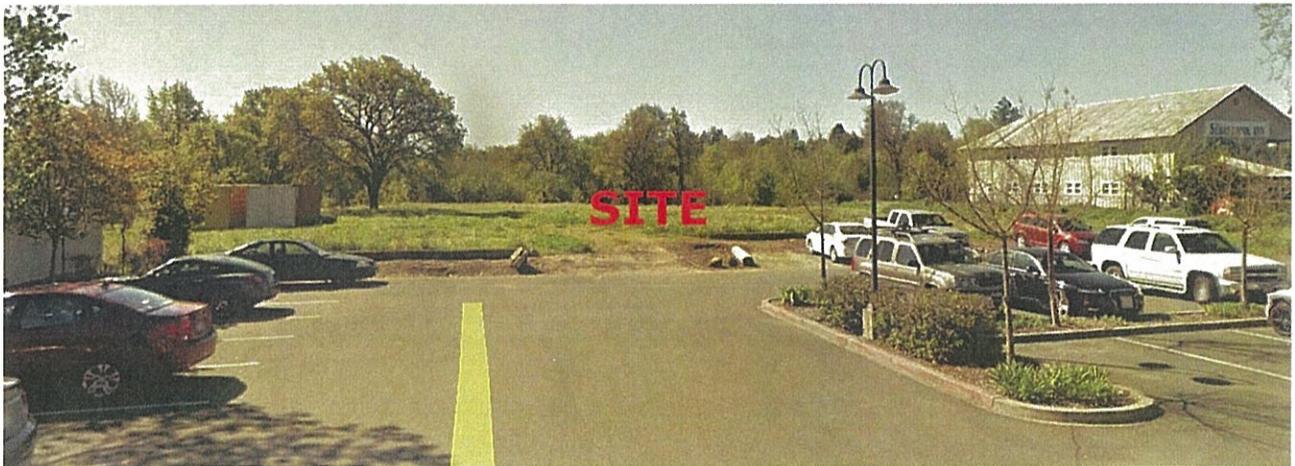
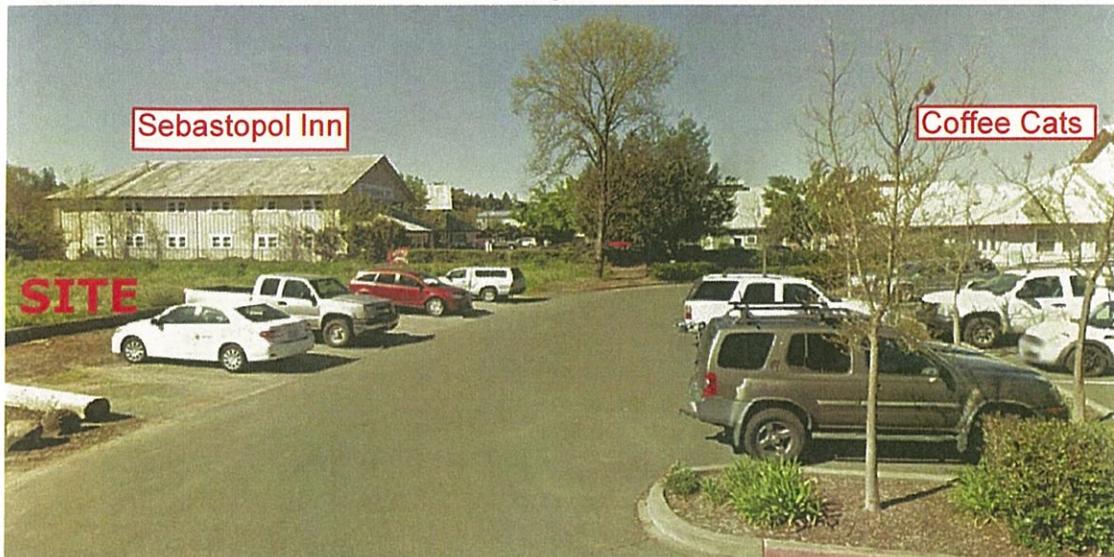


PHOTO
AUG 15 2018
BY: *[Signature]*

Closer view into site from existing parking lot/end of Morris St.



View from Morris St. driveway west towards Sebastopol Inn



View from Sebastopol Inn Parking lot towards site



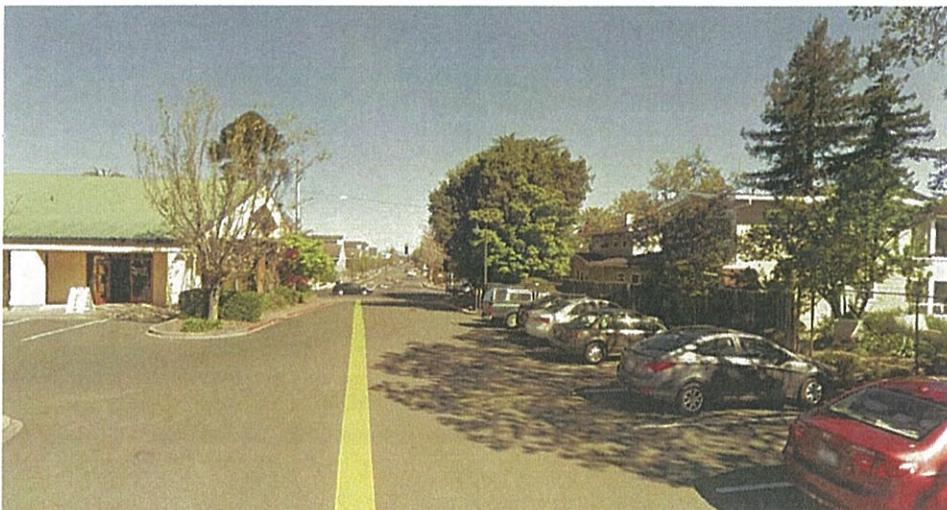
View from end of Morris back to existing commercial and parking



View east to adjacent parcel with second unit next to parking lot



View north from end of Morris St. parking lot towards intersection



Additional Aerial Photos from different angles



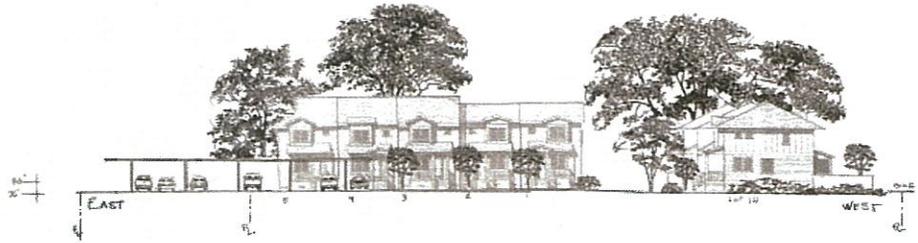
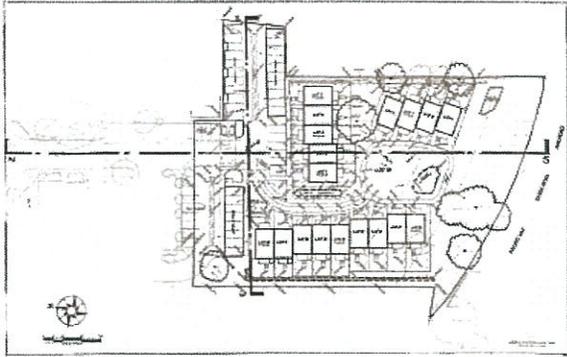
View from the Barlow looking south



View from Village Park looking west



View from Coffee Cats



OCT 01 2018

REVISIONS

Katherine Austin, AIA
 179 SE Race Way 324 So. Main Street
 Bend, OR 97702 Sebastopol, CA 95472

DAVIS TOWN HOMES
 Morris St. Sebastopol, CA

Cross Site Sections
 North South & East West

Date 9-30-2018
 Scale 1" = 20'
 Drawn KA
 Job
 Sheet
A2.2
 OF 2 Sheets

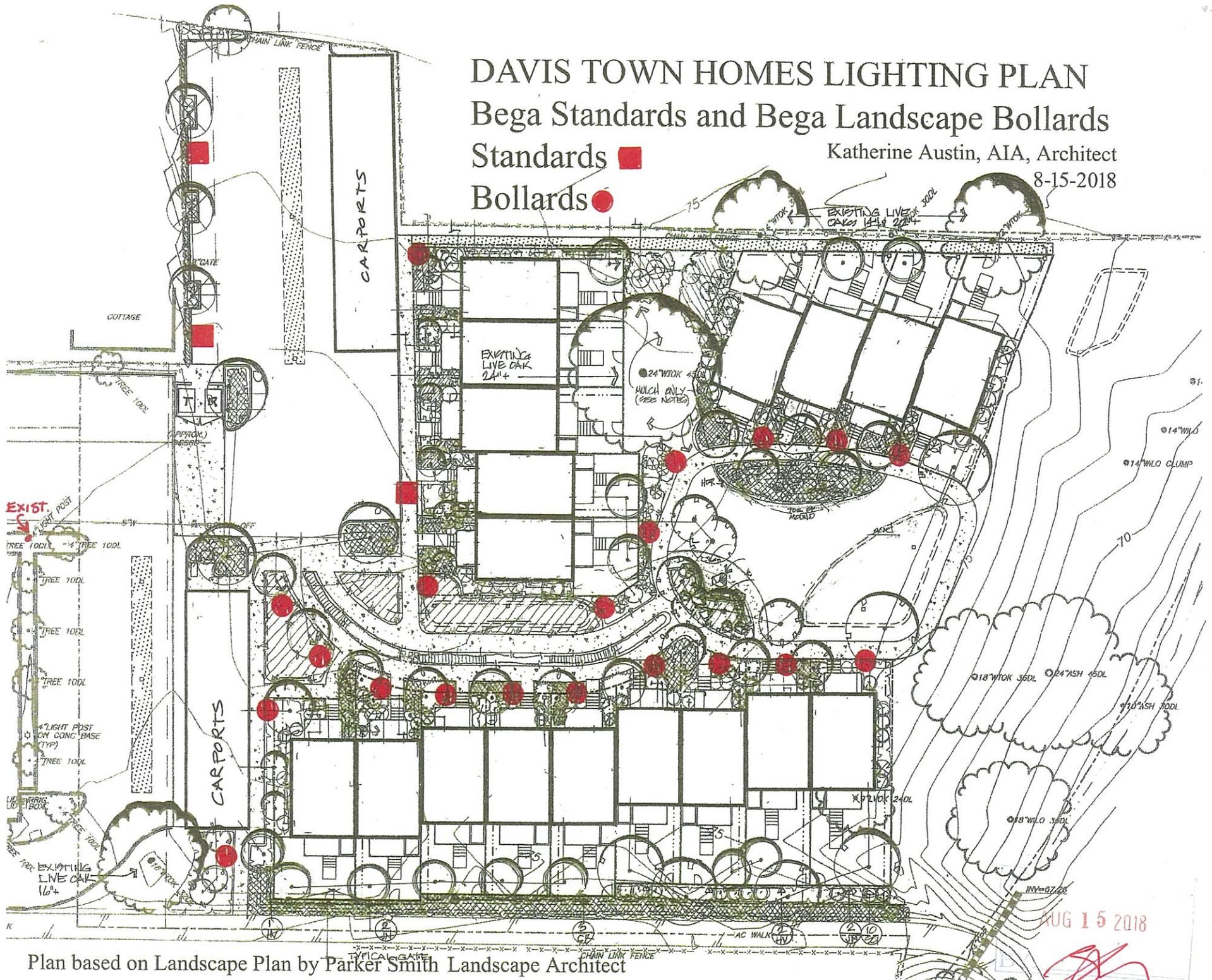
DAVIS TOWN HOMES LIGHTING PLAN

Bega Standards and Bega Landscape Bollards

Standards ■
Bollards ●

Katherine Austin, AIA, Architect

8-15-2018



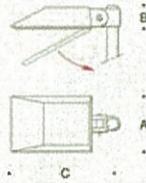
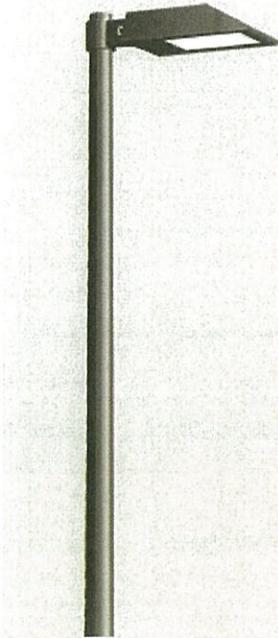
Plan based on Landscape Plan by Parker Smith Landscape Architect

AUG 15 2018



Proposed lighting for Davis Town homes

Light standard for parking lot



Pole top with shielded light source

Designed for the illumination of walkways, open pedestrian scale landscape areas, parking areas, and roadways featuring full cut-off light distribution.

Pole top luminaires with asymmetrical flat beam light distribution. Hinged door assembly with clear tempered glass diffuser.

Integral 120-277V electronic driver, 0-10V dimmable.

U.L. listed, suitable for wet locations.

Protection class: IP66

Finish: Standard BEGA colors.

Single pole-top luminaire

Click product # for details	Lamp	β	Temp°C	A	B	C
77 928	15.8W LED			11 1/2	4 1/4	23 7/8
77 939	23.6W LED			11 1/2	4 1/4	23 7/8
77 954	51.6W LED			13	5 5/8	26 5/8

Twin pole-top luminaire

Click product # for details	Lamp	β	Temp°C	A	B	C
84 123	(2) 15.8W LED			11 1/2	4 1/4	47 3/4
84 124	(2) 23.6W LED			11 1/2	4 1/4	47 3/4
84 125	(2) 51.6W LED			13	5 5/8	49 1/2



Related Products

Landscape light bollards for paths

Protection class IP 65

Cast aluminium, aluminium and stainless steel

Safety glass

Reflector made of pure anodised aluminium

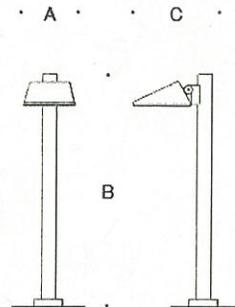
Attack angle infinitely adjustable from 0° to 90°

88 556 dimmable 1-10 V

BEGA surface washers are bolted with a mounting plate onto a foundation provided by the customer or on an anchorage unit made of hot-dip galvanised steel.

The mounting system can be used to align the luminaires.

The luminaire luminous flux and the luminaire connected wattage quoted in the table might change as a result of technical progress. On our website you will find data sheets with information on each luminaire concerning not only the current values but also the LED service life and the luminous flux in relation to the respective colour temperature.



Finish

graphite

silver

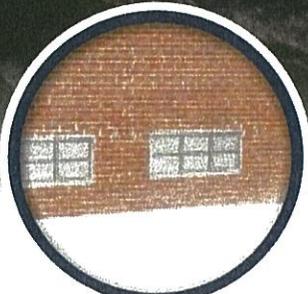
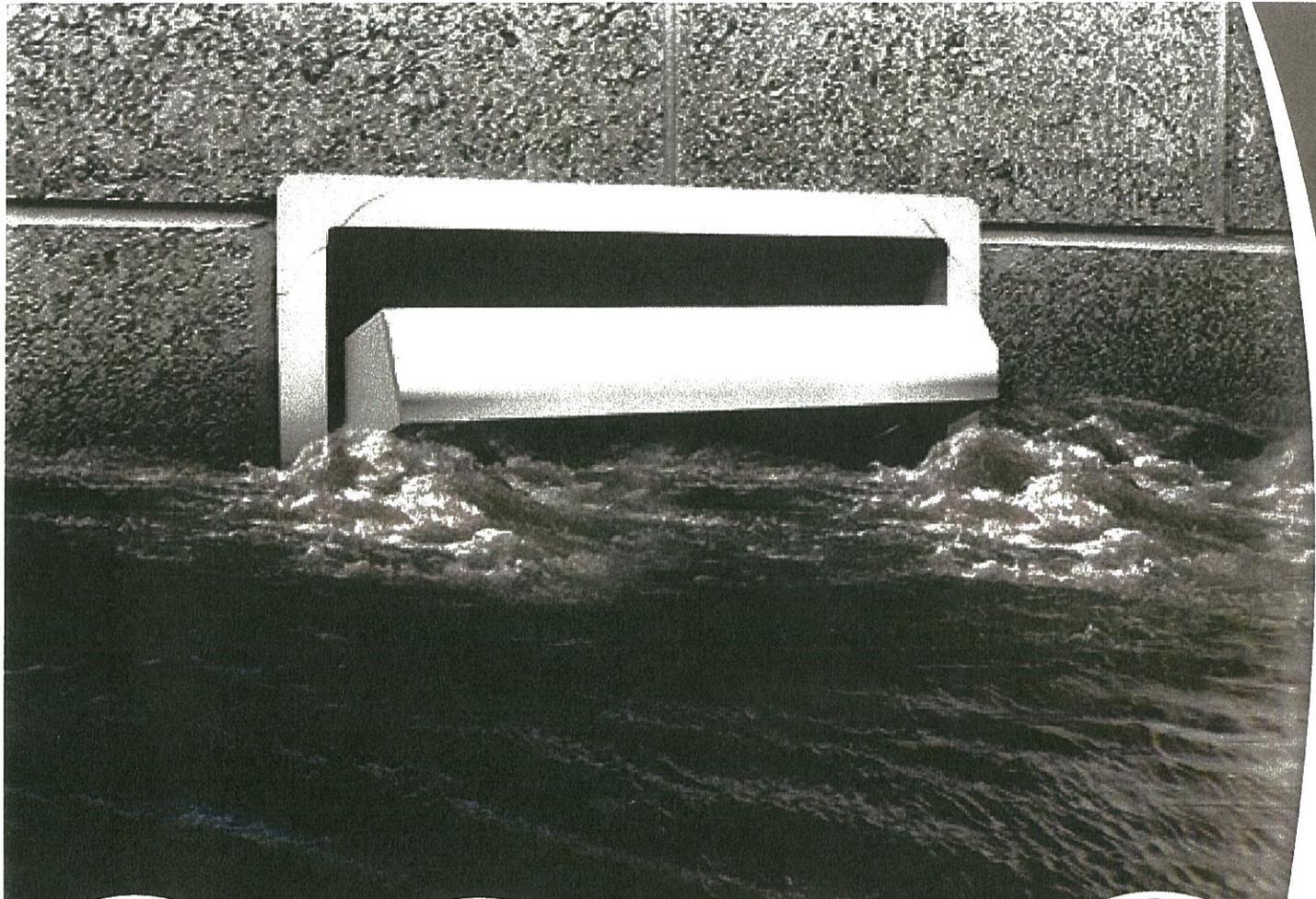
LED colour temperature

3000 K

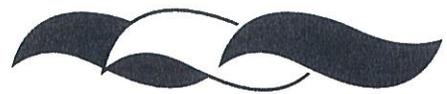
4000 K

	Lamp		Base		A	B	C	AC/DC
88 556 K3	LED	29.0 W	-	1945 lm	260	1200	360	✓
88 875	1 TC-TELI	42 W	GX 24 q-3/4		260	1200	360	✓





OCT 01 2018



SMART VENT[®]

WET & DRY FLOODPROOFING PRODUCT CATALOG



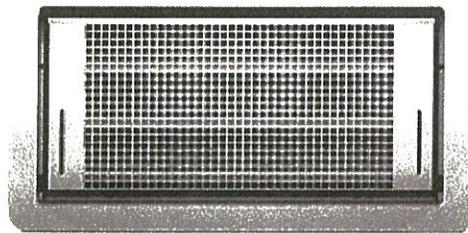
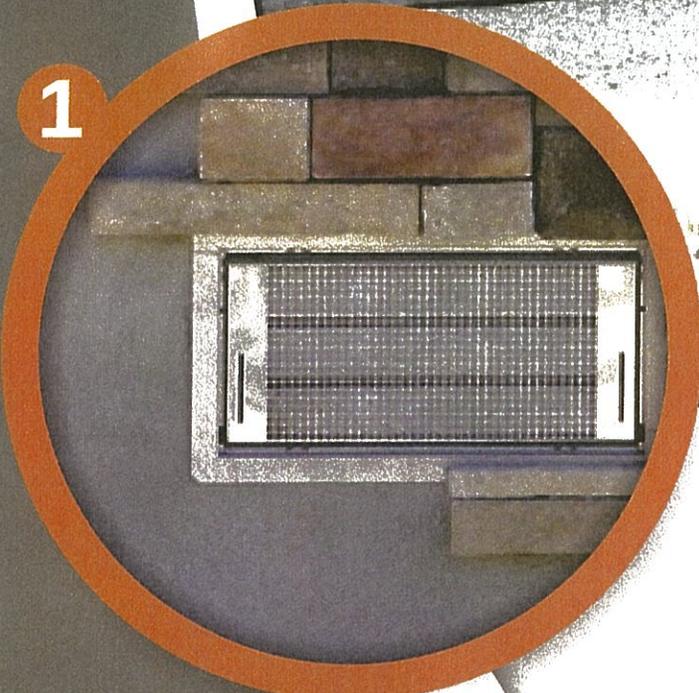
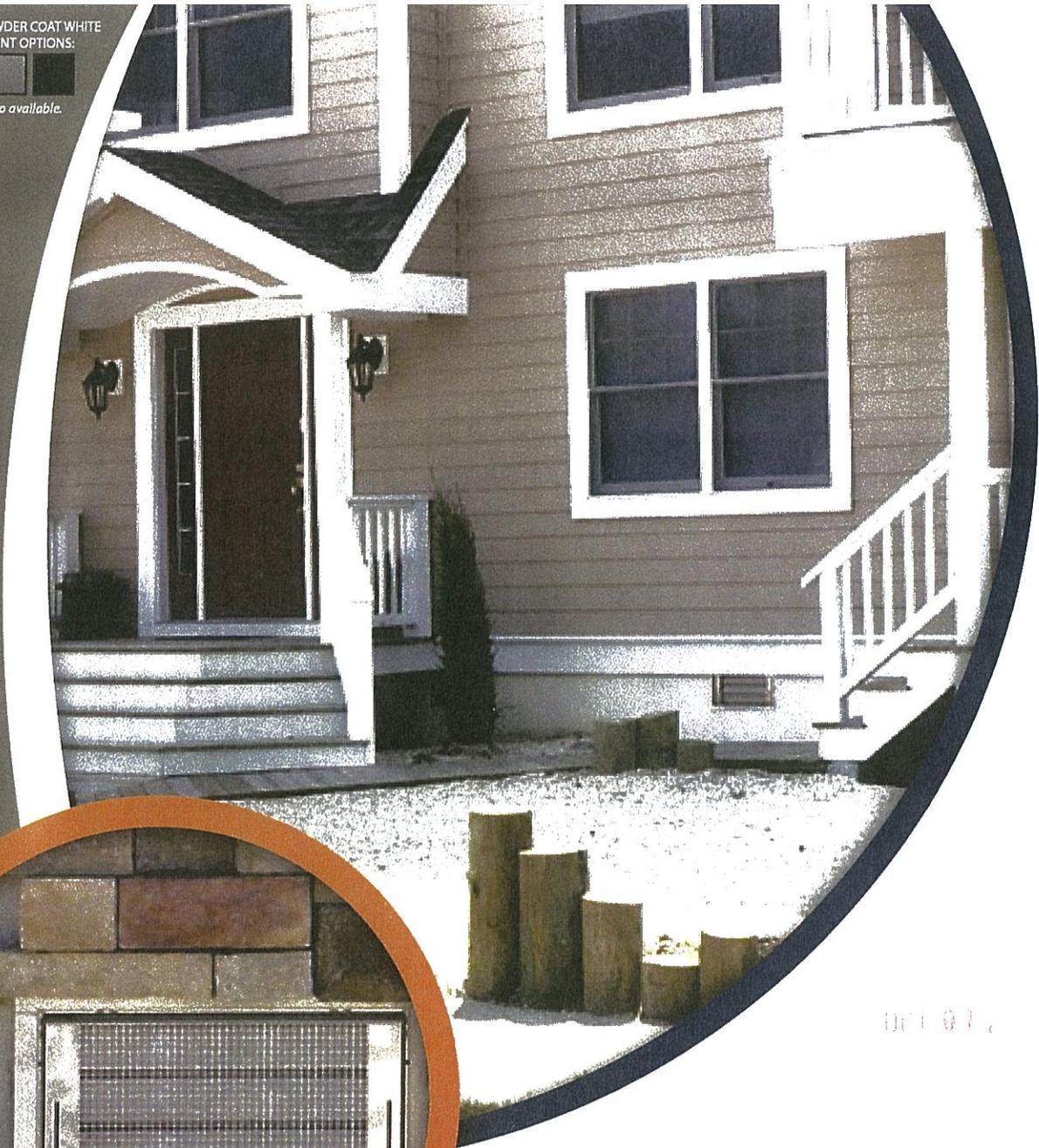
See pages 25-30
for Flood Barrier options



STANDARD FINISH POWDER COAT WHITE
POWDER COAT PAINT OPTIONS:



Custom colors also available.



MODEL NUMBER	FLOOD COVERAGE	AIR VENTILATION	VENT SIZE	ROUGH OPENING
1540-510	200 sq. ft.	51 sq. in.	16" W x 8" H x 3" D	16 $\frac{1}{4}$ in x 8 $\frac{1}{4}$ in
1540-511	400 sq. ft.	102 sq. in.	16" W x 16" H x 3" D	16 $\frac{1}{4}$ in x 16 $\frac{3}{8}$ in



To view other sizing options see pages #18-19

DUAL FUNCTION SERIES

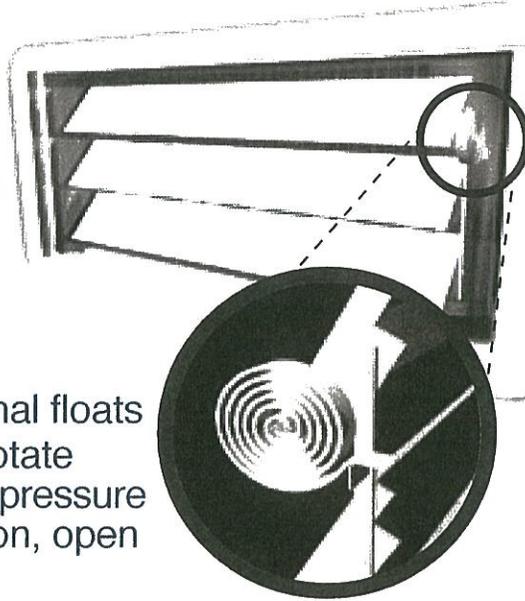
This series of vents offers certified flood venting protection with superior automatic natural ventilation control. Ideal for crawlspace applications and other areas requiring flood venting protection as well as natural air ventilation.

VENTILATION

A bimetal coil automatically opens and closes the ventilation louvers as temperature changes from 35° F to 75° F.

No electricity is required.

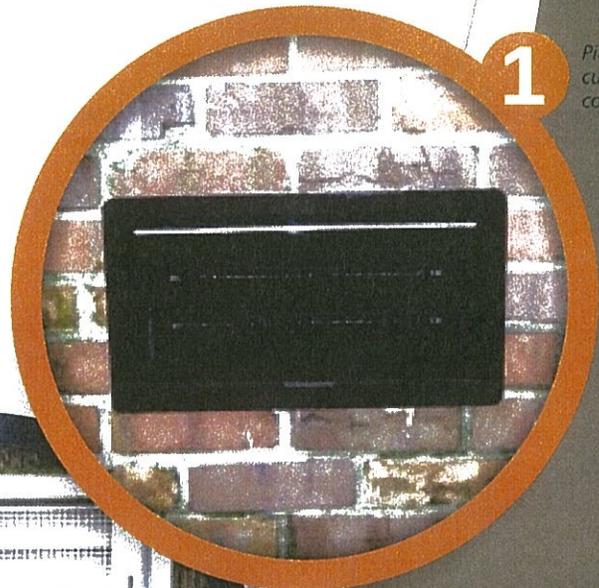
In the event of a flood, the internal floats lift to release the flood door to rotate open and relieve the hydrostatic pressure regardless of the louvers' position, open or closed.



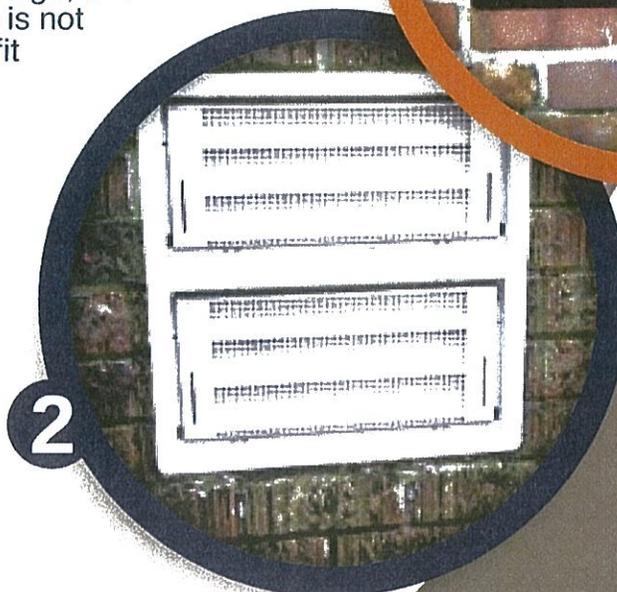
1 Smart Vent 1540-510

2 Stacker 1540-511

Stacker Models are twice as efficient as a single unit and are a great solution for large amounts of square footage, and in situations where there is not enough wall space to fit in the necessary or required single units.



Pictured in custom powder coat black



Pictured in powder coat white

OCT 10 2018

Proposed Carport Design for the Davis Town Homes Sebastopol, CA



The proposed carports are made of steel columns and supports with a corrugated metal roof and simple trim. The color proposed will be grey as shown with a light underside. The intention is to minimize the look of the carports so as not to distract from the homes. This photograph is of an apartment project in Santa Rosa but shows the style carport we propose to use.

OCT 01 2018

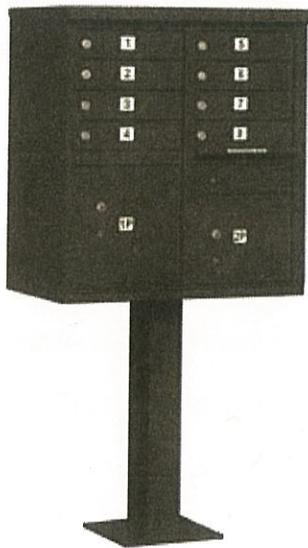


The Industry Leader in Quality Mailboxes

Back To Standard CBU's

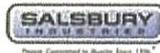
Next Item

home > shop by department > commercial mailboxes for usps delivery > cluster box units (cbu's) > standard cbu's > cluster box unit (includes pedestal) - 8 a size doors - type i - green - usps access



All Aluminum Construction

Includes Pedestal



Salsbury Cluster Box Unit (Includes Pedestal) - 8 A Size Doors - Type I - Bronze - USPS Access

Model # 3308BRZ-U

[See More Information](#)

Color



Bronze

Price **\$1,550.00**

Size 30.5" W x 62" H x 18" D

Weight 155 lbs

Volume Discount Pricing

Quantity	Price
2 - 5	\$1,435.00
6 +	\$1,320.00

[» Contact us for bulk pricing](#)

Qty 1

IN STOCK usually ships within 1-2 days

[Zoom](#)

[360° View](#)

[See Video\(s\)](#)

OCT 01 2013

Add To Cart

[More Information](#)

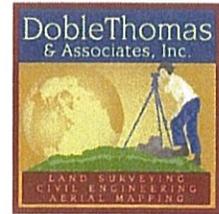
[Back to Top of Page](#)

Salsbury 3300 Series Cluster Box Units (CBU's) - U.S.P.S. Access

Features

- Salsbury Industries is officially licensed and authorized by the U.S. Postal Service to manufacture Cluster

LACO



FLOOD PLAIN GRADING SUMMARY

Project Description:

The Davis Town Homes project will consist of the development of eighteen (18) two story town homes, parking areas, one trash/recycle enclosure, driveway access to Mobile Home Park to the east, fire truck, and emergency vehicle access, stormwater bmps and landscaped areas.

100 Year Base Flood Elevation:

The Davis Town Homes site is located entirely within the 100 year flood plain of the Laguna De Santa Rosa. The Base flood elevation as shown on the FEMA FIRM map (Attached) is 78' above mean sea level.

Grading in Flood Plain:

The proposed development has been designed to conform to the City's flood plain development requirements, in the following ways:

1. The project site grading has been designed to conform to section 15.90.070 Fill which reads:

A. Except as permitted by the provisions of SMC 15.90.050, no imported fill shall be permitted in areas of special flood hazard except where an equivalent amount of soil material is removed from the project site to a location outside of the flood hazard area and such placement is approved by the City Engineer or where provision has been made on the subject property to balance the capacity to store floodwaters and accommodate potential surface flow in an amount equal to the amount of floodwater likely to be displaced by the fill. Specific measures as required by the City Engineer and Building Official shall be taken to prevent erosion and surface runoff to adjacent properties. All fill shall be compacted per the requirements of the City Engineer at the time of placement.

B. In areas of special flood hazard, any person may place up to five cubic yards of material for every 10,000 square feet of landscaped area, for use solely for landscape maintenance or gardening at a residence or business in any one calendar year.

2. The project site grading provides for a "No Net Fill" design which conforms to the City's definition:

A. "Fill" means any natural or processed earthen material of any nature whatsoever, including, but not limited to, soil, gravel, crushed rock, concrete or asphalt imported to a lot, tract or parcel other than those materials that are directly incorporated into a building or structure. For purposes of these regulations, "fill" shall not include road, driveway, sidewalk, bicycle path or approved parking area materials used to construct: up to six inches of subgrade, including gravel or rock; up to six inches of material necessary to construct a roadway crown if determined appropriate by the City Engineer; or up to six inches of above-grade concrete, asphalt, gravel or other paving material.

3. The project is expected to result in 770 cubic yards of Cut and 735 cubic yards of Fill resulting in a net cut of 35 cubic yards.

OCT 01 2018

LACO



Should you require any additional information related to our preliminary grading plan, please feel free to contact me at doblek@lacoassociates.com or by phone at 707-462-0222

Regards,

A handwritten signature in black ink, appearing to read "Kevin Doble".

Kevin Doble

Senior Project Manager

LACO Associates

Eureka | Ukiah | Santa Rosa

Advancing the quality of life for generations to come

707 462-0222

<http://www.lacoassociates.com>

<http://www.doblethomas.com>

OCT 01 2018

National Flood Hazard Layer FIRMette



38°24'21.01"N



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth Zone AE, AO, AH, VE, AR
		Regulatory Floodway

OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Area of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D

OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard Zone X
		Effective LOMRs
GENERAL STRUCTURES		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall

OTHER FEATURES		20.2 Cross Sections with 1% Annual Chance Water Surface Elevation
		17.5 Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
OTHER FEATURES		Coastal Transect Baseline
		Profile Baseline
		Hydrographic Feature

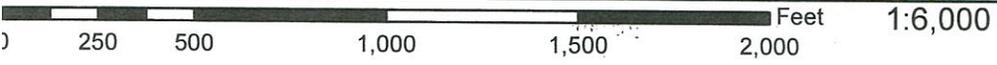
MAP PANELS		Digital Data Available
		No Digital Data Available
		Unmapped

The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 9/12/2018 at 8:16:13 PM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.



USGS The National Map: Orthoimagery. Data refreshed October 2017.

38°23'52.82"N

122°48'49.40"W

Initial Storm Water Management Plan

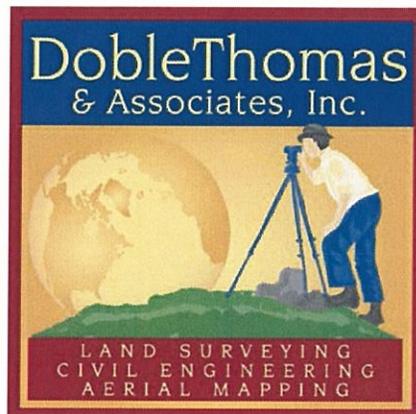
Prepared For

Davis Townhomes

Planned Development

Sebastopol Avenue
Sebastopol, Ca
LACO Job #9054.00

LACO



PREPARED BY

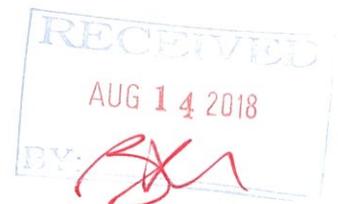
LACO ASSOCIATES
776 S. State Street
Ukiah, Ca 95482
(707) 462-0222

July 20, 2018

**Prepared under the supervision of
Rodney L. Wilburn, Jr, RCE No.69388**



My License Expires 30, June 2020



Project Description:

The Dan Davis Town Homes project will consist of the development of eighteen (18) two story town homes, parking areas, one trash/recycle enclosure, driveway access to Mobile Home Park to the east, fire truck, and emergency vehicle access, stormwater bmps and landscaped areas.

The existing site is a commercially zoned downtown property. The location is at the south terminus of Morris Street behind and to the south of commercial properties that face onto Sebastopol Avenue and an existing parking lot. A bike path and the Sebastopol Inn are located to the west. The railroad forest is located to the south. The Village Park and Tomadachi Park are located to the east. There are no wetlands or creek on site, 4 large oaks within the buildable area are proposed to remain. The general drainage pattern for this site is in a southern direction. There is no off site run-on occurring with this project. Our proposed drainage design will intercept any runoff in proposed permeable pavement, roadside bioretention areas, or vegetated swale with bioretention located in each Drainage Management Area (DMA); please refer to Preliminary SUSMP Exhibit (WQ-1) for exact locations.

Project Triggers:

The project will involve the construction of 31,162sft of new and replaced impervious areas. According to Chapter 2, of the 2017 Storm Water LID Technical Manual. Project site that create or replace less than 1.0 acres of impervious surface are required to comply with Delta Volume and Treatment. There are no creek outfalls and therefore the size of the impervious area is the only project trigger.

Pollution Prevention Measures:

Source control pollution prevention measures would include, Emergency Vehicle Access aisle and parking area sweeping and all onsite Trash Enclosures will be covered.

Runoff Reduction Measures:

Downspouts from roof gutters will be disconnected from the storm drain system and discharge into landscape areas and swales. Interceptor trees will be planted throughout the site and 4 Large Oak Trees on site will be preserved. The total tributary area used for delta volume and treatment calculations has been reduced by taking credit for these measures. See attached sheet (WQ-2)

Types of BMPs

The selected BMPs for the developed site will include the following:

Universal LID Features

- Interceptor Trees
- Impervious area disconnection

Priority 1 BMPs

- Vegetated Swale with Bio-retention (P1-06)
- Roadside Bio-retention – No Curb and Gutter (P1-02)

Priority 2 BMPs

- Pervious Pavement (P2-06)

Level of Treatment and Volume Capture

The site hydrology has been divided up into individual Drainage Management Areas (DMAs). There is no offsite run-on as described above so only delta volume and treatment of onsite runoff is provided.

The design goal of delta volume and treatment has been achieved, we have elected to design stormwater treatment system for 100% volume capture (hydromodification), as demonstrated in the Summary Results of the Storm Water Calculator and Exhibit WQ-1.

Maintenance and Funding

BMPs shall be inspected and maintained as described in the Storm Water Low Impact Development Technical Design Manual. All BMPs are located on private land throughout the entire

property furthermore, BMPs will be the sole responsibility of and funded by the Davis Townhomes Home Owner Association and Property Owner Association. All legal paperwork and agreements will be provided with the Final SUSMP Report.

Maintenance of BMPs

The maintenance of selected BMPs is recommended as follows:

Interceptor Trees

At a minimum inspection and maintenance shall include the following:

- Annual inspection prior to the rainy season.
- Annual proper watering and application of mulch.
- Routine pruning and weeding as needed.
- Replacement of trees as needed.

Impervious area disconnection

At a minimum inspection and maintenance shall include the following:

- Routinely inspect for ponding water near building foundation
- Annually inspect for undercutting/washouts at the outlet of pipe
- Annually inspect for splash blocks or rain chain damage
- Annually inspect for vegetation or debris blocking outlet of pipe

Vegetated Swale with Bio-retention (P1-06)

At a minimum maintenance shall include the following:

- Mow and irrigate during dry weather to the extent necessary to keep vegetation alive. Where 6-inch high grasses are used, the grass height shall be at least 3 inches after mowing. Where mowed grasses are shown, the grass height shall be mowed when the height exceeds 3 inches.
- Remove obstructions and trash from vegetated swale.
- Pesticides and fertilizers shall not be used in the swale.

Vegetated Swales shall be inspected and maintained monthly during the rainy season to review:

- Obstructions and trash.
- Pondered flow is drained within 72 hours after a rainfall event.
- Condition of grasses.
- If pondered water is observed, the first few inches of topsoil should be removed and replaced. If pondered water is still present, further grading and replacement may be necessary to prevent mosquito breeding.

Roadside Bio-retention – No Curb and Gutter (P1-02)

At a minimum maintenance shall include the following:

- Dry street sweeping upon completion of construction
- Dry street sweeping annually, and
 - When water is observed flowing in the gutter during a low intensity storm.
 - Algae is observed in the gutter.
 - Sediment/debris covers 1/3 of the gutter width or more.
- Inspect twice annually for sedimentation and trash accumulation in the gutter. Obstructions and trash shall be removed and properly disposed of.
- Inspect twice during the rainy season for ponded water.
- Pesticides and fertilizers shall not be used in the bioretention area.
- Plants should be pruned, weeds pulled and dead plants replaced as needed.

Attachments:

1. Determination Worksheet
2. BMP Selection Tables
3. Preliminary Calculations
4. Preliminary Details
5. Fact Sheets
6. Soils Data
7. Preliminary SUSMP Exhibit (WQ-1)
8. Water Quality Credit Exhibit (WQ-2)

FOR OFFICE USE ONLY:

Does this project require permanent storm water BMP's?

Y N

Date Submitted: _____



Print Form

File No:	Quadrant
Related Files:	
Set:	
Department Use Only	

2017 Storm Water LID Determination Worksheet

PURPOSE AND APPLICABILITY: This determination worksheet is intended to satisfy the specific requirements of "ORDER NO. R1-2015-0030, NPDES NO. CA0025054 NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT AND WASTE DISCHARGE REQUIREMENTS FOR DISCHARGES FROM THE MUNICIPAL SEPARATE STORM SEWER SYSTEMS." Additional design requirements imposed by Governing Agencies, such as local grading ordinances, CAL Green, CEQA, 401 permitting, and hydraulic design for flood control still apply as appropriate. Additionally, coverage under another regulation may trigger the requirement to design in accordance with the Storm Water LID Technical Design Manual.

Part 1: Project Information

Davis Townhomes

Project Name

Sebastopol Avenue

Project Site Address

Sebastopol, CA 95472

Project City/State/Zip

Permit Number(s) - (if applicable)

LACO Associates

Designer Name

Ukiah, CA 95482

Designer City/State/Zip

Daniel Davis

Applicant (owner or developer) Name

1051 Todd Road

Applicant Mailing Address

Santa Rosa, CA 95407

Applicant City/State/Zip

707-975-0794

Applicant Phone/Email/Fax

776 S. State Street

Designer Mailing Address

707-462-0222

Designer Phone/Email

Type of Application/Project:

Subdivision
 Grading Permit
 Building Permit
 Hillside Development
 Design Review
 Use Permit
 Encroachment
 Time Extensions
 Other : _____

PART 2: Project Exemptions

1. Is this a project that creates or replaces *less than* 10,000 square feet of impervious surface¹, including all project phases and off-site improvements?

Yes No

¹ Impervious surface replacement, such as the reconstruction of parking lots or excavation to roadway subgrades, is not a routine maintenance activity. Reconstruction is defined as work that replaces surfaces down to the subgrade. Overlays, resurfacing, trenching and patching are defined as maintenance activities per section VI.D.2.b.

2. Is this project a routine maintenance activity² that is being conducted to maintain original line and grade, hydraulic capacity, and original purpose of facility such as resurfacing existing roads and parking lots?

Yes No

3. Is this project a stand alone pedestrian pathway, trail or off-street bike lane?

Yes No

4. Did you answer "YES" to any of the questions in Part 2?

YES: This project will *not* need to incorporate permanent Storm Water BMP's as required by the NPDES MS4 Permit. **Please complete the "Exemption Signature Section" on Page 4.**

NO: Please complete the remainder of this worksheet.

Part 3: Project Triggers

Projects that Trigger Requirements:

Please answer the following questions to determine whether this project requires permanent Storm Water BMP's and the submittal of a SW LIDs as required by the NPDES MS4 Permit order No. R1-2015-0030.

1. Does this project create or replace a combined total of 10,000 square feet or more of impervious surface¹ including all project phases and off-site improvements?

Yes No

2. Does this project create or replace a combined total or 10,000 square feet or more of impervious streets, roads, highways, or freeway construction or reconstruction³? Yes No

3. Does this project create or replace a combined total of 1.0 acre or more of impervious surface¹ including all project phases and off-site improvements? Yes No

4. Did you answer "YES" to any of the above questions in Part 3?

YES: This project will need to incorporate permanent Storm Water BMP's as required by the NPDES MS4 Permit. **Please complete remainder of worksheet and sign the "Acknowledgement Signature Section" on Page 4.**

NO: This project will *not* need to incorporate permanent Storm Water BMP's as required by the NPDES MS4 permit. **Please complete the "Exemption Signature Section" on Page 4.**

1 Impervious surface replacement, such as the reconstruction of parking lots or excavation to roadway subgrades, is not a routine maintenance activity. Reconstruction is defined as work that replaces surfaces down to the subgrade. Overlays, resurfacing, trenching and patching are defined as maintenance activities per section VI.D.2.b.

2 "Routine Maintenance Activity" includes activities such as overlays and/or resurfacing of existing roads or parking lots as well as trenching and patching activities and reroofing activities per section VI.D.2.b.

3 "Reconstruction" is defined as work that extends into the subgrade of a pavement per section VI.D.2.b.

Davis Townhomes

Part 4: Project Description

1. Total Project area: 63,462

square feet
acres

2. Existing land use(s): (check all that apply)

Commercial Industrial Residential Public Other

Description of buildings, significant site features (creeks, wetlands, heritage trees), etc.:

Currently, the site consist of a vacant lot. There are no wetlands or creek on site, 4 large oaks within the buildable area are proposed to remain.

3. Existing impervious surface area: 0

square feet
acres

4. Proposed Land Use(s): (check all that apply)

Commercial Industrial Residential Public Other

Description of buildings, significant site features (creeks, wetlands, heritage trees), etc.:

The Dan Davis Townhomes project will consist of the development of eighteen (18) two story town homes, parking areas, one trash/recycle enclosure, driveway access to Mobile Home Park to the east, fire truck, and emergency vehicle access, stormwater bmps and landscaped areas.

5. Existing impervious surface area: 37,258

square feet
acres

Acknowledgment Signature Section:

As the property owner or developer, I understand that this project is required to implement permanent Storm Water Best Management Practices and provide a Storm Water Low Impact Development Submittal (SW LIDS) as required by the City's National Pollutant Discharge Elimination System (NPDES) Municipal Separate Storm Sewer Systems (MS4) Permit Order No. R1-2015-0030. *Any unknown responses must be resolved to determine if the project is subject to these requirements.

Applicant Signature

Date

Exemption Signature Section:

As the property owner or developer, I understand that this project as currently designed does not require permanent Storm Water BMP's nor the submittal of a Storm Water Low Impact Development Submittal (SW LIDS) as required by the City's National Pollutant Discharge Elimination System (NPDES) Municipal Separate Storm Sewer Systems (MS4) Permit*. I understand that redesign may require submittal of a new Determination Worksheet and may require permanent Storm Water BMP's.

Applicant Signature

Date

* This determination worksheet is intended to satisfy the specific requirements of "ORDER NO. R1-2015-0030, NPDES NO. CA0025054 NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT AND WASTE DISCHARGE REQUIREMENTS FOR DISCHARGES FROM THE MUNICIPAL SEPARATE STORM SEWER SYSTEMS." Additional design requirements imposed by Governing Agencies, such as local grading ordinances, CAL Green, CEQA, 401 permitting, and hydraulic design for flood control still apply as appropriate. Additionally, coverage under another regulation may trigger the requirement to design in accordance with the Storm Water LID Technical Design Manual.

Implementation Requirements: All calculations shall be completed using the "Storm Water Calculator" available at: www.srcity.org/stormwaterLID

Hydromodification Control/100% Volume Capture: Capture (infiltration and/or reuse) of 100% of the volume of runoff generated by a 1.0" 24-hour storm event, as calculated using the "Urban Hydrology for Small Watersheds" TR-55 Manual method. This is a retention requirement.

Treatment Requirement: Treatment of 100% of the flow calculated using the modified Rational Method and a known intensity of 0.20 inches per hour.

Delta Volume Capture Requirement: Capture (infiltration and/or reuse) of the increase in volume of storm water due to development generated by a 1.0" 24-hour storm event, as calculated using the "Urban Hydrology for Small Watersheds" TR-55 Manual method. This is a retention requirement.

Project Name: Davis Townhomes

Best Management Practice (BMP)	Detail Sheet	Detail Title	Can be used with...										BMP in priority selected?	Yes	No	Unique Identifier of BMP per plan	Explanation of selection	Other notes:	
			High Ground Water Contamination	Slope Constraints	Achieves Treatment	Volume Capture	Runoff Reduction Measure	Yes	No	Unique Identifier of BMP per plan	Explanation of selection	Other notes:							
Universal BMP- to be considered on all projects.	Living Roof	N/A	N/A	X	X	X		X	X										
	Rainwater Harvesting	N/A	N/A	X	X	X		X											
Runoff Reduction Measures	Interceptor Trees	N/A	N/A	X	X	X			X				✓		IT				
	Bovine Terrace	RRM-01	Bovine Terrace	X					X										
	Vegetated Buffer Strip	RRM-02	Vegetated Buffer Strip						X										
	Impervious Area Disconnection	N/A	N/A	X	X	X			X				✓		D.R.				
Priority 1- to be installed with no underdrains or liners. Must drain all staging water within 72 hours.	Bioretention	P1-02	Roadside Bioretention - no C & G					X	X				✓		BRA#				
	Vegetated Swale-with Bioretention	P1-06	Swale with Bioretention					X	X				✓		BRS#				
	Constructed Wetlands	N/A	N/A					X	X										
Priority 2 BMPs- with subsurface drains installed above the capture volume.	Bioretention	P2-02	Roadside Bioretention- Flush Design Roadside					X	X										
		P2-03	Roadside Bioretention- Contiguous SW					X	X										
		P2-04	Roadside Bioretention- Curb Opening					X	X										
		P2-05	Roadside Bioretention- No C & G					X	X										
	Constructed Wetlands	N/A	N/A					X	X										

Date: 07/19/18

Page 1 of 2

BMP Selection Table

Best Management Practice (BMP)	Detail Sheet	Detail Title	Can be used with...			Slope Constraints Achieved	Treatment	Volume Capture	Runoff Reduction Measure	BMP in priority selected?		Unique Identifier of BMP per plates	Explanation of selection	Other notes:	
			High Ground Water Contamination	Slope Constraints	Achieved					Yes	No				
Priority 3 BMPs- installed with subdrains and/or impermeable liner. Does not achieve volume capture and must be used as part of a treatment train.	Bioretention	P3-02	Roadside Bioretention - Flush Design Roadside	X	X	X	X								
		P3-03	Roadside Bioretention- Contiguous SW	X	X	X	X								
		P3-04	Roadside Bioretention- Curb Opening	X	X	X	X								
	Flow Through Planters	P3-05	Flow Through Planters	X	X	X	X								
	Vegetated Swale	P3-06	With Bioretention	X	X	X	X	X							
		P3-07	Vegetated Swale	X	X	X	X								
	Priority 4 BMPs- does not achieve volume capture and must be used as part of a	Tree Filter Unit			X	X	X	X							
Modular Bioretention				X	X	X	X								
Priority 5 BMPs- does not achieve volume capture and must be used as part of a treatment train.	Chambered Separator Units			X	X	X	X								
	Centrifugal Separator Units			X	X	X	X								
	Trash Excluders			X	X	X	X								
	Filter Inserts			X	X	X	X								
Priority 6 BMPs- see the "Offset Program" chapter for details.	Offset Program						N/A	N/A	N/A						
Other	Detention			X											



STORM WATER CALCULATOR

LID BMP Summary Page & Site Global Values

Project Information: Project Name: DAVIS TOWNHOMES Address/Location: SEBASTOPOL AVE Designer: KDOBLE Date: 7/16/2018	Site Information: Mean Seasonal Precipitation (MSP) of Project Site: 35.00 (inches) K=MSP/31 K= 1.17 Impervious area - pre development: 6,969.0 ft ² Impervious area - post development: 31,162.0 ft ²	Based upon the pre and post development impervious area, the post construction BMP requirement is: <div style="text-align: center; color: red; font-weight: bold; font-size: 1.2em;">Delta Volume & Treatment</div>
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Summary of Saved BMP Results:

BMP ID:	Tributary Area		Requirements			BMP Design Results						
	Tributary Area (ft ²)	Runoff Reduction Measures (Y/N)	Type of Requirement Met	Type of BMP Design	Percent Achieved	Hydromodification Control		Flow Base Treatment		Delta Volume Capture		
						Required V _{Hydromod} (ft ³)	Achieved (ft ³)	Required Q Treatment (cfs)	Achieved (ft ³)	Required Vdelta (ft ³)	Achieved (ft ³)	
1	DMA-1	6,225	Yes	Hydromod Volume Capture	Priority 2: P2-06 Permeable Pavement	126.1	419.7875	529.2000				
2	DMA-2	5,718	Yes	Hydromod Volume Capture	Priority 1: P1-02 Roadside Bioretention - No Curb and Gutter	106.9	392.9402	420.0000				
3	DMA-3	5,787	Yes	Hydromod Volume Capture	Priority 2: P2-06 Permeable Pavement	103.5	409.1439	423.3600				
4	DMA-4	15,636	Yes	Hydromod Volume Capture	Priority 1: P1-02 Roadside Bioretention - No Curb and Gutter	102.0	375.1600	382.5000				
5	DMA-5	9,174	Yes	Hydromod Volume Capture	Priority 1: P1-02 Roadside Bioretention - No Curb and Gutter	105.6	282.4096	298.2000				
6	DMA-6	11,844	Yes	Hydromod Volume Capture	Priority 1: P1-06 Swale with Bioretention	126.6	252.8307	320.0000				
7	DMA-7	9,078	Yes	Hydromod Volume Capture	Priority 1: P1-02 Roadside Bioretention - No Curb and Gutter	129.8	242.6698	315.0000				
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STORM WATER CALCULATOR

BMP Tributary Parameters		Project Name: DAVIS TOWNHOMES
BMP ID:	DMA-1	
BMP Design Criteria:	Delta Volume & Treatment	
Type of BMP Design:	Priority 2: P2-06 Permeable Pavement	
BMP's Physical Tributary Area:	6,225.0 ft ²	
Description/Notes:		

Runoff Reduction Measures	Resulting reduced Tributary Area used for BMP sizing =	5,285.0 ft ²
	Total Runoff Reduction Measures =	940.0 ft ²

Interceptor Trees		Total Number of <u>New</u> trees in BMP Tributary Area: 4
Number of <i>new</i> interceptor <i>Evergreen Trees</i> :	0	
Number of <i>new</i> interceptor <i>Deciduous Trees</i> :	4	
Square footage of qualifying <i>existing</i> tree canopy:	0.0 ft ²	
Disconnected Roof Drains		
Select disconnection condition:	Select disconnection condition	
Disconnected Roof Drains Method 1		
Roof area of disconnected downspouts:	0 ft ²	
Disconnected Roof Drains Method 2		
Percent of rooftop area:	0 %	
Select Density:	1 Units per Acre	
Paved Area Disconnection		
Paved Area Type:	Porous Pavement	
Alternatively designed paved area:	540.0 ft ²	
Buffer Strips & Bovine Terraces		
Area draining to a Buffer Strip or Bovine Terrace:	0.0 ft ²	

Hydromodification Requirement: 100% Volume Capture; V_{HYDROMOD}	V _{HYDROMOD} =	419.79 ft ³
Post development hydrologic soil type within tributary area:	C: 0.05 - 0.15 in/hr infiltration (transmission) rate	
Post development ground cover description:	Impervious - Paved Parking, Rooftop, Driveways	
CN _{POST} :	98	
User Composite post development CN:	0.0	

BMP Sizing Tool: Hydromodification Requirement		Percent of Goal Achieved =	126.06 %
	BMP Volume Below Ground		Ponded Water Above Ground
Porosity:	0.40	Depth:	0.08 ft
Depth below perforated pipe if present:	2.25 ft	Width:	6.00 ft
Width:	6.00 ft	Length:	90.00 ft
Length:	90.00 ft	Area:	0.00 ft ²
Area:	0.00 ft ²		



STORM WATER CALCULATOR

BMP Tributary Parameters		Project Name: DAVIS TOWNHOMES
BMP ID:	DMA-2	
BMP Design Criteria:	Delta Volume & Treatment	
Type of BMP Design:	Priority 1: P1-02 Roadside Bioretention - No Curb and Gutter	
BMP's Physical Tributary Area:	5,718.0	ft ²
Description/Notes:		

Runoff Reduction Measures	Resulting reduced Tributary Area used for BMP sizing =	4,947.1	ft ²
	Total Runoff Reduction Measures =	770.9	ft ²

Interceptor Trees			
Number of <i>new</i> interceptor Evergreen Trees :	0	Total Number of <i>New</i> trees in BMP Tributary Area:	5
Number of <i>new</i> interceptor Deciduous Trees :	5		
Square footage of qualifying existing tree canopy:	0.0		ft ²
Disconnected Roof Drains			
Select disconnection condition:	Runoff is directed across landscape; Width of area: 15' to 19'		
Disconnected Roof Drains Method 1		Disconnected Roof Drains Method 2	
Roof area of disconnected downspouts:	387	Percent of rooftop area:	0 %
	ft ²	Select Density:	1 Units per Acre
Paved Area Disconnection			
Paved Area Type:	Porous Pavement		
Alternatively designed paved area:	0.0		ft ²
Buffer Strips & Bovine Terraces			
Area draining to a Buffer Strip or Bovine Terrace:	0.0		ft ²

Hydromodification Requirement: 100% Volume Capture; V_{HYDROMOD}	V _{HYDROMOD} =	392.94	ft ³
Post development hydrologic soil type within tributary area:	C: 0.05 - 0.15 in/hr infiltration (transmission) rate		
Post development ground cover description:	Impervious - Paved Parking, Rooftop, Driveways		
CN _{POST} :	98		
User Composite post development CN:	0.0		

BMP Sizing Tool: Hydromodification Requirement		Percent of Goal Achieved =	106.89	%
	BMP Volume Below Ground		Ponded Water Above Ground	
Porosity:	0.40	Depth:	0.25	ft
Depth below perforated pipe if present:	2.00	Width:	0.00	ft
Width:	0.00	Length:	0.00	ft
Length:	0.00	Area:	400.00	ft ²
Area:	400.00			



STORM WATER CALCULATOR

BMP Tributary Parameters		Project Name: DAVIS TOWNHOMES
BMP ID:	DMA-3	
BMP Design Criteria:	Delta Volume & Treatment	
Type of BMP Design:	Priority 2: P2-06 Permeable Pavement	
BMP's Physical Tributary Area:	5,787.0	ft²
Description/Notes:		

Runoff Reduction Measures	Resulting reduced Tributary Area used for BMP sizing =	5,151.0	ft²
	Total Runoff Reduction Measures =	636.0	ft²

Interceptor Trees		Total Number of <u>New</u> trees in BMP Tributary Area: 0
Number of <i>new</i> interceptor Evergreen Trees :	0	
Number of <i>new</i> interceptor Deciduous Trees :	0	
Square footage of qualifying existing tree canopy :	408.0	ft²
Disconnected Roof Drains		
Select disconnection condition:	Select disconnection condition	
Disconnected Roof Drains Method 1		
Roof area of disconnected downspouts:	0	ft²
Disconnected Roof Drains Method 2		
Percent of rooftop area:	0	%
Select Density:	1	Units per Acre
Paved Area Disconnection		
Paved Area Type:	Porous Pavement	
Alternatively designed paved area:	432.0	ft²
Buffer Strips & Bovine Terraces		
Area draining to a Buffer Strip or Bovine Terrace:	0.0	ft²

Hydromodification Requirement: 100% Volume Capture; V_{HYDROMOD}	V _{HYDROMOD} =	409.14	ft³
Post development hydrologic soil type within tributary area:	C: 0.05 - 0.15 in/hr infiltration (transmission) rate		
Post development ground cover description:	Impervious - Paved Parking, Rooftop, Driveways		
CN _{POST} :	98		
User Composite post development CN:	0.0		

BMP Sizing Tool: Hydromodification Requirement		Percent of Goal Achieved =	103.47	%
BMP Volume Below Ground		Ponded Water Above Ground		
Porosity:	0.40	Depth:	0.08	ft
Depth below perforated pipe if present:	2.25	Width:	6.00	ft
Width:	6.00	Length:	72.00	ft
Length:	72.00	Area:	0.00	ft²
Area:	0.00			



STORM WATER CALCULATOR

BMP Tributary Parameters		Project Name: DAVIS TOWNHOMES
BMP ID:	DMA-4	
BMP Design Criteria:	Delta Volume & Treatment	
Type of BMP Design:	Priority 1: P1-02 Roadside Bioretention - No Curb and Gutter	
BMP's Physical Tributary Area:	15,636.0	ft ²
Description/Notes:		

Runoff Reduction Measures	Resulting reduced Tributary Area used for BMP sizing =	10,375.1	ft ²
	Total Runoff Reduction Measures =	5,261.0	ft ²

Interceptor Trees		Total Number of <u>New</u> trees in BMP Tributary Area: 23
Number of <i>new</i> interceptor Evergreen Trees :	0	
Number of <i>new</i> interceptor Deciduous Trees :	23	
Square footage of qualifying existing tree canopy :	683.0	ft ²

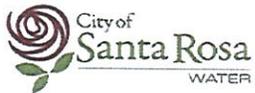
Disconnected Roof Drains		Select disconnection condition: Runoff is directed across landscape; Width of area: 10' to 14'
Disconnected Roof Drains Method 1	Roof area of disconnected downspouts: 5,821	ft ²
Disconnected Roof Drains Method 2	Percent of rooftop area:	0 %
	Select Density:	1 Units per Acre

Paved Area Disconnection		Paved Area Type: Porous Pavement
Alternatively designed paved area:	0.0	ft ²

Buffer Strips & Bovine Terraces	Area draining to a Buffer Strip or Bovine Terrace: 0.0	ft ²
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Hydromodification Requirement: 100% Volume Capture; V_{HYDROMOD}		V _{HYDROMOD} = 375.16	ft ³
Post development hydrologic soil type within tributary area:	C: 0.05 - 0.15 in/hr infiltration (transmission) rate		
Post development ground cover description:	Residential - 1/8 acre or less (town houses)		
CN _{POST} :	90		
User Composite post development CN:	0.0		

BMP Sizing Tool: Hydromodification Requirement		Percent of Goal Achieved =	101.96	%
	BMP Volume Below Ground		Ponded Water Above Ground	
Porosity:	0.40		Depth:	0.25 ft
Depth below perforated pipe if present:	1.50 ft		Width:	0.00 ft
Width:	0.00 ft		Length:	0.00 ft
Length:	0.00 ft		Area:	450.00 ft ²
Area:	450.00 ft ²			



STORM WATER CALCULATOR

BMP Tributary Parameters		Project Name: DAVIS TOWNHOMES
BMP ID:	DMA-5	
BMP Design Criteria:	Delta Volume & Treatment	
Type of BMP Design:	Priority 1: P1-02 Roadside Bioretention - No Curb and Gutter	
BMP's Physical Tributary Area:	9,174.0	ft ²
Description/Notes:		

Runoff Reduction Measures	Resulting reduced Tributary Area used for BMP sizing =	7,810.5	ft ²
	Total Runoff Reduction Measures =	1,363.6	ft ²

Interceptor Trees		Total Number of <u>New</u> trees in BMP Tributary Area: 5
Number of <i>new</i> interceptor Evergreen Trees :	0	
Number of <i>new</i> interceptor Deciduous Trees :	5	
Square footage of qualifying existing tree canopy :	0.0	ft ²

Disconnected Roof Drains		Select disconnection condition: Runoff is directed across landscape; Width of area: 10' to 14'
Disconnected Roof Drains Method 1	Roof area of disconnected downspouts: 1,919	ft ²
Disconnected Roof Drains Method 2	Percent of rooftop area: 0	%
	Select Density: 1	Units per Acre

Paved Area Disconnection		Paved Area Type: Porous Pavement
	Alternatively designed paved area: 0.0	ft ²

Buffer Strips & Bovine Terraces	Area draining to a Buffer Strip or Bovine Terrace: 0.0	ft ²
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Hydromodification Requirement: 100% Volume Capture; V_{HYDROMOD}		V _{HYDROMOD} = 282.41	ft ³
Post development hydrologic soil type within tributary area:	C: 0.05 - 0.15 in/hr infiltration (transmission) rate		
Post development ground cover description:	Residential - 1/8 acre or less (town houses)		
CN _{POST} :	90		
User Composite post development CN:	0.0		

BMP Sizing Tool: Hydromodification Requirement		Percent of Goal Achieved = 105.59	%
	BMP Volume Below Ground	Ponded Water Above Ground	
Porosity:	0.40	Depth:	0.25 ft
Depth below perforated pipe if present:	2.00 ft	Width:	0.00 ft
Width:	0.00 ft	Length:	0.00 ft
Length:	0.00 ft	Area:	284.00 ft ²
Area:	284.00 ft ²		



STORM WATER CALCULATOR

BMP Tributary Parameters		Project Name: DAVIS TOWNHOMES
BMP ID:	DMA-6	
BMP Design Criteria:	Delta Volume & Treatment	
Type of BMP Design:	Priority 1: P1-06 Swale with Bioretention	
BMP's Physical Tributary Area:	11,844.0 ft ²	
Description/Notes:		

Runoff Reduction Measures	Resulting reduced Tributary Area used for BMP sizing =	6,992.3 ft ²
	Total Runoff Reduction Measures =	4,851.7 ft ²

Interceptor Trees		
Number of <i>new</i> interceptor Evergreen Trees :	0	Total Number of <u>New</u> trees in BMP Tributary Area: 12
Number of <i>new</i> interceptor Deciduous Trees :	12	
Square footage of qualifying existing tree canopy :	395.0 ft ²	
Disconnected Roof Drains		
Select disconnection condition:	Runoff is directed across landscape; Width of area: 20' to 24'	
Disconnected Roof Drains Method 1		Disconnected Roof Drains Method 2
Roof area of disconnected downspouts:	3,838 ft ²	Percent of rooftop area: 0 %
		Select Density: 1 Units per Acre
Paved Area Disconnection		
Paved Area Type:	Porous Pavement	
Alternatively designed paved area:	0.0 ft ²	
Buffer Strips & Bovine Terraces		
Area draining to a Buffer Strip or Bovine Terrace:	0.0 ft ²	

Hydromodification Requirement: 100% Volume Capture; V_{HYDROMOD}		V _{HYDROMOD} = 252.83 ft ³
Post development hydrologic soil type within tributary area:	C: 0.05 - 0.15 in/hr infiltration (transmission) rate	
Post development ground cover description:	Residential - 1/8 acre or less (town houses)	
CN _{POST} :	90	
User Composite post development CN:	0.0	

BMP Sizing Tool: Hydromodification Requirement		Percent of Goal Achieved = 126.57 %
	BMP Volume Below Ground	Ponded Water Above Ground
Porosity:	0.40	Depth: 0.25 ft
Depth below perforated pipe if present:	2.00 ft	Width: 0.00 ft
Width:	2.00 ft	Length: 200.00 ft
Length:	200.00 ft	Area: 0.00 ft ²
Area:	0.00 ft ²	



STORM WATER CALCULATOR

BMP Tributary Parameters		Project Name: DAVIS TOWNHOMES
BMP ID:	DMA-7	
BMP Design Criteria:	Delta Volume & Treatment	
Type of BMP Design:	Priority 1: P1-02 Roadside Bioretention - No Curb and Gutter	
BMP's Physical Tributary Area:	9,078.0	ft ²
Description/Notes:		

Runoff Reduction Measures	Resulting reduced Tributary Area used for BMP sizing =	6,711.5	ft ²
	Total Runoff Reduction Measures =	2,366.6	ft ²

Interceptor Trees		Total Number of <u>New</u> trees in BMP Tributary Area: 4
Number of <i>new</i> interceptor Evergreen Trees :	0	
Number of <i>new</i> interceptor Deciduous Trees :	4	
Square footage of qualifying existing tree canopy :	1,747.0	ft ²

Disconnected Roof Drains		Select disconnection condition: Runoff is directed across landscape; Width of area: 10' to 14'
Disconnected Roof Drains Method 1	Roof area of disconnected downspouts: 2,429	ft ²
Disconnected Roof Drains Method 2	Percent of rooftop area:	0 %
	Select Density:	1 Units per Acre

Paved Area Disconnection		Paved Area Type: Porous Pavement
Alternatively designed paved area:	0.0	ft ²

Buffer Strips & Bovine Terraces	Area draining to a Buffer Strip or Bovine Terrace: 0.0	ft ²
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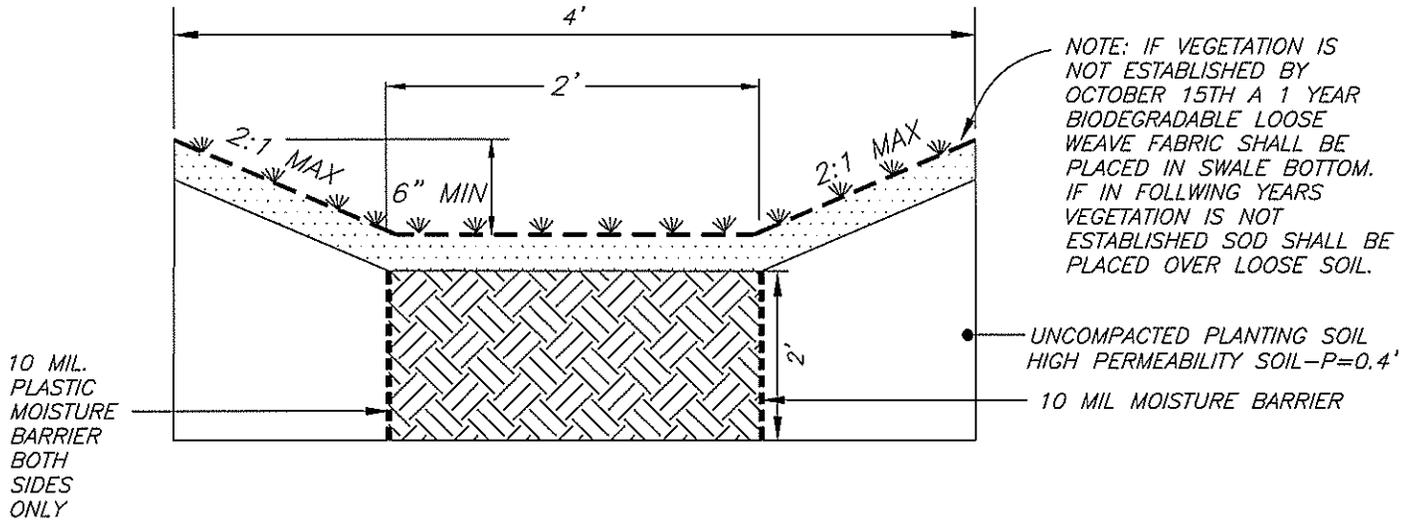
Hydromodification Requirement: 100% Volume Capture; V_{HYDROMOD}		V _{HYDROMOD} = 242.67	ft ³
Post development hydrologic soil type within tributary area:	C: 0.05 - 0.15 in/hr infiltration (transmission) rate		
Post development ground cover description:	Residential - 1/8 acre or less (town houses)		
CN _{POST} :	90		
User Composite post development CN:	0.0		

BMP Sizing Tool: Hydromodification Requirement		Percent of Goal Achieved = 129.81	%
BMP Volume Below Ground		Ponded Water Above Ground	
Porosity:	0.40	Depth:	0.25 ft
Depth below perforated pipe if present:	2.00 ft	Width:	0.00 ft
Width:	0.00 ft	Length:	0.00 ft
Length:	0.00 ft	Area:	300.00 ft ²
Area:	300.00 ft ²		

PROJECT	DAVIS TOWNHOMES
CLIENT	DANIEL DAVIS
LOCATION	SEBASTOPOL AVE., SEBASTOPOL, CA 95472
PRELIMINARY DETAILS	

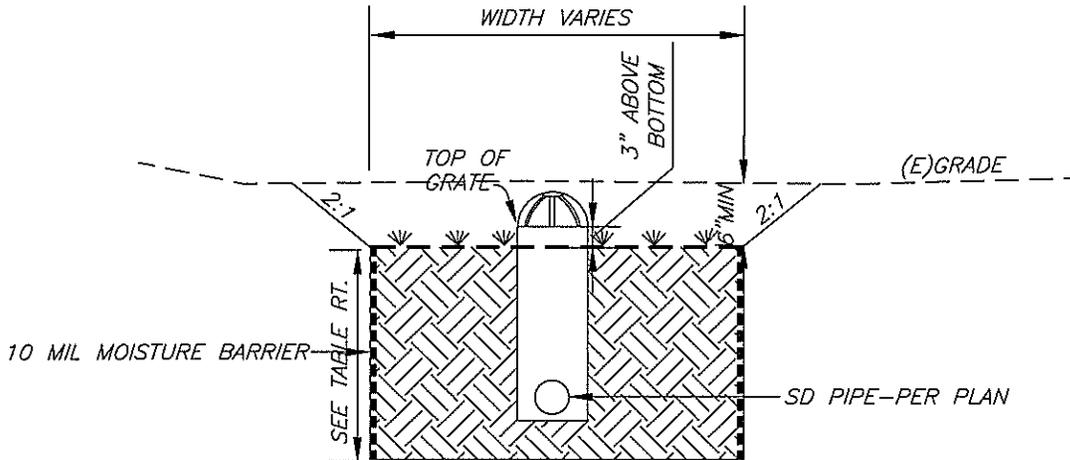
BY	GG	FIGURE	1 OF 2
DATE	07-31-18		
CHECK	KD	JOB NO.	9054.00
SCALE	NTS		

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VEGETATED SWALE W/BIO-RETENTION

NO SCALE



BIO-RETENTION AREAS SHALL BE CONSTRUCTED UNDER THE DIRECTION OF THE PROJECT ENGINEER AND SOILS ENGINEER

BIO-RETENTION AREA

TYPICAL SECTION NO SCALE

SOIL DEPTH TABLE

BMP ID.	SOIL DEPTH	SF
BRA#1	2.0'	400
BRA#2	2.0'	284
BRA#3	1.5'	450
BRA#4	2.0'	300

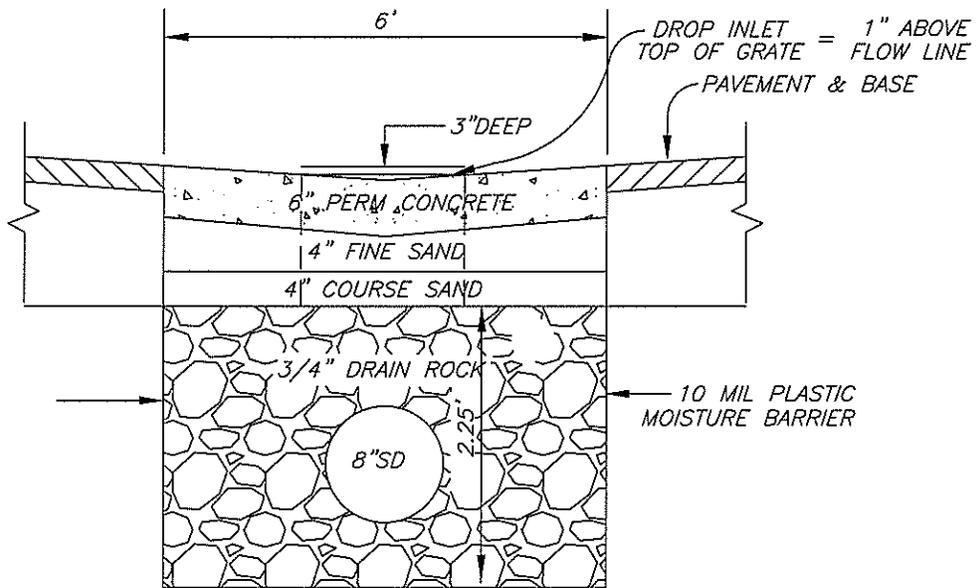
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PROJECT	DAVIS TOWNHOMES	BY	GG	FIGURE	2 OF 2
CLIENT	DANIEL DAVIS	DATE	07-31-18		
LOCATION	SEBASTOPOL AVE., SEBASTOPOL, CA 95472	CHECK	KD	JOB NO.	9054.00
PRELIMINARY DETAILS			SCALE	NTS	

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VALLEY GUTTER

PERMEABLE CONCRETE DETAIL

NO SCALE

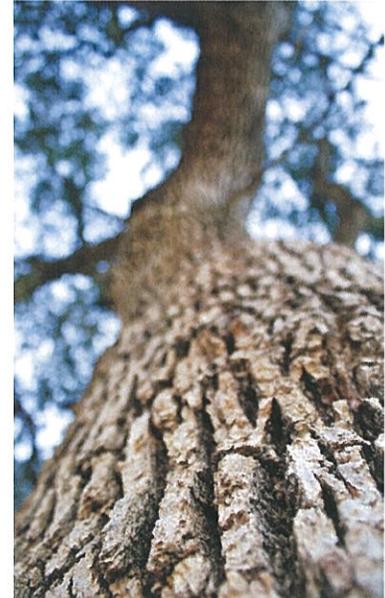
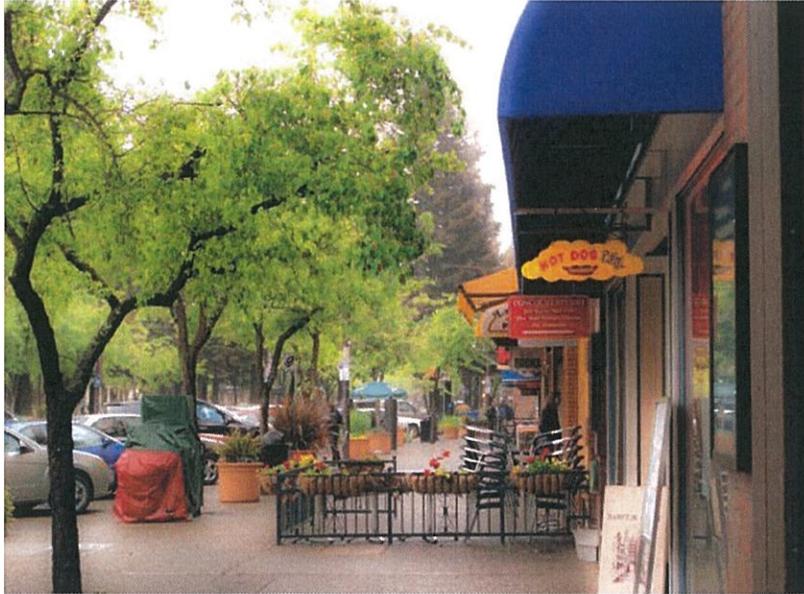
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FACT SHEET- INTERCEPTOR TREES

Runoff Reduction Measure

INTERCEPTOR TREES

Also known as: Tree Credits



DESCRIPTION

Interceptor trees are new or existing trees with their trunks located within 25' of impervious areas. Trees intercept rain water on their leaves and branches, allowing rain water to evaporate or run down the branches and trunk of the tree where it readily infiltrates into the soil. Tree roots also increase infiltration. Trees also provide shade over impervious surfaces which reduce peak flow in streams and provide shade which reduces the “heat island” effects of urban areas.

Interceptor tree credits are calculated into the site design by reducing the amount of tributary area that must be used to calculate treatment and volume capture. New deciduous trees provide a credit of 100 ft², new evergreen trees provide 200 ft², and existing trees provide one half of the existing canopy. The total area reduction credit due to the use of interceptor trees cannot exceed one half of the total physical tributary area. All calculations shall be completed using the “Storm Water Calculator” available at www.srcity.org/stormwaterLID.



FACT SHEET- INTERCEPTOR TREES

ADVANTAGES

- Reduces the size of downstream storm water BMPs.
- Enhances water quality of downstream water bodies through natural processes.
- Aesthetically pleasing.
- Provides shade to cool pavement and reduces surface runoff temperatures.
- Aids in removal of air pollutants and noise reduction.
- Trees required by the permitting agency may be counted as interceptor trees.
- Establishes habitat for birds and other pollinators like butterflies and bees.

KEY DESIGN FEATURES

- Appropriate new trees must be selected from the approved **Tree List** included in Appendix G.
- Existing trees must be adequately protected during construction.
- Only tree that overhang impervious areas or whose trunks are within 25' of impervious areas can qualify as interceptor trees.
- All calculations shall be completed using the "Storm Water Calculator" available at www.srcity.org/stormwaterLID.

INSPECTION AND MAINTENANCE REQUIREMENTS

A maintenance plan shall be provided with the Final SWLID Submittal. The maintenance plan shall include recommended maintenance practices, state the parties responsible for maintenance and upkeep, specify the funding source for ongoing maintenance with provisions for full replacement when necessary, and provide site specific inspection checklist.

At a minimum inspection and maintenance shall include the following:

- Annual inspection prior to the rainy season.
- Annual proper watering and application of mulch.
- Routine pruning and weeding as needed.
- Replacement of trees as needed.

FACT SHEET- IMPERVIOUS AREA DISCONNECTION

Runoff Reduction Measure

IMPERVIOUS AREA DISCONNECTION

Including: splash blocks, rain chains, bubble up emitters, and pavement disconnection.



OVERVIEW

Impervious area disconnection allows storm water from impervious areas, such as rooftops and pavement, to be directed to pervious natural or landscaped areas and infiltrate into the soil. Impervious surfaces that drain directly to catch basins or storm drains are a directly connected impervious area. These areas prevent storm water infiltration into the soil or filtering through vegetation and soil. Impervious areas also increase the speed and amount of runoff from a site, which may contribute to peak flows and scour in downstream creeks and waterways.

This BMP addresses these issues by disconnecting direct discharges by using: splash blocks, bubble-up emitter, and paved area disconnection.

DOWNSPOUT DISCONNECTION- DESCRIPTION

Disconnecting downspouts and using splash blocks or rain chains is a low tech option to hard piped downspout systems. Existing downspouts can be retrofitted.

ADVANTAGES

- Reduces the size of downstream storm water BMPs.
- Can be used on sloped sites.
- Increases infiltration potential.
- Increases time of concentration.

FACT SHEET- IMPERVIOUS AREA DISCONNECTION

- Can be used as a retrofit BMP.

LIMITATIONS

- Adjacent buildings need to be considered in design.
- Ultimate storm water collection needs to be considered in design.
- May not be appropriate on all sites due to space constraints.

KEY DESIGN FEATURES

- Sites should be evaluated to ensure disconnecting downspouts won't have negative impacts.
- Rain water must be directed away from foundations and footings.
- Downspouts should not be directed to paved areas or across sidewalks.
- Landscaped areas receiving roof water should be adequately sized to prevent runoff or erosion and to allow for infiltration.
- All calculations shall be completed using the "Storm Water Calculator" available at www.srcity.org/stormwaterLID.



BUBBLE-UP EMITTER-DESCRIPTION

Bubble-up emitters work very much like disconnected downspouts with splash blocks, but allow for storm water to be released further from the building or into landscape areas that are not directly adjacent to the building.

ADVANTAGES

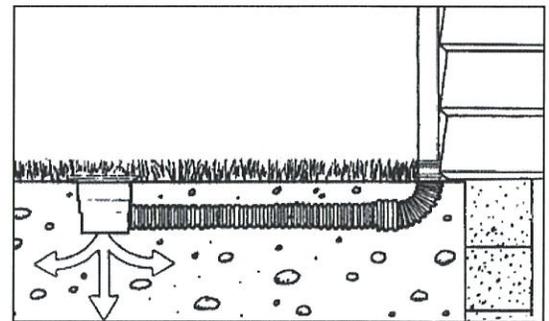
- Reduces the size of downstream storm water BMPs.
- Takes water away from buildings.
- Increases infiltration potential.
- Increases time of concentration.
- Can be used as a retrofit BMP.

LIMITATIONS

- Adjacent buildings need to be considered in design.
- Ultimate storm water collection needs to be considered in design.
- May not be appropriate on all sites due to space constraints.

KEY DESIGN FEATURES

- Rain water must be directed away from foundations and footings.
- Downspouts should not be directed to paved areas or across sidewalks.
- 4" diameter SDR-35 pipe required as a minimum.



FACT SHEET- IMPERVIOUS AREA DISCONNECTION

Runoff Reduction Measure

- Distance and location of emitter relative to the building must be approved by a Licensed Geotechnical Engineer.
- Landscaped areas receiving roof water should be adequately sized to prevent runoff or erosion and allow for infiltration.
- Landscaped areas receiving roof water need to be designed to ensure proper drainage and to prevent ponding water.
- May be installed with a bottomless emitter to allow for infiltration. Bottom of emitter should be placed over drain rock to prevent sedimentation of pipe.
- Emitter should be equipped with “pop up” cover to prevent mosquito breeding.
- All calculations shall be completed using the “Storm Water Calculator” available at www.srcity.org/stormwaterLID.



PAVED AREA DISCONNECTION-DESCRIPTION

Paved areas that can be graded so that they drain onto pervious area, such as landscape or natural area can increase the opportunity for infiltration and minimize the size of downstream treatment.

ADVANTAGES

- Reduces the size of downstream storm water BMPs.
- Increases infiltration potential.
- Increases time of concentration.

LIMITATIONS

- Areas receiving flow need to be adequately sized and stabilized.
- Ultimate storm water collection needs to be considered in design.
- May not be appropriate on all sites due to space constraints.
- May be limited by site slopes.

KEY DESIGN FEATURES

- Rain water must be directed away from foundations and footings.
- Downspouts should not be directed to paved areas or across sidewalks.
- Landscaped areas receiving roof water should be adequately sized to prevent runoff or erosion and to allow for infiltration.
- All calculations shall be completed using the “Storm Water Calculator” available at www.srcity.org/stormwaterLID.

FACT SHEET- POROUS PAVEMENT

POROUS PAVEMENT

Also know as: Unit Pavers, Porous Concrete, and Pervious Pavement



DESCRIPTION

Porous Pavement is a system comprised of load-bearing, durable surface together with an underlying layered structure that temporarily stores water prior to infiltration or drainage to a controlled outlet. The surface can be porous such that water infiltrates across the entire surface of the material (e.g., crushed aggregate, porous concrete and porous asphalt), or it can be constructed of impermeable blocks separated by spaces and joints, through which the water can drain. This latter system is termed ‘permeable’ paving. Pervious paving is used for light vehicle loading in parking areas. For a surface to be considered porous it must allow water to infiltrate in to the soil below. Perforated pipe may be installed high in the section and the volume below the perforated pipe may be counted toward volume capture if no impermeable liner is installed. Sections with perforated pipe or liners at the bottom provide treatment only.

ADVANTAGES

- Significant flow attenuation and improvement in water quality.
- Can remove both the soluble and fine particulate pollutants.
- Roof runoff can be piped into the subsurface storage area directly, which would increase the level of flow attenuation.
- Within lined systems, there is the opportunity for stored runoff to be piped out for reuse.
- Pervious pavements have a high level of applicability because they are unobtrusive.

FACT SHEET- POROUS PAVEMENT

LIMITATIONS

- Can become clogged if improperly installed or maintained. However, this problem is minimized by the ease with which small areas of paving can be cleaned or replaced when blocked or damaged.
- Use should be limited to car parking areas and other lightly trafficked or nontrafficked areas. Pervious surfaces are currently not considered suitable for roadways within the public right-of-way.
- Prohibited in areas of known contamination. If soil and/or groundwater contamination is present on the site or within a 100' radius of the proposed location, the North Coast Regional Board review and approval is required.
- Do not use in areas of slope instability where infiltrated storm water may cause failure. Slope stability shall be determined by a licensed Geotechnical Engineer.
- Do not use in locations that can negatively impact building foundation or footings. Location shall be approved by a licensed Geotechnical Engineer.

KEY DESIGN FEATURES

- The subgrade should be able to sustain traffic loading without excessive deformation.
- The granular capping and sub-base layers should give sufficient load-bearing to provide an adequate construction platform and base for the overlying pavement layers.
- Pervious pavements require a single size grading to create voids for infiltration. The choice of materials is therefore a compromise between stiffness, permeability and storage capacity.
- Because the sub-base and capping will be in contact with water for extended periods, the strength and durability of the aggregate particles when saturated and subjected to wetting and drying should be assessed.
- Pervious concrete shall be designed and installed as described by the current version of CalTrans "Pervious Pavement Design Guidance."
- Must be installed under the supervision of a Certified Craftsman level contractor.



SIZING DESIGN- GOAL AND REQUIREMENTS

- **For all projects:** The treatment component requires that all of the runoff generated by this water quality design storm from impermeable surfaces must be treated on site for the pollutants of concern.
- **For projects that increase the amount of impervious surface, but create or replace less than a total of one acre:** The **Delta Volume Capture** component requires that any increase in volume due to development for the water quality design storm must be

FACT SHEET- POROUS PAVEMENT

infiltrated and/or reused on site. Further discussion of the Treatment and Delta Volume Capture requirements and the accompanying formulas can be found in Chapter 6.

- **For projects that create or replace one acre or more of impervious surface:** These larger projects must mitigate their impacts by meeting the **Hydromodification Requirement** by capturing 100% of the post development volume generated by the water quality rain event.
- All calculations shall be completed using the “Storm Water Calculator” available at www.srcity.org/stormwaterLID.

INSPECTION AND MAINTENANCE REQUIREMENTS

A maintenance plan shall be provided with the Final SW LID Submittal. The maintenance plan shall include recommended maintenance practices, state the parties responsible for maintenance and upkeep, specify the funding source for ongoing maintenance with provisions for full replacement when necessary and provide site specific inspection checklist.

At a minimum inspection and maintenance shall include the following:

- Keep landscaped areas well maintained
- Prevent soil from washing onto pavement

Pervious Pavement shall be inspected and maintained 2-3 times per year:

a) Vacuum clean surface using commercial sweeping machines at the following times:

- End of winter (April)
- Mid-summer (July / August)
- After autumn leaf-fall (November)
- Inspect outlets annually

b) As needed maintenance:

- If routine cleaning does not restore infiltration rates, then reconstruction of part of the pervious surface may be required.
- The surface area affected by hydraulic failure should be lifted for inspection of the internal materials to identify the location and extent of the blockage.
- Surface materials should be lifted and replaced after brush cleaning. Geotextiles, if used, may need complete replacement.
- Sub-surface layers may need cleaning and replacing.
- Due to the accumulation of pollutants, removed silts may need to be disposed of as controlled waste.

VEGETATED SWALE

Also know as: Bioretention Swale, Treatment Swale, and Grassy Swale



DESCRIPTION

The swale best management practice (BMP) functions as a soil and plant-based filtration and infiltration feature that removes pollutants through a variety of natural physical, biological, and chemical treatment processes. Vegetated swales are open, shallow channels with vegetation covering the side slopes and bottom that collect and slowly convey runoff flow to downstream discharge points. They are designed to treat runoff through filtering by the vegetation in the channel, filtering through a subsoil matrix, and/or infiltration into the underlying soils. They trap particulate pollutants (suspended solids and trace metals), promote infiltration, and reduce the flow velocity of storm water runoff. Vegetated swales can serve as part of a storm water drainage system and can replace curbs, gutters and storm sewer systems.

ADVANTAGES

- Can be designed to achieve Treatment, Delta Volume Capture, or Hydromodification requirements.

- Enhances water quality of downstream water bodies through natural processes.
- Aesthetically pleasing.
- The vegetation reduces heat island effects and improves an area's landscape.
- Vegetated swales can be designed to convey high flow as well as water quality flow.

LIMITATIONS

- A thick vegetative cover is needed for these practices to function properly.
- Swales are more susceptible to failure if not properly maintained than other treatment BMPs.
- Can be difficult to avoid channelization, which may cause erosion and limit infiltration potential.
- Not effective and may even erode when flow velocities are high, if the grass cover is not properly maintained.
- May not be appropriate for industrial sites or locations where spills may occur.
- Grassed swales cannot treat a very large drainage area. Large areas may be divided and treated using multiple swales.
- Should not be used in areas of known contamination. If soil and/or groundwater contamination is present on the site or within a 100' radius of the proposed BMP location, the North Coast Regional Water Quality Control Board will need to be contacted and the site reviewed.
- Should not be used in areas of slope instability where infiltrated storm water may cause failure. Slope stability should be determined by a licensed geotechnical engineer.
- Do not use in locations that can negatively impact building foundation or footings. Location shall be approved by a licensed Geotechnical Engineer.

KEY DESIGN FEATURES

- The longest flow path for the swale shall have a minimum retention time of 12 minutes for conditions when the treatment flows enter the Vegetated Swale uniformly along the swale length. The longest flow path for the swale shall have a minimum retention time of 8 minutes if 90 percent or more of the treatment flow enters the swale at the upstream end.
- Swale should be designed so that the water level does not exceed 2/3rds the height of the grass or 4 inches, whichever is less, at the design treatment rate.
- Longitudinal slopes between 1% and 2.5% are recommended.
- Maximum allowable slope is 8% slope. In steep areas, check dams up to 24-inches high and at least 25 feet apart are allowed.
- Trapezoidal channels are normally recommended but other configurations, such as parabolic, can also provide substantial water quality improvement and may be easier to mow than designs with sharp breaks in slope.

- Swales constructed in cut are preferred, or in fill areas that are far enough from an adjacent slope to minimize the potential for gopher damage. Do not use side slopes constructed of fill, which are prone to structural damage by gophers and other burrowing animals.
- Shall be planted with plants from the approved **Plant List** and **Tree List** included in Appendix F and shall be planted to achieve 51% cover.
- Vegetated swales shall have a maximum treatment width of 10 feet. The vegetated swale bed shall be at least 2-feet wide and no more than 7-feet wide. Parallel swales may be used if calculations show greater width is needed.
- The bed of the swale flow area shall slope at about 2% from toe of side slope to center of swale. Side slopes shall be no greater than a 2 to 1 slope.
- If vegetation is not established prior to rain, additional soil stabilization methods may be necessary.
- If the 10 or 100-year storm event flow velocity is greater than 4 feet per second, a permanent geofabric liner shall be used that is rated for the calculated flow velocity.
- If used, the perforated pipe trench shall be backfilled with $\frac{3}{4}$ " crushed rock with a 2-inch bed underneath and 6-inch cover.



SIZING DESIGN- GOAL AND REQUIREMENTS

- **For all projects:** The treatment component requires that all of the runoff generated by this water quality design storm from impermeable surfaces must be treated on site for the pollutants of concern.
- ***For projects that increase the amount of impervious surface, but create or replace less than a total of one acre:*** The **Delta Volume Capture** component requires that any increase in volume due to development for the water quality design storm must be infiltrated and/or reused on site. Further discussion of the Treatment and Delta Volume Capture requirements and the accompanying formulas can be found in Chapter 6.
- ***For projects that create or replace one acre or more of impervious surface:*** These larger projects must mitigate their impacts by meeting the **Hydromodification Requirement** by capturing 100% of the post development volume generated by the water quality rain event.
- All calculations shall be completed using the “Storm Water Calculator” available at www.srcity.org/stormwaterLID.

INSPECTION AND MAINTENANCE REQUIREMENTS

A maintenance plan shall be provided with the Final SWLID Submittal. The maintenance plan shall include recommended maintenance practices, state the parties responsible for maintenance and upkeep, specify the funding source for ongoing maintenance with provisions for full replacement when necessary and provide site specific inspection checklist.

At a minimum maintenance shall include the following:

- Mow and irrigate during dry weather to the extent necessary to keep vegetation alive. Where 6-inch high grasses are used, the grass height shall be at least 3 inches after mowing. Where mowed grasses are shown, the grass height shall be mowed when the height exceeds 3 inches.
- Remove obstructions and trash from vegetated swale.
- Pesticides and fertilizers shall not be used in the swale.

Vegetated Swales shall be inspected and maintained monthly during the rainy season to review:

- Obstructions and trash.
- Pondered flow is drained within 72 hours after a rainfall event.
- Condition of grasses.
- If ponding is observed, grading will be required to restore positive drainage.

FACT SHEET- BIORETENTION

BIORETENTION

Also known as: Rain garden, roadside bioretention, and bioretention cell



DESCRIPTION

The bioretention area best management practice (BMP) functions as a soil and plant-based filtration and infiltration feature that removes pollutants through a variety of natural physical, biological, and chemical treatment processes.

ADVANTAGES

- Can be designed to achieve Treatment, Delta Volume Capture, or Hydromodification requirements.
- Enhances water quality of downstream water bodies through natural processes.
- Aesthetically pleasing.
- The vegetation can provide shade and wind breaks, absorbs noise, reduces heat island effects and improves an area's landscape.
- Provides habitat for birds and attracts other pollinators like butterflies and bees.
- Does not interrupt utility installation.
- Does not interfere with tree planting.

FACT SHEET- BIORETENTION

LIMITATIONS

- Specialized design is required for areas where street slopes exceed 10%.
- Should not be used in areas of know contamination. If soil and/or groundwater contamination is present on the site or within a 100' radius of the proposed BMP location, the North Coast Regional Water Quality Control Board will need to be contacted and the site reviewed.
- Should not be used in areas of high groundwater. In general a minimum of 2' of clearance should be provided between the bottom of the bioretention cell and seasonal high groundwater.
- Should not be used in areas of slope instability where infiltrated storm water may cause failure. Slope stability should be determined by a licensed geotechnical engineer.
- Do not use in locations that can negatively impact building foundation or footings. Location shall be approved by a licensed Geotechnical Engineer.

KEY DESIGN FEATURES

ALL BIORETENTION

- Structural soil should be used within the bioretention area requiring load bearing capacity (adjacent to roadways and/or buildings).
- Structural soil, if used, shall be installed as described in Appendix E.
- Some BMPs may not require the use of structural soil and a more organic type planting soil and/or treatment media may be used in its place. It may be possible in some cases to use native soil or to amend the native soil so that it is suitable. Use of non-structural soil will depend on evaluation of the criteria in "Chapter 4-Site Assessment" as well as consideration of structural needs and may require evaluation by a licensed Geotechnical Engineer.
- Underlining native soil should remain un-compacted to preserve infiltration capacity. Fence off the area during construction to protect it from compaction.
- Bottom of bioretention should be un-lined to allow infiltration into native soil.
- Moisture barrier must be installed vertically to protect road sub-base and any trenches adjacent to the bioretention area.
- If used, pervious concrete shall be designed and installed as described in Appendix E and protected during construction to prevent sediment loading.
- If the porous gutter design option is used additional trash and sediment capture BMPs is required.
- A curb opening type design may be used in place of a porous gutter if appropriate for the project and does not require additional trash capture.
- Bioretention areas shall be planted with plants from the approved **Plant List** and **Tree List** included in Appendix F and shall be planted to achieve 51% cover.
- All bioretention areas shall be designed with a designated high flow bypass inlet for storms larger than the design storm.

FACT SHEET- BIORETENTION

- For designs that include perforated pipe, the 6" perforated pipe must be installed a minimum of 6" below the adjacent road structural section.
- Perforated pipe shall be installed in straight runs only.
- The volume below the perforated pipe must be sufficient to hold and infiltrate the design volume.

SIZING DESIGN- GOAL AND REQUIREMENTS

- **For all projects:** The treatment component requires that all of the runoff generated by this water quality design storm from impermeable surfaces must be treated on site for the pollutants of concern.
- **For projects that increase the amount of impervious surface, but create or replace less than a total of one acre:** The **Delta Volume Capture** component requires that any increase in volume due to development for the water quality design storm must be infiltrated and/or reused on site. Further discussion of the Treatment and Delta Volume Capture requirements and the accompanying formulas can be found in Chapter 6.
- **For projects that create or replace one acre or more of impervious surface:** These larger projects must mitigate their impacts by meeting the **Hydromodification Requirement** by capturing 100% of the post development volume generated by the water quality rain event.
- All calculations shall be completed using the "Storm Water Calculator" available at www.srcity.org/stormwaterLID.

INSPECTION AND MAINTENANCE REQUIREMENTS

A maintenance plan shall be provided with the Final SWLID Submittal. The maintenance plan shall include recommended maintenance practices, state the parties responsible for maintenance and upkeep, specify the funding source for ongoing maintenance with provisions for full replacement when necessary and provide site specific inspection checklist.

At a minimum maintenance shall include the following:

- Dry street sweeping upon completion of construction
- Dry street sweeping annually, and
 - When water is observed flowing in the gutter during a low intensity storm.
 - Algae is observed in the gutter.
 - Sediment/debris covers 1/3 of the gutter width or more.
- Inspect twice annually for sedimentation and trash accumulation in the gutter. Obstructions and trash shall be removed and properly disposed of.
- Inspect twice during the rainy season for ponded water.
- Pesticides and fertilizers shall not be used in the bioretention area.
- Plants should be pruned, weeds pulled and dead plants replaced as needed.

Soil Map—Sonoma County, California



Soil Map may not be valid at this scale.

Map Scale: 1:763 if printed on A portrait (8.5" x 11") sheet.



Map projection: Web Mercator Corner coordinates: WGS84 Edge tics: UTM Zone 10N WGS84



Natural Resources
Conservation Service

Web Soil Survey
National Cooperative Soil Survey

5/9/2018
Page 1 of 3

MAP LEGEND

- Area of Interest (AOI)**
-  Area of Interest (AOI)
- Soils**
-  Soil Map Unit Polygons
-  Soil Map Unit Lines
-  Soil Map Unit Points
- Special Point Features**
-  Blowout
-  Borrow Pit
-  Clay Spot
-  Closed Depression
-  Gravel Pit
-  Gravelly Spot
-  Landfill
-  Lava Flow
-  Marsh or swamp
-  Mine or Quarry
-  Miscellaneous Water
-  Perennial Water
-  Rock Outcrop
-  Saline Spot
-  Sandy Spot
-  Severely Eroded Spot
-  Sinkhole
-  Slide or Slip
-  Sodic Spot
-  Spoil Area
-  Stony Spot
-  Very Stony Spot
-  Wet Spot
-  Other
-  Special Line Features
- Water Features**
-  Streams and Canals
- Transportation**
-  Rails
-  Interstate Highways
-  US Routes
-  Major Roads
-  Local Roads
- Background**
-  Aerial Photography

MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:20,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service
 Web Soil Survey URL:
 Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Sonoma County, California
 Survey Area Data: Version 11, Sep 21, 2017

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Dec 31, 2009—Nov 22, 2016

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
BcA	Blucher fine sandy loam, overwash, 0 to 2 percent slopes	2.1	100.0%
Totals for Area of Interest		2.1	100.0%



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Area of Interest (AOI) Soil Map

Search

Map Unit Legend

Sonoma County, California (CA097)

Sonoma County, California (CA097)

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
BcA	Blucher fine sandy loam, overwash, 0 to 2 percent slopes	2.1	100.0
Totals for Area of Interest		2.1	100.0

FOIA | [Accessibility Statement](#) | [Privacy Policy](#)

Map Unit Description [Printable Version](#)

Report — Map Unit Description

Sonoma County, California

BcA—Blucher fine sandy loam, overwash, 0 to 2 percent slopes

Map Unit Setting

National map unit symbol: hfb4
 Elevation: 0 to 500 feet
 Mean annual precipitation: 25 to 50 inches
 Mean annual air temperature: 57 to 61 degrees F
 Frost-free period: 250 to 270 days
 Farmland classification: Prime farmland if irrigated and either protected from flooding or not frequently flooded during the growing season

Map Unit Composition

Blucher and similar soils: 85 percent
 Minor components: 15 percent

Estimates are based on observations, descriptions, and transects of the mapunit.

Description of Blucher

Setting

Landform: Alluvial fans, drainageways
 Landform position (two-dimensional): Footslope
 Landform position (three-dimensional): Side slope, tread
 Down-slope shape: Linear
 Across-slope shape: Linear
 Parent material: Alluvium derived from sedimentary rock

Typical profile

H1 - 0 to 20 inches: fine sandy loam
 H2 - 20 to 34 inches: fine sandy loam
 H3 - 34 to 60 inches: clay loam

Properties and qualities

Slope: 0 to 2 percent
 Depth to restrictive feature: More than 80 inches
 Natural drainage class: Somewhat poorly drained
 Runoff class: Medium
 Capacity of the most limiting layer to transmit water (Ksat): Moderately low to moderately high (0.06 to 0.20 in/hr)
 Depth to water table: About 42 to 60 inches
 Frequency of flooding: Occasional
 Frequency of ponding: Occasional
 Salinity, maximum in profile: Nonsaline to very slightly saline (0.0 to 2.0 mmhos/cm)
 Available water storage in profile: High (about 9.7 inches)

Interpretive groups

Land capability classification (irrigated): 2w
 Land capability classification (nonirrigated): 3w
 Hydrologic Soil Group: C
 Hydric soil rating: Yes

Minor Components

Pajaro

Percent of map unit: 8 percent
 Hydric soil rating: No

Steinbeck

Percent of map unit: 7 percent
 Hydric soil rating: No

Description — Map Unit Description

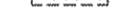
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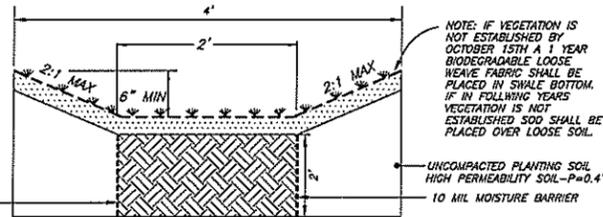


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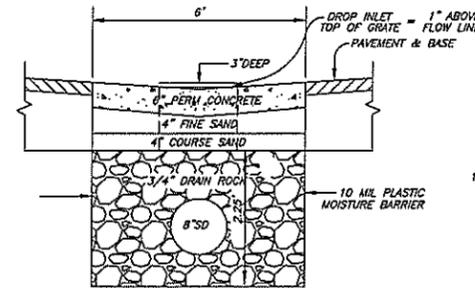
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LEGEND

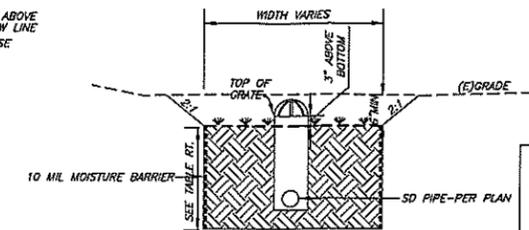
-  DRAINAGE MANAGEMENT AREA (DMA)
-  BIORETENTION AREA
-  PERMEABLE PAVEMENT
-  BRA BIORETENTION
-  BRS VEGETATED SWALE WITH BIORETENTION



VEGETATED SWALE W/BIO-RETENTION
NO SCALE



VALLEY GUTTER PERMEABLE CONCRETE DETAIL
NO SCALE



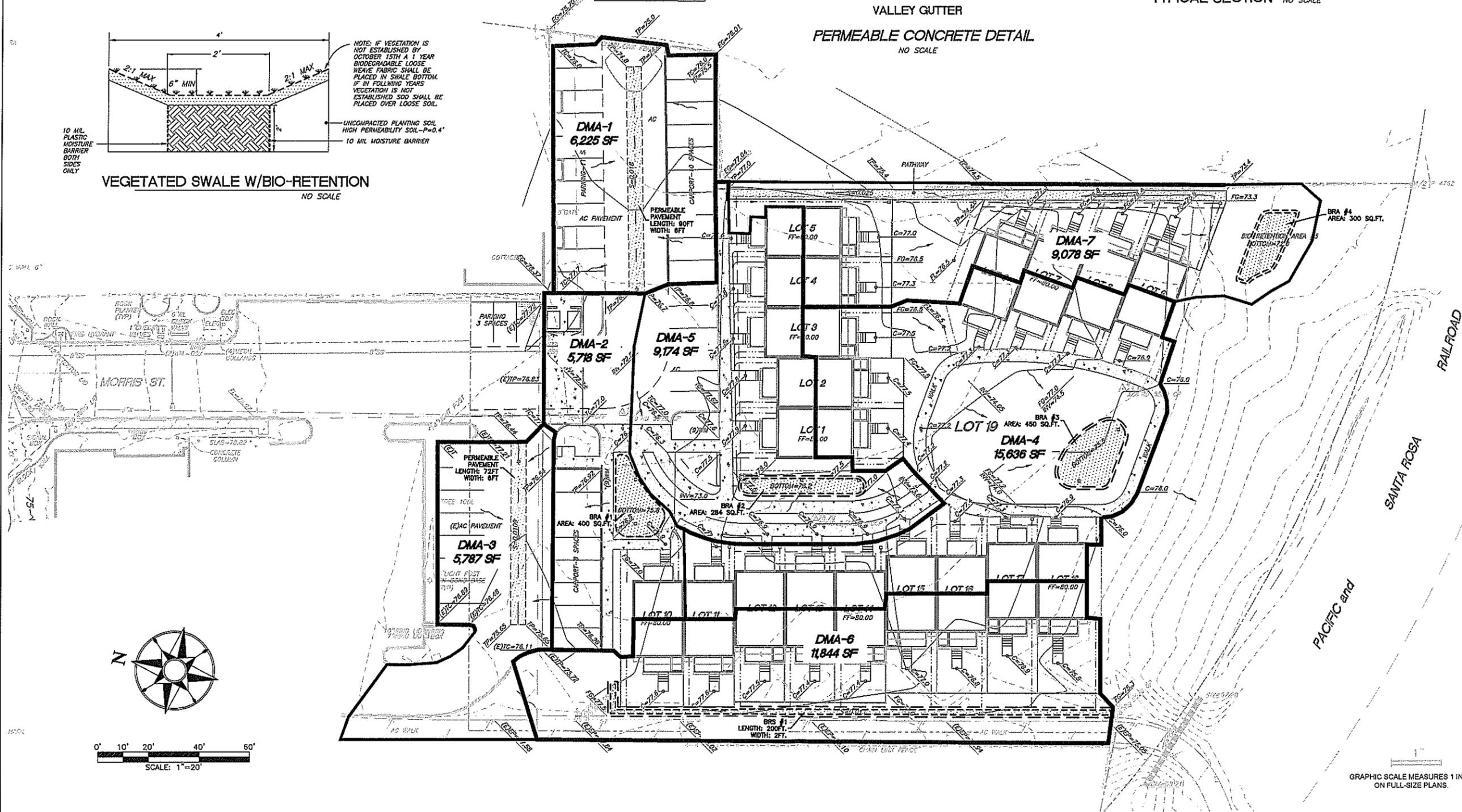
SOIL DEPTH TABLE

BMP ID.	SOIL DEPTH	SF
BRA#1	2.0'	450
BRA#2	2.0'	284
BRA#3	1.5'	450
BRA#4	2.0'	300

BIO-RETENTION AREAS SHALL BE CONSTRUCTED UNDER THE DIRECTION OF THE PROJECT ENGINEER AND SOILS ENGINEER

BIO-RETENTION AREA TYPICAL SECTION
NO SCALE

PAVEMENT TO MATCH (E)GRADE TO EAST. FENCE TO BE REMOVED TO PROVIDE ACCESS & EGRESS TO APN 004-083-017



SUSMP

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1-800-515-5054 www.lacoassociates.com

NO.	HISTORY / REVISION	BY	CHK.	DATE

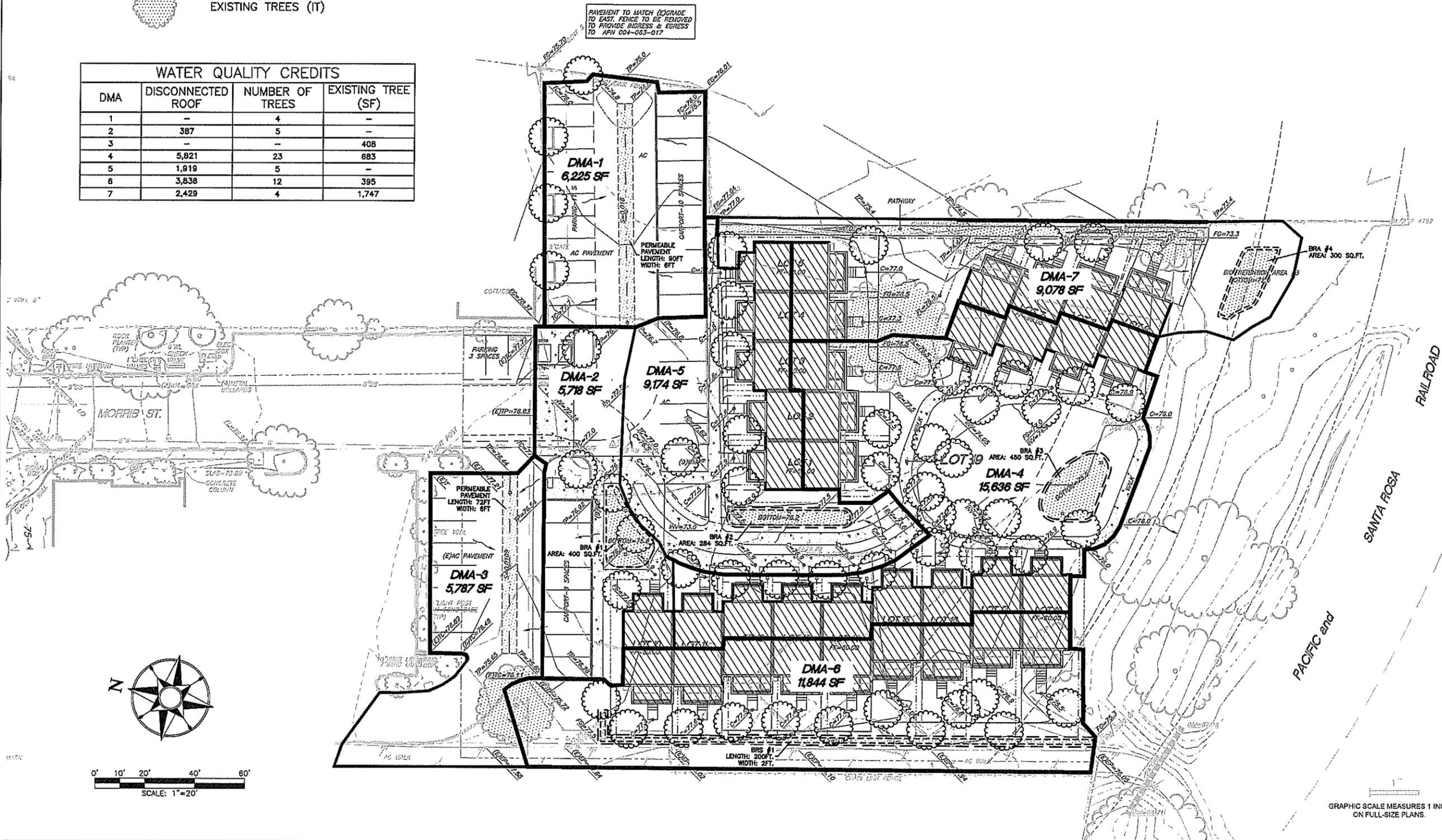
DAVIS TOWNHOMES
SUSMP
DANIEL DAVIS
1051 TODD ROAD, SANTA ROSA, CA. 95407

DRAWN	GG
CHECK	KD
APPROVED	X
DATE	07-19-2018
JOB NUMBER	9054.00
CHEET	DRAWING
WQ-1	WQ-2

LEGEND

-  DISCONNECTED ROOF (DR)
-  PROPOSED TREES (IT)
-  EXISTING TREES (IT)

WATER QUALITY CREDITS			
DMA	DISCONNECTED ROOF	NUMBER OF TREES	EXISTING TREE (SF)
1	-	4	-
2	387	5	-
3	-	-	408
4	5,821	23	683
5	1,919	5	-
6	3,838	12	395
7	2,429	4	1,747



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NO.	HISTORY / REVISION	BY	CHK.	DATE

DAVIS TOWNHOMES
WATER QUALITY CREDITS

DANIEL DAVIS
1051 TODD ROAD, SANTA ROSA, CA. 95407

DRAWN	GG
CHECK	KD
APPROVED	X
DATE	07-19-2018
JOB NUMBER	9054.00
SHEET	DRAWING
WQ-2	WQ-2

MITIGATED NEGATIVE DECLARATION / INITIAL STUDY

Davis Townhomes
6737 Sebastopol Avenue

CITY OF SEBASTOPOL
PLANNING DEPARTMENT
CITY HALL
7120 BODEGA AVENUE
SEBASTOPOL, CALIFORNIA 95472

March 7, 2019

I. Environmental Checklist Forms - Mitigated Negative Declaration

1. Project Title	Davis Townhomes
2. Lead Agency Name and Address	City of Sebastopol – Planning Department, 7120 Bodega Avenue, Sebastopol, CA 95472
3. Contact Person and Phone Number	Dana Morrison, Assistant Planner. (707) 823-6167
4. Project Location	6737 Sebastopol Avenue [aka SR12], Sebastopol Ca 95472 (APN 004-063-036)
5. Project Sponsor's Name and Address	Dan Davis – 1051 Todd Road, Santa Rosa Ca 95407
6. General Plan Designation	Central Core
7. Zoning	Downtown Core
8. Description of Project	<p>The Project proposes development of eighteen (18); attached, 1,180 square foot, 2-bedroom, 1.5 bathroom, 2-story townhomes on a 1.74 acre lot. The proposed town homes are clustered around a central open space, in three clusters. Lot sizes vary from ~1250 to ~1800 square feet. The town homes are proposed to have 6-foot-deep front and rear porches (which will have a storage closet). In addition, there will be private rear yards of a minimum 20 feet deep by 20 feet wide with rear gates.</p> <p>Project will include 18 carport spaces along with 18 assigned surface parking spaces and two (2) visitor parking spaces, for a total of 38 parking space. The proposed development also includes gated driveway access to Park Village (to be used in case of emergency), fire and emergency vehicle access, storm water retention areas, and new landscaping including the addition of 56 new trees, trash and recycling enclosure, and one ganged mail box. Four (4) of the parking spaces are proposed to be electric car charging stations.</p>
9. Surrounding Land Uses and Setting	The location of the subject property is south of the end of Morris Street, behind and to the south of commercial properties that face onto Sebastopol Avenue [aka SR12] and an existing parking lot that contains approximately 32 parking spaces. A multipurpose path and the Sebastopol Inn are located to the west. The Railroad Forest is located to the south. A residential property, Park Village, and Tomodachi Park are located to the east.
10. Other public agencies whose approval is required (Permits, financing approval, or participation agreement.)	No outside public agency approval is required for the proposed Project.
11. Have California Native American tribes traditionally	Yes, a referral letter and attachments was sent to Tribal Heritage Preservation Officer for the Federated Indians of

<p>and culturally affiliated with the project area requested consultation pursuant to Public Resources Code section 21080.3.1? If so, has consultation begun?</p>	<p>Graton Rancheria on 11.14.2018. Additional information regarding the Project was requested. These additional requested materials were sent on 1.23.2019; no response was received. A follow up email was sent on 2.19.2019; no response has been received as of compiling this study.</p>
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ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated on the checklist that follows:

<input checked="" type="checkbox"/>	Aesthetics	<input type="checkbox"/>	Agriculture and Forestry	<input checked="" type="checkbox"/>	Air Quality
<input checked="" type="checkbox"/>	Biological Resources	<input checked="" type="checkbox"/>	Cultural Resources	<input type="checkbox"/>	Geology/Soils
<input type="checkbox"/>	Greenhouse Gas Emissions	<input type="checkbox"/>	Hazards and Hazardous Materials	<input checked="" type="checkbox"/>	Hydrology/Water Quality
<input type="checkbox"/>	Land Use/Planning	<input type="checkbox"/>	Mineral Resources	<input type="checkbox"/>	Noise
<input type="checkbox"/>	Population/Housing	<input type="checkbox"/>	Public Services	<input type="checkbox"/>	Recreation
<input checked="" type="checkbox"/>	Transportation/Traffic	<input type="checkbox"/>	Tribal Cultural Resources	<input type="checkbox"/>	Utilities/Service Systems
<input checked="" type="checkbox"/>	Mandatory Findings of Significance				

DETERMINATION: On the basis of this initial evaluation:

I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.

I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.

I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.

I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.

I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.



Kari Svanstrom, CEQA Coordinator

3/7/2019

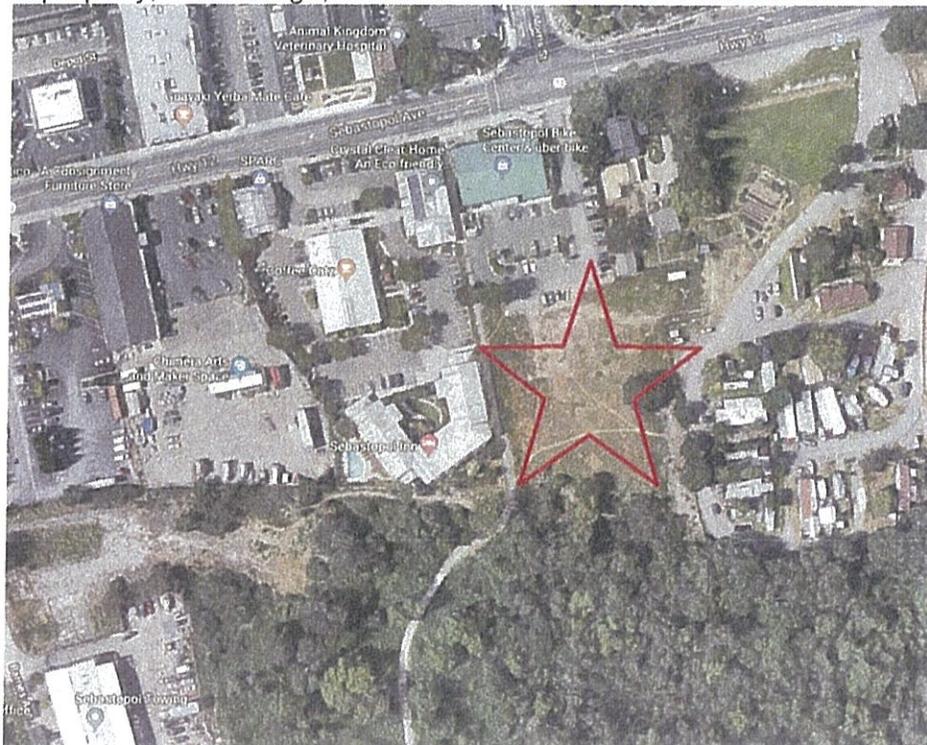
II. Project Description

The Project proposes development of eighteen (18); attached, 1,180 square foot, 2-bedroom, 1.5 bathroom, 2-story townhomes. The proposed town homes are clustered around a central open space, in three clusters. Lot sizes vary from ~1250 to ~1800 square feet. The town homes are proposed to have 6-foot-deep front and rear porches (which will have a storage closet). In addition, there will be private rear yards of a minimum 20 feet deep by 20 feet wide with rear gates.

Project will include 18 carport spaces along with 18 assigned surface parking spaces and two (2) visitor parking spaces, for a total of 38 parking space. The proposed development also includes gated driveway access to Park Village (to be used in case of emergency), fire and emergency vehicle access, storm water retention areas, and new landscaping including the addition of 56 new trees, trash and recycling enclosure, and one ganged mail box. Four (4) of the parking spaces are proposed to be electric car charging stations.

Further information regarding lighting, materials and colors are detailed in the project description submitted by the applicant. The style of the homes are proposed to be farmhouse or late craftsman to compliment much of the architecture found throughout Sebastopol. Roof pitches and front and rear porch roofs will be varied; and all porches will have decorative railings which will be painted to coordinate with the individual color trim of the home.

The location of the subject property is south of the end of Morris Street, behind and to the south of commercial properties that face onto Sebastopol Avenue and an existing parking lot that contains approximately 32 parking spaces. A bike path and the Sebastopol Inn are located to the west. The Railroad Forest is located to the south. A residential property, Park Village, and Tomodachi Park are located to the east.



III. Evaluation of Potential Environmental Impacts

I. **AESTHETICS:** Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Substantially degrade the existing visual character or quality of the site and its surroundings?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Create a new source of substantial light or glare, which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

Item a: Less Than Significant with Mitigation Incorporated. A scenic vista is a public view of a valued visual resource. Scenic vistas generally include public views that provide visual access to large panoramic views of natural features, unusual terrain, or unique urban or historic features, for which the field of view can be wide and extend into the distance, and focal views that focus on a particular object, scene, or feature of interest.

As describe above, in the Project Description, the Project Site is undeveloped and relatively flat. The Site is surrounded by existing residential and commercial development to the north, east and west. Visual resources are primarily limited to those located adjacent to the Project Site due to the existing developments in the surrounding areas. To the south of the Project Site is the Railroad Forest property, which is owned by the City and designated as Wetlands/Scenic Open Space. An existing bike connector path is located just to the west of the Project Site, views currently exist (from this path) of the Railroad Forest property. The Project includes a 50' setback from the Railroad Forest and required restoration of vegetation within this setback. While the proposed development may partially block the view from the bike path, the mitigation requiring restoration and further plantings in the 50' abutting the Railroad Forest property will likely increase the scenic value along this section of the path.

The Project Site is not viewable from any panoramic vistas located nearby. The increase in development in the area the Project Site would be difficult to discern within the greater fabric of the surrounding development. The two-story Project would not interfere with the skyline and horizon line that are available from the intersection of Highway 12 and Morris Street. Furthermore, the Project will be subject to a number of Conditions of Approval regarding height, color selection and screening which will reduce potential impacts on any scenic vistas.

Construction activities generally cause a temporary contrast to, and disruption in, the general order and aesthetic character of an area. Although temporary in nature, construction activities may create a visually unappealing look on the Project Site. During construction activities for the Project, the visual appearance of the site would be altered due to the presence of construction equipment and activities. Some of the activity would be visible from the roadway (Highway 12) located to the north of the Project Site, as well as pedestrians and bikers along the bike path located to the west; as well as from viewer from the Hotel nearby. However, temporary construction fencing would be placed along the periphery of the Project Site to screen much of the construction activity from view at the street level.

Overall, while affecting the visual character of the Project area on a short-term basis, Project construction activities would not substantially alter or degrade the existing visual character or quality of the Project Site and surrounding area, for the following reasons: 1) views of construction would be limited in duration and locations; 2) the Project Site appearance would be typical of construction sites in urban areas; 3) construction fencing would be placed along the periphery of the Project Site to screen much of the construction from view at street and bike path level.

Mitigation:

- AES-1: Building height shall be limited to 2 stories.
- AES-2: Exterior colors shall be selected which blend with the natural surroundings. Final colors shall be approved by the Design Review Board.
- AES-3: Plantings shall be included along the eastern property line, in addition to the proposed 6' fence, to provide additional screening.
- AES-4: Construction fencing shall be placed along the periphery of the Project Site on the north, east and west property lines to screen construction activity from view. The southern construction fencing shall run along the required 50 setback buffer from the Railroad Forest property and not from the property line.

Item b: No Impact. The Project Site is not located along a state scenic highway. The nearest officially designated scenic highway (Highway 116) is ~1/5 of mile west of the Project Site, and no City-designated scenic roads are near the Project Site. Views of the Project Site are not available from Highway 116, view from Highway 12 are substantially obscured by existing commercial and residential developments. Therefore, the Project would not substantially damage scenic resources, including those located within a state or City-designated scenic highway. Only one tree on-site is proposed for removal and the Project proposes the addition of 56+ trees (not including required plantings in the 50' setback from the Railroad). As such, the removal of this tree during construction would not substantially alter or degrade the existing visual character of the Project Area. Overall, the Project would not result in an impact to scenic resources within a scenic highway.

Item c: Less Than Significant Impact. The proposed Project will introduce development in residential form on a site that is currently vacant but was historically developed as a cherry processing/packaging plant. This land use transition is in accordance with the City's General Plan and will be conformance with the Zoning Code upon adoption of the Planned Community rezone. The Project will be subject to Conditions of Approval regarding height, color selection and screening which will reduce potential impacts on any scenic vistas. Furthermore, consistency with the General Plan and compliance with the provisions of Zoning Ordinance are in place to guide future development in a manner that will result in less than significant impacts on the visual character of the area.

Item d: Less Than Significant Impact. The proposed Project will include exterior lighting, which will be reviewed by the Design Review Board to ensure that there is no substantial increase in light levels on adjacent properties and to minimize overspill and impacts on the night sky. No substantial light or glare will result. There will be less than significant impacts regarding light or glare.

II. AGRICULTURE AND FOREST RESOURCES: In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to nonagricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Involve other changes in the existing environment, which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

Item a: No Impact. The Project Site is located at the edge of an urbanized area (the City of Sebastopol). As discussed in the project description the Project Site is surrounded on three sides by existing commercial and residential development. No agricultural uses or operations occur on the site or in the vicinity of the Project Site. The Project Site and surrounding area are also not mapped as Prime Farmland, Unique Farmland, or Farmland of Statewide Importance pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency Department of Conservation. As such, the Project would not convert farmland to a non-agricultural use. No impacts would occur, and no mitigation measures are required.

Item b: No Impact. The Project Site is zoned by the City of Sebastopol as Downtown Core with an ESOS (Environmental and Scenic Open Space) district overlay. The Project Site is not zoned for agricultural use. Furthermore, none of the surrounding properties are zoned for agricultural use. The Project Site and surrounding area are also not enrolled under a Williamson Act Contract. Therefore, the Project would not conflict with any zoning for agricultural uses or a Williamson Act Contract. No impacts would occur, and no mitigation measures are required.

Item c: No Impact. As previously discussed, the Project Site is located in an urbanized area surrounded by commercial and residential development to the north, east and west; the site was also previously developed as a cherry packing plant, until it burned down in 1995. The Project Site does not include any forest land or timberland but is adjacent to a property zoned as Primary Wetland District and contains a riparian forest. The Project Site is currently zoned for commercial uses and is not zoned and/or use as forest land. Therefore, the Project would not conflict with the existing zoning for, or cause rezoning of, forest land or timberland as defined by the Public Resources Code. No impacts would occur, and no mitigation measures are required.

Item d: No Impact. As previously discussed, the Project Site is located at the edge of an urbanized area, zoned as Downtown Core, and does not include any forest land or timberland. Therefore, the Project would not result in the loss or conversion of forest land to non-forest use. No impacts would occur, and no mitigation measures are required.

Item e: No impact. The Project Site is located at the edge of an urbanized area of the City of Sebastopol and does not include farmland. The Project Site and surrounding area are not mapped as farmland, are not zoned as farmland or agricultural use, and do not contain any agricultural uses. As such, the Project would not result in the conversion of farmland to non-agricultural use. No impacts would occur, and no mitigation measures are required.

III. AIR QUALITY: Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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a. Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Create objectionable odors affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

The Project is located in central/southern Sonoma County, where air quality is regulated by the Bay Area Air Quality Management District (BAAQMD). The region is non-attainment for the federal and state ozone standards, the state PM10 standards, and the federal and state PM2.5 standards. The region is in attainment or unclassified for all other ambient air quality standards. BAAQMD prepares air quality plans (AQPs) that include projected emissions inventories and account for emission reductions strategies in order to demonstrate how the region will achieve the ambient air quality standards by the given deadlines.

Item a: Less Than Significant Impact. The Project will not exceed thresholds of significance of the BAAQMD nor will it obstruct air quality plans. There will be no significant impacts.

Item b: Less Than Significant with Mitigation Incorporated. The Project will not violate any BAAQMD standard nor will it contribute substantially to an existing or projected air quality violation. The Project would result in increased air pollutant emissions from the Project Site during construction (short-term) and operation (long-term). Construction-related pollutants would be associated with sources such as construction worker vehicle trips, the operation of construction equipment, site grading and preparation activities, the application of architectural coatings. During Project operation, air pollutants would be minimal and would mainly be associated with pollutants emitted on a daily basis from motor vehicle travel.

Mitigation:

- AQ-1: Basic measures to control dust and exhaust shall be utilized during construction. During any construction period ground disturbance, the applicant shall ensure that the project contractor implement measures to control dust and exhaust. Implementation of the measures recommended by BAAQMD and listed below would reduce the air quality impacts associated with grading and new construction to a less than significant level. The contractor shall implement the following best management practices that are required of all projects:
 - All haul trucks transporting soil, sand, and other loose material off-site shall be covered.

- All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per week. The use of dry power sweeping is prohibited.
- All vehicle speeds on unpaved roads shall be limited to 15 miles per hour (mph).
- All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.
- Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure Title 13, Section 2485 of California Code of Regulations [CCR]). Clear signage shall be provided for construction workers at all access points.
- All construction equipment shall be maintained and properly tuned in accordance with manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation.
- Post a publicly visible sign with the telephone number and person to contact at the Lead Agency regarding dust complaints. This person shall respond and take corrective action within 48 hours. The Air District's phone number shall also be visible to ensure compliance with applicable regulations.

The Project will have a less than significant impact as it relates to community risk caused by constructions activities, after this mitigation is incorporated.

Item c: Less Than Significant Impact. Operational-period emissions for the Project would be less than significant due to its size and nature. The Project would not result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors).

Item d: Less Than Significant Impact. Sensitive receptors are groups of individuals, including children, the elderly, the acutely ill, and the chronically ill, that may be more susceptible to health risks due to chemical exposure, and sensitive-receptor population groups are likely to be located at hospitals, medical clinics, schools, playgrounds, childcare centers, residences, and retirement homes. There are no existing senior residential apartments, schools, day care centers, playgrounds, hospitals or medical clinics located within close proximity to the Project Site. One single-family home and the Park Village Mobile Home Park are located immediately to the north and northeast of the Project site. The proposed Project is a residential development, and there will not be any on-site Toxic Air Contaminant (TAC) emission sources during operation. Because most passenger vehicles are gasoline-combusted, the Project would not generate significant amount of Diesel Particulate Matter (DPM) emissions during operation. Therefore, the Project would not result in significant health impacts on sensitive receptors during operation.

Item e: Less Than Significant Impact. Land uses typically considered associated with odors include wastewater treatment facilities, waste- disposal facilities, or agricultural operations. The Project does not contain land uses typically associated with emitting objectionable odors. During operation of the Project, odors would primarily consist of vehicles traveling to and from the townhomes site. Diesel exhaust and volatile organic compounds would be emitted during construction of the Project, which are objectionable to some; however, emissions would disperse rapidly from the Project Site and therefore would not create objectionable odors affecting a substantial number of people. As such, construction odor impacts would be less

than significant. These occurrences would not produce significant odors; therefore, operational impacts would be less than significant.

IV. BIOLOGICAL RESOURCES: Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

Item a,b & d: Less Than Significant Impact with Mitigation Incorporated. The Project Site is unique as it is located on the edge of an urbanized area and is considered part of the City's

Downtown, but abuts an environmentally sensitive habitat, the Laguna de Santa Rosa. The Project site was previously developed and, while it does abut an environmentally sensitive habitat, it does not contain sensitive habitat. The site appears to be used by common domestic and wildlife species adapted to human disturbance, and has a low potential for presence of wildlife species of concern, such as special status or nesting birds, or special status bats. The site currently consists of a paved parking lot on the very northern edge, and then predominantly consists of gravel/sand with a mix of native and non-native grasses and thistles. The Railroad Forest property, which abuts the southern edge of the property is an environmentally sensitive habitat area.

City staff contacted Prunuske Chatham Inc., the environmental consulting firm that developed the Laguna Wetlands Preserve Restoration and Management Plan (adopted January 5, 2016) to understand better any concerns related to this site given its prior development and its proximity to the Railroad Forest site. While this parcel was not part of their Plan study, it was noted there were no delineated wetlands on the site at this time. The main concerns with development of this (or other) proximate sites were the cumulative impacts of development near the Laguna, including new impervious surfaces and water quality of runoff (as well as quantity) and the need to buffer the sensitive habitat located along the southern end of the proposed development.

Mitigation:

In terms of mitigations, a number of measures have been recommended to enhance the protection of sensitive biological habitat located in the neighboring Railroad Forest:

- BIO-1: A 50-foot environmental setback from the south property line shall be maintained in perpetuity as a buffer to the environmental resources of the adjacent Railroad Forest property (APN 004-072-014). An open space restriction for this area shall be included on the Final Map.
- BIO-2: Maintenance of this setback and buffer area shall include the following restrictions and requirements:
 - a. The applicant shall develop a plan wherein all Invasive plants shall be removed within this area prior to final inspection.
 - b. The buffer area shall be replanted with native species appropriate to provide a buffer to the Railroad Forest area prior to final inspection.
 - c. The removal and replanting plan shall be developed by, or reviewed and approved by, a qualified biologist and City Staff prior to issuance of a building permit.
 - d. CCR's shall include provisions for post-construction maintenance and control of exotic plant species within this setback area.
 - e. CCR's shall include the prohibition of the development of walking paths, hardscapes, play structures, or accessory structures; or the placement of permanent fixtures or furniture within this buffer. Only restoration shall be permitted in the 50' buffer.
- BIO-3: During construction, heavy equipment undercarriages and tires be washed prior to entering the site in order to remove any invasive plant seeds.
- BIO-4: A pre-construction nesting study shall be required if construction commences within the nesting time of special-status species that occur in the Project vicinity.
- BIO-5: Construction mitigations shall include temporary fencing at the 50 foot setback, with no construction staging or travel permitted within this area.

Item c: No Impact. There are no designated wetlands on the site. No grading, separation, fill or removal of wetlands is associated with this Project.

Item e: Less Than Significant Impact. The City has a Tree Protection Ordinance that protects certain types of trees based on size and species. The majority of Project Site trees are anticipated to be retained as part of the development. Native trees protected by the Tree Ordinance will require a Tree Removal Permit, subject to Tree Board approval, if they have diameter(s) at breast height of 10 inches or more. Landscaping improvements at the entire Project Site will be implemented, which will involve the addition of 50 or more trees and landscape plantings, subject to Design Review Board approval. If approved, the Project will be consistent with the City's Tree Ordinance.

Item f: No Impact. The City of Sebastopol has a Laguna Wetlands Preserve Restoration and Management Plan, adopted in January 2016. This Project does not conflict with the Laguna Restoration and Management Plan.

V. CULTURAL RESOURCES: Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Cause a substantial adverse change in the significance of a historical resource as defined in § 15064.5?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to § 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

A Cultural Resources Evaluation was completed by William Roop, M.A., RPA of Archaeological Resource Service for the proposed townhomes development located at 6737 Sebastopol Avenue, see Exhibits.

Item a: Less Than Significant with Mitigation Incorporated. The evaluation found that no significant or potentially significant artifacts, archaeological deposits or features have been identified within the Project area. Previous evaluations in the vicinity have identified potentially significant historic era features in fill soils associated with former fruit processing operations. As such, it must be concluded that there is a potential for the discovery of historic era artifacts and/or features identified with historic era activities in Sebastopol. There is a potential that previously undisturbed features could be present in the Project area. Although evidence to date is not particularly supportive of this; in the event that any unanticipated artifacts or cultural features are discovered during grading or underground excavations all work in the vicinity of the find shall be stopped until the discovery area can be evaluated by an archaeologist and appropriate actions are taken.

Item b: No Impact. Site has significant fill from previous development as a cherry packing plant. Per the archeological report conducted by William Coop of Archaeological Resource Service, there are no known archeological resources at the Project Site.

Item c: No Impact. There are no known paleontological or geological resources at the Project Site or within its vicinity.

Item d: No Impact. There are no known human remains at the Project Site or within its vicinity (see response to item a).

Mitigation

- CR-1: In the event that any unanticipated artifacts or cultural features are discovered during grading or underground excavations all work in the vicinity of the find shall be stopped until the discovery area can be evaluated by an archaeologist and appropriate actions are taken.

VI. GEOLOGY AND SOILS: Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
ii. Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iii. Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
iv. Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

Item a: Other than known and Earthquake-related risks (discussed below), the Project development is not expected to expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death as it relates to the following:

Item a.i: Less Than Significant Impact. The Project Site is not located within an Earthquake Fault Study Zone and no known fault traces traverse the site. Therefore, the risk of ground rupture within the limits of the Project Site is low. The Uniform Building Code does not require any special structural engineering beyond the code requirements for construction, given that the Project Site is not located within an Earthquake Fault Study Zone. The new buildings will be constructed with the current California Building Code, which is intended to ensure appropriate earthquake safety.

Item a.ii: Less Than Significant Impact. The Healdsburg-Rodgers Creek and the San Andreas Fault, which are the nearest active faults, are located approximately 8 miles northeast and 12 miles southeast of Sebastopol. The Project Site will be subjected to very strong ground-shaking during a major to moderate earthquake along these faults. It is reasonable to assume on the basis of current technology and historical evidence that the Project Site will be subjected to at least one moderate to severe earthquake that could produce potentially damaging ground-shaking. Furthermore, it is anticipated that the Project Site will periodically experience small to moderate magnitude earthquakes. Adherence to the Building Code will reduce potential impacts from seismic activity at the Project Site to a less than significant level.

Item a.iii: Less Than Significant Impact. The Project Site is located in an area with low to moderate susceptibility to liquefaction according to the 'Liquefaction Hazard Map' as published by the Association of Bay Area Governments. The new buildings will be developed to address potential impacts from seismic-related ground failure, including liquefaction, and will be required to comply with current Building Code seismic safety standards.

Item a.iv: No Impact. The Project Site consists of predominantly flat terrain, with some sloping occurring at the southern-most end of the property, with no identified landslide hazards. This sloped area will not be utilized for the development of structures and is required to be maintained in its natural state.

Item b: Less Than Significant Impact. The potential for erosion at the Project Site is considered to be very low due to its flat topography, the Project be designed to mitigate urban runoff and include storm-water control measures consistent with state and local regulations. The applicant will be required to submit an erosion control plan as part of the Improvement Plan submittal, as conditioned by the Engineering Department.

Item c: Less Than Significant Impact. The Project Site is relatively flat, except along the southern end of the property, when it begins to slope towards the Railroad Forest. The Project site is not located in a hazard or landslide area.

Item d: Less Than Significant Impact. The applicant will be required to submit a detailed soils report to the City for its review, which is certified by an engineer, registered in the State of California and qualified to perform soils work, as required by the Engineering Department. The soils report would include a minimum geotechnical investigation with regards to liquefaction, expansive soils, and seismic safety, if it is required. Adherence to the recommendations of the engineer and the Building Code will reduce potential impacts from seismic activity at the Project Site to a less than significant level.

Item e: No Impact. The Project will be connected to the City's wastewater system. Septic tanks and other alternative systems are not permitted so this issue is not applicable to the Project.

VII. GREENHOUSE GAS EMISSIONS: Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

Items a & b: Less Than Significant Impact. The proposed Project is consistent with the 2016 City of Sebastopol General Plan. The greenhouse gas (GHG) emissions anticipated by the implementation of the General Plan fall below the Bay Area Air Quality Management District (BAAQMD) thresholds of significance for greenhouse gas emissions.

Additionally, the General Plan incorporates provisions to further reduce greenhouse gas emissions. In 2016, the City of Sebastopol partnered with the Sonoma County Regional Climate Protection Authority (RCPA) to produce personalized goals that will reduce greenhouse gases in each city and town as part of the Climate Action Plan 2020 (CAP). Most of the policies in the CAP are related to transportation, "green building", energy efficiency, and renewable energy. The CAP is not included in the General Plan itself, but integrates the strategies and actions identified in the relevant elements of the General Plan. The Project incorporates many of the elements which are listed as goals for reducing greenhouse gas emissions, such as: provides shade tree plantings; reduces travel demand due to walkability (close to services such as banks, pharmacies and grocery stores); provides bicycle parking for each unit; provides electric vehicle charging stations; and, provides water retention areas. Further mitigations (TRANS-2)

will require the Project to provide pedestrian and bicycle connections to the existing multi-purpose trail located on the west side of the site.

The proposed Project is located within the City's Central Core, which means that residents of the development will have easy access to various services within walking distance, as well as easy access to public transportation. The nearest bus stop is located along Morris Street and is ~ 500 feet from the Project Site.

The proposed Project would comply with Green Building Code requirements, would have a less than significant impact on the environment, and would have no impact on implementation of plans, policies, or regulations adopted for the purpose of reducing greenhouse gas emissions.

VIII. HAZARDS AND HAZARDOUS MATERIALS: Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

or working in the project area?				
g. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
h. Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

Item a: No Impact. The nature of the proposed Project as residential town homes will not involve the transport, use, and disposal of hazardous resources on a commercial scale. Households would use chemically based products and pesticides in small amounts, which may be defined as hazardous. A Condition of Approval has been added which requires the CC&Rs for this development to include details regarding the maintenance of common and/or private open space located on the Project Site; which must also include a prohibition of the use of non-biodegradable and toxic chemicals in maintenance of both common and private open space areas.

Item b: No Impact. The nature of the proposed Project as residential town homes will not create uses would not produce reasonably foreseeable upset and accident conditions that could cause a release of hazardous materials.

Item c: No Impact. The Analy High School and the Laguna High School lie within one-half mile north and south of the Project Site respectively. In the absence of uses associated with the proposed Project that produce, use, or transport hazardous materials, no impact upon these schools would result.

Item d: No Impact. Site is not so listed.

Item e: No Impact. The Project Site is not located within an area subject to an airport land use plan or within 2 miles of an airport. The closest airport is the Charles M. Schulz – Sonoma County Airport, located ~23.3 miles from the Project Site. Given the distance between the Project Site and the Sonoma County Airport, the Project would not have the potential to result in a safety hazard. Therefore, no impact would occur, and no mitigation measures are required.

Item f: No Impact. The Project Site is not located within the vicinity of a private airstrip. No impact would occur, and no mitigation measures are required.

Item g: Less Than Significant Impact. The Project includes an easement that is utilized for an emergency egress route from the neighboring Park Village Mobile Home Park. Currently Park Village (a mobile home park, located in the flood plain) has an emergency evacuation route, which utilizes the Project Site via this easement. The Project includes the redevelopment of the existing fence and gate along the eastern property line of the Project Site, where it abuts the Park Village Property; however, the design will maintain the access necessary to maintain the emergency access evacuation route, in the case of a flood event or other emergency. As such, the Project will not have a significant impact to an established emergency evacuation plan.

Item h: No Impact. The Project Site is not located in a Wildland Urban Interface Zone.

IX. HYDROLOGY AND WATER QUALITY: Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of preexisting nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner, which would result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner, which would result in flooding on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f. Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g. Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
h. Place within a 100-year flood hazard area structures, which would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

dam?				
j. Inundation by seiche, tsunami, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

Item a: Less Than Significant Impact. The Project involves construction of new buildings and a parking lot on a site which was previously developed and filled; and has no existing natural water features. The Project will be required to abide by the City's urban runoff/stormwater standards. The Project will be required to meet all City of Sebastopol storm water requirements as set forth in the Municipal Code and in addition may be required to obtain a Construction General Storm Water Permit from the State Regional Water Quality Control Board to ensure compliance with State requirements.

Item b: Less Than Significant Impact. The Project is within long-term, planned parameters of water use identified by the General Plan. The total annual water production was approximately 27% of maximum potential production in 2017, according to the 2017 Level of Service Report. There is substantial remaining production capacity sufficient to accommodate the proposed Project. The City has determined that there is adequate water system capacity, production and distribution to accommodate this Project. The Project will be required to include on-site water detention and pervious paving where feasible to promote recharge, and will be required to provide low-water use fixtures to reduce impacts of the Project.

Item c: Less Than Significant Impact. The Project Site does not contain any naturally occurring creeks or bodies of water. The Project includes the development of bio retention areas for drainage and stormwater runoff. The Project does not appear to have potential for any substantial erosion in that it will be developed on a site that was previously developed. The residential units and parking lot will also be developed in accordance with City standards which regulate storm water quality. If determined necessary by the City Engineer, a condition of approval will require the applicant to submit an Erosion Control Plan.

Item d: Less Than Significant Impact. The Project development includes the creation of bio retention areas to filter stormwater.

Item e: Less Than Significant Impact. The Project sites bio retention areas will minimize overland runoff before it is dissipated off-site.

Item f: Less Than Significant Impact. See 'Item a' for the response.

Item g: Less Than Significant with Mitigation Incorporated. The Project Site is within the 100-year flood plain, as such, it will require a Floodplain Development Permit. This permit provides a context for appropriate development-specific requirements for construction of operation of the use, and ensures the Project complies with the City's flood hazard requirements. The residential structures will be constructed so that the lowest finished floor is 2 feet above the required base-flood elevation to protect against flooding of structures as part of this requirement. With the Floodplain Development Permit the impact will be less than significant as mitigated.

Item h: Less Than Significant with Mitigation Incorporated. The site is not located within a regulatory flood way.

Item i: Less Than Significant with Mitigation Incorporated. As mentioned before, the Project is subject to the City's flood hazard requirements, which includes the requirement that residential structures be constructed so that the lowest finished floor is 2 feet above the base-flood elevation to protect against flooding of structures. The Project also includes flood vents to allow flow of water under structures.

Item j: No Impact. The Project is not located in an area that is susceptible to inundation by seiche, tsunami, or mudflow.

Mitigation:

- HYDRO-1: A Flood Plain Development Permit shall be required, and the Project shall comply with the City's Floodplain Ordinance (SMC 15.16).

X. LAND USE AND PLANNING: Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Conflict with any applicable habitat conservation plan or natural community conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

Item a: No Impact. The Project Site is presently located within the City of Sebastopol. The Project is an infill development that will not physically divide an established community.

Item b: Less Than Significant Impact. The applicant includes multiple entitlements, which require hearings by different City bodies. The entitlements include: 1) A request to modify the ESOS study requirements and approval of study modification (visual analysis); 2) a Planned Community Zoning designation; 3) a major Use Permit for a fully residential project in a commercial zone; (4) a Tentative Map for subdivision of more than 4 parcels; 5) Design Review for a subdivision of three or more units; and, 6) Environmental Review (California Environmental Quality Act, or CEQA review).

The Project site is located within the CD: Central Core Zoning District and has a General Plan Land Use designation of Central Core. The Planned Community Development (which includes the development of solely residential in a commercial district) is a conditionally permitted use

and requires a Use Permit in this district. With the Planned Community and Use Permit approval the Project will be consistent with the General Plan and Zoning Ordinance in terms of land use.

ESOS. The Project Site has an environmental overlay ESOS: Environmental and Scenic Open Space. The ESOS study may be reduced to a visual resource analysis based on the site characteristic and development history of the site. The Planning Commission approved the reduction on October 23, 2018. Approval of the ESOS visual resource analysis will make the Project consistent with the General Plan and Zoning Ordinance ESOS requirements.

A 50' setback from Railroad Forest property is required as part of the ESOS requirements to provide adequate buffer to the existing sensitive habitat located to the south of the Project Site.

The Project also requires Design Review Board approval, following Planning Commission and City Council review, to ensure that it is consistent with the City's design objectives in terms of architecture and landscape design. Impacts will be less than significant with these approvals.

Item c: No Impact. The City has not adopted a habitat conservation plan or natural community conservation plan applicable to this site.

XI. MINERAL RESOURCES: Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

Item a: No Impact. There are no known mineral resources at the Project Site.

Item b: No Impact. There are no locally-imported mineral resource recovery sites delineated in the General Plan.

XII. NOISE: Would the project result in:

	Potentially Significant Impact	Less Than Significant with	Less Than Significant Impact	No Impact

		Mitigation Incorporated		
a. Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

Item a & b: Less Than Significant Impact. The City's Noise Ordinance calls for daytime levels of 55 dBA, and nighttime levels of 45 dBA or lower in commercial zones. Long term use (residential) are not a significant noise generator and will be subject to these requirements.

Item c: Less Than Significant Impact. In terms of long-term effects, operation of a residential development is not identified as a significant noise generator and therefore the development of this Project will not result in the generation of noise levels that contribute substantially to the noise environment. No operations are anticipated that will generate excessive groundborne vibration or noise levels. Overall, the development would result in typical noise generated by a residential development. Impacts will be less than significant.

Item d: Less Than Significant. Construction activities will result in a temporary increase in noise levels, however construction hours will be subject to City ordinance limits. Impacts will be less than significant.

Item e: No Impact. The proposed Project is not located within an airport land use plan or within two miles of a public airport and would not expose people to excessive noise level. Project will have no impact.

Item f: No Impact. The proposed Project is not located in the vicinity of a private airstrip and would not expose people to excessive noise level. Project will have no impact.

XIII. POPULATION AND HOUSING: Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

Item a: Less Than Significant Impact. The Project involves the creation of 18 townhomes. The Project represents incremental residential growth that will not induce substantial growth in the area; will be subject to the City's Growth Management Ordinance.

Item b: No Impact. The proposed Project Site is currently undeveloped and will not displace people or residences. Therefore, it does not generate an impact based on displacements of people or structures.

Item c: No Impact. The proposed Project Site is currently undeveloped and will not displace people or residences. Therefore, it does not generate an impact based on displacements of people or structures.

XIV. PUBLIC SERVICES

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for				

new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
• Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
• Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
• Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
• Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
• Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

Item a: Less Than Significant Impact. The Project can be adequately serviced by existing police and fire facilities and services, and would not have a significant effect on acceptable service ratios, response times, or other performance objectives, according to the City’s Police Department, and the Fire Department.

Construction of this Project will likely contribute to the addition of school-aged children to the local school population. (Sebastopol West County Schools have adequate capacity student numbers, an increase in school-aged children would probably be a benefit). Additionally, the Project will contribute to school resources via payment of a standard school impact fee, which is paid on net new square footage.

The proposed Project will also pay standard City Impact Fees. Impact fees provides a funding source to construct the police, fire, community amenities, government facilities, and roadway infrastructure necessary to mitigate the impacts of the growth expected in the City of Sebastopol. Therefore, no significant impacts to the environment related to the construction of recreation facilities would result with implementation of the Project, and no additional mitigations are required.

The Project is also subject to payment of the Park In-Lieu fee, and such revenues are used for capital improvements in City parks or expansion of parkland. Impacts to the City parks are expected to be modest in scope in that this is a relatively small residential development. Routine maintenance of City parks and public facilities can be accommodated by existing public facilities and City staff.

The 2016 General Plan requires one (1) acre of parkland for each 200 residents (which equates to five (5) acres for every 1,000 residents). As of the 2017, according to the City’s annual Level of Service Report, the total parkland ratio is 6.1 acres for each 1,000 residents. The proposed Project would result in the addition of 18 townhomes (~45 residents) but would this will not result in a significant increase to the use or deterioration of surrounding recreational facilities.

XV. RECREATION

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

Item a: Less Than Significant Impact. The 2016 General Plan requires one (1) acre of parkland for each 200 residents (which equates to five (5) acres for every 1,000 residents). As of the 2017, according to the City’s annual Level of Service Report, the total parkland ratio is 6.1 acres for each 1,000 residents. The proposed Project would result in the addition of 18 townhomes and approximately 45 new residents, and will not result in a significant increase to the use or deterioration of surrounding recreational facilities.

Item b: No Impact. The Project is proposing to pay park-in-lieu fees instead of dedicating and constructing park facilities.

XVI. TRANSPORTATION/TRAFFIC: Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

b. Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e. Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
f. Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

W-Trans, a traffic engineer consultant for the City of Sebastopol, prepared a detailed traffic impact study, which is attached and incorporated into this Initial Study. The study reviews the existing setting and Project impacts related to the roadway network; traffic conditions; transit conditions; pedestrian conditions; bicycle conditions; parking conditions; and site access and circulation. The study evaluated conditions and impacts at intersections in the Project area, including cumulative conditions in future years. The study utilized City standards, policies, and methodologies for its analysis.

Item a: The proposed Davis Townhomes development project would include 18 two-bedroom townhomes and will be served by 38 parking spaces.

The proposed Project is expected to generate an average of 132 daily vehicle trips, including 8 trips during the morning peak hour and 10 trips during the evening peak hour.

The study area includes the following four intersections:

1. Sebastopol Avenue (SR 12)/Morris Street
2. Sebastopol Avenue (SR 12)/Petaluma Avenue (SR116)
3. Bodega Avenue – SR 12/North Main Street – South Main Street (SR116)
4. North Main Street (SR116)/McKinley Street

Analysis indicates that all study intersections, under Existing Conditions, are operating acceptably at LOS D or better during both peak periods. These service levels would remain unchanged with the addition of the Project-related traffic generated by the townhome development, and the Project's short-term impact would be considered less than significant.

However, due to the particular sensitivity of the Sebastopol Ave (SR 12)/Morris Street intersection to congestion, the study has recommended the following mitigation measure to address the issues:

Mitigation:

- TRANS- 1: Project shall restripe the southbound Morris Street approach to create two 15-foot lanes (one inbound and one outbound), and if needed, re-install the traffic signal detector position.

Existing pedestrian, bicycle, and transit facilities to and from the Project Site are adequate to serve the Project. The Project proposes to include bicycle hooks for each unit for bicycle parking. However, pedestrian facilities between the Railroad Forest Park Trail and the project site is lacking. To mitigate this, the project shall provide a pedestrian and bicycle connection to the existing trail system located to the west. This connector will also provide a safe pedestrian access from the site to the sidewalk network at Sebastopol Avenue. This should formalize the current pedestrian and bicycle access trail to the adjacent parking lot as well.

Mitigation:

- TRANS-2: Project shall include a walkway/sidewalk connection between the Project Site and the existing multi-purpose trail on the west side of the Project Site.

Item c: No Impact. The Project Site is not located near any public or private airstrips. Project scope will not result in a change to air traffic patterns.

Item d: No Impact. Project scope will not result in the construction of any public roads.

Item e: Less Than Significant with Mitigation Incorporated. The Project Site currently has an access easement for emergency egress for the neighboring Park Village Mobile Home Park. A locked access gate between the two properties is proposed for the Project and this would be unlocked in case of emergencies to allow for residents of the Mobile Home Park to evacuate their properties. This access could also be used, if needed, to evacuate the Davis Townhomes Project.

Mitigation:

- TRANS-3: The locked access gate between the Village Park Mobile Home Park and the Davis Townhomes properties shall be unlocked and opened in case of emergencies to allow for emergency evacuation.

Item f: Less Than Significant Impact. The Project will not conflict with adopted City of Sebastopol policies, plans, or programs supporting alternative transportation.

XVII. TRIBAL CULTURAL RESOURCES: Would the project cause a substantial adverse change in the significance of a tribal cultural resource, as defined in Public Resources Code Section 21074 as either a site, feature, place cultural landscape that is

geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Listed or eligible for a listing in the California Register of Historic Resources, or in a local register of historic resources as defined in Public Resources Code Section 5020.1(k)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1 (In applying the criteria as set forth in this Section, the lead agency shall consider the significance of the resource to a California Native American Tribe)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

A Cultural Resources Evaluation was completed by William Roop, M.A., RPA of Archaeological Resource Service for the proposed townhomes development located at 6737 Sebastopol Avenue, see Exhibits.

Item a: No Impact. Per the Cultural Resources Evaluation completed by William Roop, M.A., the site is not listed or eligible for a listing in the California Register of Historic Resources, or in a local register. Furthermore, the evaluation found that no significant or potentially significant artifacts, archaeological deposits or features have been identified within the Project area.

Item b: No Impact. A referral letter and attachments detailing the Project scope was sent to Tribal Heritage Preservation Officer for the Federated Indians of Graton Rancheria on 11.14.2018. Additional information regarding the Project were requested. The additional requested materials were sent on 1.23.2019; no response was received. A follow up email was sent on 2.19.2019; no response has been received as of the publication date (3.7.2019) of this study.

XVIII. UTILITIES AND SERVICE SYSTEMS: Would the project:

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Exceed wastewater treatment requirements of the applicable	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Regional Water Quality Control Board?				
b. Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f. Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g. Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

Item a: Less Than Significant Impact. Based on the 2017 annual Level of Service Report provided to the City Council (incorporated by reference), ample capacity remains in the City of Sebastopol's waste water treatment allocation to serve this development and meet applicable requirements of the Regional Water Quality Control Board. The Level of Service report indicates that 2017 City-wide wastewater flows were at approximately 41% of treatment capacity. That figure includes allowances for known undeveloped projects. The Project is within the planned growth identified in the General Plan.

Item b: Less Than Significant Impact. The site is currently improved with water and sewer connections. New connections may be necessary, given the scope of the Project. The Project will be required to inspect existing connections and evaluate any specific water or sewer line improvements needed to meet current code requirements, and to provide such improvements as part of an Improvement Plan. Once connected, per the 2017 Level of Service report, there is ample capacity in the City of Sebastopol's water and sewer systems to accommodate this Project.

Item c: Less Than Significant Impact. An Engineering Department condition of approval will require the applicant to submit to the City of Sebastopol for review and approval by the City Engineer, a hydrology study, hydraulic calculations and drainage plans prepared by a Registered Civil Engineer licensed in the State of California, in accord with applicable City

standards. In addition, a Regional Water Quality Board Construction General stormwater permit may be required to ensure compliance with State storm water requirements.

Item d: Less Than Significant Impact. See 'Item b' for the response. Based on the 2017 annual Level of Service Report provided to the City Council (incorporated by reference), ample capacity remains in the City of Sebastopol's water system to serve the proposed development. The Level of Service report indicates that 2017 water production was at approximately 27% of pumping capacity.

Item e: Less Than Significant Impact. See 'Item a' for the response.

Item f: Less Than Significant Impact. The solid waste from the development will be collected and disposed of by the City's franchise hauler Recology. There is sufficient capacity in the disposal system to accommodate the additional solid waste that will be generated by this Project.

Item g: Less Than Significant Impact. The solid waste generated by the development will be handled in compliance with federal, state, and local statutes.

XIX. MANDATORY FINDINGS OF SIGNIFICANCE

	Potentially Significant Impact	Less Than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
a. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Does the project have environmental effects which will cause substantial adverse effects on human beings,	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

either directly or indirectly?				
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Discussion:

Item a: Less Than Significant Impact. Please refer to all sections of the Initial Study, including but not limited to Section II - Agriculture and Forest Resources, Section VI – Geology and Soils, Section VII –Greenhouse Gas Emissions, Section X – Land Use and Planning, Section XIV – Public Services. The Project will not create significant agricultural, air quality, cultural or other effects referenced by this question.

Item b: Less Than Significant with Mitigation Incorporated. This Initial Study identifies potential cumulative adverse impacts associated with the construction of the Project in regards to the following; Aesthetics, Air Quality, Biological Resources, Cultural Resources, Hydrology/Water Quality, and Transportation/Traffic. Mitigation measures will reduce impacts to less-than-significant levels. Mitigation Measures include the following:

Aesthetics –

- o AES-1: Building height shall be limited to 2 stories.
- o AES-2: Exterior colors shall be selected which blend with the natural surroundings. Final colors shall be approved by the Design Review Board.
- o AES-3: Plantings shall be included along the eastern property line, in addition to the proposed 6' fence, to provide additional screening.
- o AES-4: Construction fencing shall be placed along the periphery of the Project Site on the north, east and west property lines to screen construction activity from view. The southern construction fencing shall run along the required 50 setback buffer from the Railroad Forest property and not from the property line.

Air Quality -

- o AQ-1: Include basic measures to control dust and exhaust during construction. During any construction period ground disturbance, the applicant shall ensure that the project contractor implement measures to control dust and exhaust. Implementation of the measures recommended by BAAQMD and listed below would reduce the air quality impacts associated with grading and new construction to a less than significant level. The contractor shall implement the following best management practices that are required of all projects:
 - All haul trucks transporting soil, sand, and other loose material off-site shall be covered.
 - All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per week. The use of dry power sweeping is prohibited.
 - All vehicle speeds on unpaved roads shall be limited to 15 miles per hour (mph).
 - All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.
 - Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure Title 13, Section 2485 of California Code of Regulations [CCR]). Clear signage shall be provided for construction workers at all access points.

- All construction equipment shall be maintained and properly tuned in accordance with manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation.
- Post a publicly visible sign with the telephone number and person to contact at the Lead Agency regarding dust complaints. This person shall respond and take corrective action within 48 hours. The Air District's phone number shall also be visible to ensure compliance with applicable regulations.

Biological Resources –

- BIO-1: A 50-foot environmental setback from the south property line shall be maintained in perpetuity as a buffer to the environmental resources of the adjacent Railroad Forest property (APN 004-072-014). An open space restriction for this area shall be included on the Final Map.
- BIO-2: Maintenance of this setback and buffer area shall include the following restrictions and requirements:
 - The applicant shall develop a plan wherein all Invasive plants shall be removed within this area prior to final inspection.
 - The buffer area shall be replanted with native species appropriate to provide a buffer to the Railroad Forest area prior to final inspection.
 - The removal and replanting plan shall be developed by, or reviewed and approved by, a qualified biologist and City Staff prior to issuance of a building permit.
 - CCR's shall include provisions for post-construction maintenance and control of exotic plant species within this setback area.
 - CCR's shall include the prohibition of the development of walking paths, hardscapes, play structures, or accessory structures; or the placement of permanent fixtures or furniture within this buffer. Only restoration shall be permitted in the 50' buffer.
- BIO-3: During construction, heavy equipment undercarriages and tires be washed prior to entering the site in order to remove any invasive plant seeds.
- BIO-4: A pre-construction nesting study shall be required if construction commences within the nesting time of special-status species that occur in the Project vicinity.
- BIO-5: Construction mitigations shall include temporary fencing at the 50 foot setback, with no construction staging or travel permitted within this area.

Cultural Resources -

- CR-1: In the event that any unanticipated artifacts or cultural features are discovered during grading or underground excavations all work in the vicinity of the find shall be stopped until the discovery area can be evaluated by an archaeologist and appropriate actions are taken.

Hydrology/Water Quality –

- HYDRO-1: A Flood Plain Development Permit shall be required, and the Project shall comply with the City's Floodplain Ordinance (SMC 15.16).

Transportation/Traffic –

- TRANS-1: Project shall restripe the southbound Morris Street approach to create two 15-foot lanes (one outbound and one inbound), and if needed, re-install the traffic signal detector position.
- TRANS-2: Project shall include a walkway/sidewalk connection between the Project Site and the existing multi-purpose trail on the west side of the Project Site.
- TRANS-3: The locked access gate between the Village Park Mobile Home Park and the Davis Townhomes properties shall be unlocked and opened in case of emergencies to allow for emergency evacuation.

Item c: Less Than Significant Impact. This Initial Study does not identify any potential substantial adverse impacts on human beings associated with the Project.

EXHIBITS

- Traffic Study, prepared by W-Trans – March 2019
- Cultural Resources Evaluation, prepared by Archaeological Resource Service – January 2019

DOCUMENTS INCORPORATED BY REFERENCE (references for baseline regulations) -

*Documents available for review at the Sebastopol Planning Department
7120 Bodega Avenue, Sebastopol, CA 95472*

- City of Sebastopol General Plan and EIR – adopted November, 2016 [State Clearinghouse #2016032001]
- City of Sebastopol Zoning Ordinance
- Laguna Wetlands Preserve Restoration and Management Plan adopted – January 2016
- Climate Action Plan 2020 and Beyond, Sonoma County Regional Climate Protection Authority – July 2016.
- BAAQMP Air Quality Plan – May 2017
- City of Sebastopol Annual Level of Service Report – 2017



Traffic Impact Study for the Davis Townhomes



Prepared for the City of Sebastopol

Submitted by
W-Trans

March 5, 2019



**TRAFFIC ENGINEERING
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- A. Collision Rate Calculations
- B. Intersection Level of Service Calculations



Executive Summary

The Davis Townhomes proposed project will build 18 two-bedroom townhomes on an existing vacant lot at the terminated end of Morris Street, south of Sebastopol Avenue in the City of Sebastopol. The proposed project is expected to generate an average of 132 daily vehicle trips, including 8 trips during the morning peak hour and 10 trips during the evening peak hour.

The study area includes the intersections of Sebastopol Avenue (SR 12)/Morris Street, Sebastopol Avenue (SR 12)/Petaluma Avenue (SR 116), Bodega Avenue-SR 12/North Main Street-South Main Street (SR 116), and North Main Street/McKinley Street. Analysis indicates that under Existing Conditions the study intersections are all operating acceptably at LOS D or better during both peak periods. These service levels would remain unchanged upon the addition of project-related traffic, and the project's short-term impact would be considered less-than-significant.

However, because delay generated by the southern leg could impact delay and congestion during the peak congestion periods, it would be appropriate to provide measures to reduce this impact. Recommended signal timing and resulting simulations with the updated timing plan, the intersection of Sebastopol Avenue (SR 12)/Morris Street will experience a decreased delay of approximately 5 seconds which would offset the delay created by the project. In order to provide an additional layer of reduced traffic impact, the southbound approach should be restriped from the current to allow southbound right-turn traffic to make a right on red when there is one car queued up waiting to make a left turn.

Existing pedestrian, bicycle, and transit facilities are generally adequate to serve the project site. According to the site plan, bicycle hooks are provided for each unit for bicycle parking. Pedestrian facilities between the Forest Park Trail and the project site is lacking and should be considered for design enhancements. It is recommended that the project include plans for walkway/sidewalk connection between the project site to the Forest Park Trail.

Introduction

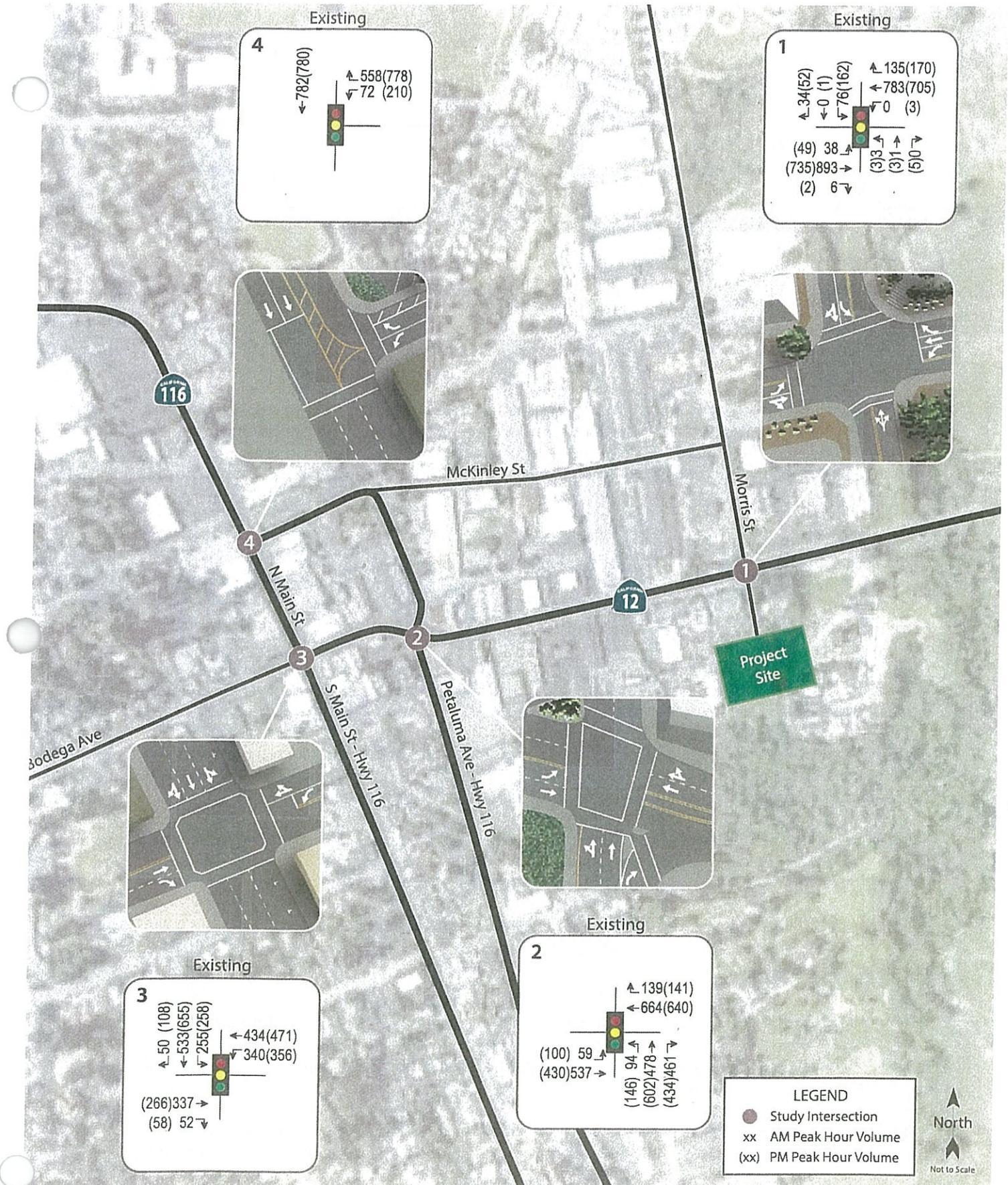
This report presents an analysis of the potential traffic impacts that would be associated with development of a proposed residential project to be located where Morris Street terminates south of Sebastopol Avenue (SR 12) in the City of Sebastopol. The traffic study was completed in accordance with the criteria established by the City of Sebastopol, and is consistent with standard traffic engineering techniques.

Prelude

The purpose of a traffic impact study is to provide City staff and policy makers with data that they can use to make an informed decision regarding the potential traffic impacts of a proposed project, and any associated improvements that would be required in order to mitigate these impacts to a level of insignificance as defined by the City's General Plan or other policies. Vehicular traffic impacts are typically evaluated by determining the number of new trips that the proposed use would be expected to generate, distributing these trips to the surrounding street system based on existing travel patterns or anticipated travel patterns specific to the proposed project, then analyzing the impact the new traffic would be expected to have on critical intersections or roadway segments. Impacts relative to access for pedestrians, bicyclists, and to transit are also addressed.

Project Profile

The proposed project includes 18 two-story townhomes to be constructed on a currently vacant lot in the City of Sebastopol. The project site is located at the south end of Morris Street, as shown in Figure 1.



Transportation Setting

Operational Analysis

Study Area and Periods

The study area consists of the following intersections:

1. Sebastopol Avenue (SR 12)/Morris Street
2. Sebastopol Avenue (SR 12)/Petaluma Avenue (SR 116)
3. Bodega Avenue-SR 12/North Main Street-South Main Street (SR 116)
4. North Main Street (SR 116)/McKinley Street

Operating conditions during the a.m. and p.m. peak periods were evaluated to capture the highest potential impacts for the proposed project as well as the highest volumes on the local transportation network. The morning peak hour occurs between 7:00 and 9:00 a.m. and reflects conditions during the home to work or school commute, while the p.m. peak hour occurs between 4:00 and 6:00 p.m. and typically reflects the highest level of congestion during the homeward bound commute.

Study Intersections

Sebastopol Avenue (SR 12)/Morris Street is a signalized intersection with protected left-turn phasing on the eastbound and westbound Sebastopol Avenue approaches and split phasing on the northbound and southbound Morris Street approaches. There are marked crosswalks on the north, south, and west legs of the intersection.

Sebastopol Avenue (SR 12)/Petaluma Avenue (SR 116) is a signalized intersection with Petaluma Avenue flowing one-way northbound. Left-turns have protected phasing on the eastbound Sebastopol Avenue approach. There are marked crosswalks on all four legs of the intersection.

Bodega Avenue-SR 12/North Main Street-South Main Street (SR 116) is a signalized intersection with Main Street flowing one-way southbound. The westbound Sebastopol Avenue approach has protected left-turn phasing. There are marked crosswalks on all four legs of the intersection.

North Main Street (SR 116)/McKinley Street is a signalized tee-intersection with McKinley Street terminating. McKinley Street is a one-way westbound street and North Main Street becomes a one-way southbound street just south of the intersection. There are marked crosswalks on the south and east legs.

The locations of the study intersections and the existing lane configurations and controls are shown in Figure 1.

Collision History

The collision history for the study area was reviewed to determine any trends or patterns that may indicate a safety issue. Collision rates were calculated based on records available from the California Highway Patrol as published in their Statewide Integrated Traffic Records System (SWITRS) reports. The most current five-year period available is January 1, 2014 through December 31, 2018.

As presented in Table 1, the calculated collision rates for the study intersections were compared to average collision rates for similar facilities statewide, as indicated in *2014 Collision Data on California State Highways*,

California Department of Transportation (Caltrans). All four study intersections had collision rates lower than the Statewide average for similar intersections. The collision rate calculations are provided in Appendix A.

Table 1 – Collision Rates at the Study Intersections

Study Intersection	Number of Collisions (2014-2018)	Calculated Collision Rate (c/mve)	Statewide Average Collision Rate (c/mve)
1. Sebastopol Ave (SR 12)/Morris St	5	0.14	0.27
2. Sebastopol Ave (SR 12)/Petaluma Ave (SR 116)	12	0.26	0.27
3. Bodega Ave-SR 12/N Main St-S Main St (SR 116)	6	0.15	0.27
4. N Main St/McKinley St	1	0.03	0.21

Note: c/mve = collisions per million vehicles entering

While Sebastopol Avenue (SR 12)/Petaluma Avenue (SR 116) had a lower average collision rate than the statewide average, it had an injury rate of 50.0 percent, which exceeds the statewide average of 41.9 percent on similar facilities. There were two separate pedestrian-involved collisions with vehicles making a northbound left turn and a pedestrian in the crosswalk on the west leg. The City is studying several intersections, including this one, to improve the signal timing and possible coordination within the downtown core. Both reported collisions involving a pedestrian had a primary collision factor of pedestrian right-of-way violation. With the planned signal timing improvements, an emphasis should be placed on considering pedestrian safety.

Alternative Modes

Pedestrian Facilities

Pedestrian facilities generally include sidewalks, crosswalks, pedestrian signal phases, curb ramps, curb extensions, and various streetscape amenities such as lighting, benches, etc. In general, a network of sidewalks, crosswalks, traffic signals, and curb ramps provide access for pedestrians in the vicinity of the project site. The Forest Park Trail is located along the western border of the project property, and provides a connection to the Joe Rodota Trail south of project site.

- **Sebastopol Avenue (SR 12)** – Sebastopol Avenue is the main connector between the City’s eastern edge and the Barlow area and the downtown with nearby neighborhoods and schools. Along the project frontage there is sidewalk coverage on both sides of Sebastopol Avenue between Nelson Way and approximately one-quarter mile east of Morris Street.
- **Morris Street** – Complete sidewalk coverage is provided on the eastern side of Morris Street between Sebastopol Avenue and Eddie Lane. Partial sidewalk coverage is provided on the western side of Morris Street, with gaps in sidewalk coverage between Laguna Park Way and Eddie Lane. There are continuous sidewalk connections on both sides of the street between the project and Laguna Park Way.

Bicycle Facilities

The *Highway Design Manual*, Caltrans, 2017, classifies bikeways into four categories:

- **Class I Multi-Use Path** – a completely separated right-of-way for the exclusive use of bicycles and pedestrians with cross flows of motorized traffic minimized.
- **Class II Bike Lane** – a striped and signed lane for one-way bike travel on a street or highway.

- **Class III Bike Route** – signing only for shared use with motor vehicles within the same travel lane on a street or highway.
- **Class IV Bikeway** – also known as a separated bikeway, a Class IV Bikeway is for the exclusive use of bicycles and includes a separation between the bikeway and the motor vehicle traffic lane. The separation may include, but is not limited to, grade separation, flexible posts, inflexible physical barriers, or on-street parking.

In the project area there are several Class I multi-use bike/pedestrian paths, including the Joe Rodota Trail, which connects Petaluma Avenue with Santa Rosa; the Railroad Forest Path, which connects Sebastopol Avenue/Morris Street with the Joe Rodota Trail; and the West County Trail, which runs between Eddie Lane and Occidental Road to the north. There are existing bicycle lanes along Morris Street between Sebastopol Avenue and Eddie Lane, and along SR 116 between Mill Station Road and Lynch Road. According to the *City of Sebastopol Bicycle and Pedestrian Master Plan* (2011), a bike route is planned along Sebastopol Avenue-Bodega Avenue between Petaluma Avenue and Dutton Avenue and bike lanes are planned on Bodega Avenue between Dutton Avenue and Ragle Road. Bicyclists ride in the roadway and/or on sidewalks along all other streets within the project study area. Table 2 summarizes the existing and planned bicycle facilities in the project vicinity, as contained in the *City of Sebastopol Bicycle and Pedestrian Master Plan*.

Table 2 – Bicycle Facility Summary

Status Facility	Class	Length (miles)	Begin Point	End Point
Existing				
Railroad Forest Bike path	I	0.20	Sebastopol Ave	Joe Rodota Trail
West County Trail*	I	5.70	Eddie Ln	Forestville
Joe Rodota Trail*	I	6.30	Petaluma Ave	Santa Rosa Trail
Morris St	II	0.42	Sebastopol Ave	Eddie Ln
Gravenstein Hwy N-Healdsburg Ave- Main St-Petaluma Ave-Gravenstein Hwy S	II	2.43	Mill Station Rd	Lynch Rd
Sebastopol Ave (SR 12)	III	0.19	Morris St	Petaluma Ave
Planned				
Bodega Ave	II	0.87	Dutton Ave	Ragle Rd
Sebastopol Ave	III	0.32	Petaluma Ave	Dutton Ave

Notes: * All or portions of these bikeways are located with adjacent jurisdictions

Source: *City of Sebastopol Bicycle and Pedestrian Master Plan*, Sonoma County Transportation Authority, 2011

Transit Facilities

Sonoma County Transit (SCT) provides fixed route bus service in Sonoma County. Routes 20, 22, 24 and 52 provide regional service between the project site and surrounding communities. Each route stops on Laguna Park Way at the Sebastopol Transit Hub about one-quarter mile northwest of the project site.

Route 20 runs between the Russian River area and Santa Rosa and operates Monday through Friday, serving regional commuters to Santa Rosa with one-and-one-half to two-hour headways between 6:00 a.m. and 9:30 p.m. On the weekends, Route 20 runs from 8:00 a.m. to 8:15 p.m. with approximately three-hour headways.

Route 22 provides service between Sebastopol and Santa Rosa and operates Monday through Friday with two departures during the morning commute period and two departures during the evening commute period.

Route 24 provides service within Sebastopol, operating on weekdays with headways of about 45 minutes between 9:00 a.m. and 3:30 p.m., and on Saturdays with headways of about 45 minutes between 9:00 a.m. and 3:00 p.m.

Route 52 operates Monday through Friday between Sebastopol and Rohnert Park/Cotati. There is an approximate headway of one-and-one-half hours between buses, which run from 6:10 am to 6:20 p.m.

Two bicycles can be carried on most SCT buses. Bike rack space is on a first-come, first-served basis. Additional bicycles are allowed on SCT buses at the discretion of the driver.

Dial-a-ride, also known as paratransit, or door-to-door service, is available for those who are unable to independently use the transit system due to a physical or mental disability. Volunteer Wheels, the Americans with Disabilities Act (ADA) paratransit operator for Sonoma County Transit, is designed to serve the needs of individuals with disabilities within the incorporated areas of Sonoma County and between the County's nine incorporated cities.

Capacity Analysis

Intersection Level of Service Methodologies

Level of Service (LOS) is used to rank traffic operation on various types of facilities based on traffic volumes and roadway capacity using a series of letter designations ranging from A to F. Generally, Level of Service A represents free flow conditions and Level of Service F represents forced flow or breakdown conditions. A unit of measure that indicates a level of delay generally accompanies the LOS designation.

The study intersections were analyzed using the "Signalized" methodology published in the *Highway Capacity Manual* (HCM), Transportation Research Board, 2000. All four of the study intersections were analyzed using the HCM 2000 signalized methodology since the HCM 2010 methodologies are not compatible with the signal phasing in use. This source contains methodologies for various types of intersection control, all of which are related to a measurement of delay in average number of seconds per vehicle. The signalized methodology uses factors including traffic volumes, green time for each movement, phasing, whether the signals are coordinated or not, truck traffic, and pedestrian activity. Average stopped delay per vehicle in seconds is used as the basis for evaluation in this LOS methodology. For purposes of this study, delays were calculated using optimized signal timing.

The ranges of delay associated with the various levels of service are indicated in Table 3.

Table 3 – Signalized Intersection Level of Service Criteria

LOS A	Delay of 0 to 10 seconds. Most vehicles arrive during the green phase, so do not stop at all.
LOS B	Delay of 10 to 20 seconds. More vehicles stop than with LOS A, but many drivers still do not have to stop.
LOS C	Delay of 20 to 35 seconds. The number of vehicles stopping is significant, although many still pass through without stopping.
LOS D	Delay of 35 to 55 seconds. The influence of congestion is noticeable, and most vehicles have to stop.
LOS E	Delay of 55 to 80 seconds. Most, if not all, vehicles must stop and drivers consider the delay excessive.
LOS F	Delay of more than 80 seconds. Vehicles may wait through more than one cycle to clear the intersection.

Reference: *Highway Capacity Manual*, Transportation Research Board, 2000

Traffic Operation Standards

All study intersections are located within the City of Sebastopol's city limits and are therefore subject to the City's LOS standards. The *City of Sebastopol General Plan*, last updated in 2016, adopted Level of Service standards in Program 16.1 and as implemented by the City as follows:

- **At signalized intersections:** At signalized intersections, levels of service shall be determined for the overall intersection.
- Intersection queuing shall be evaluated in tandem with LOS. Projected queues at signalized intersections shall not extend through upstream signalized intersections.
- In evaluating circulation improvement needs at downtown intersections, mitigations should be avoided which increase capacity by widening that causes impacts to right-of-way and/or historical structures.

- Allow a minimum operation of LOS D for signalized intersections within the Downtown; a LOS C for all signalized intersections outside of the Downtown; and LOS D for all side street movements at unsignalized intersections.

The following significance criteria which the City has used in other traffic studies was also considered in this analysis:

A project would normally have a significant adverse impact on the environment if it would cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., results in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads or delays at intersections), or change the condition of an existing street (i.e., street closures, changing direction of travel) in a manner that would substantially affect access or traffic load and capacity of the street system. The specific City of Sebastopol criteria utilized for this analysis are as follows:

A project-related or cumulative traffic impact is considered to be significant if the proposed project would do any of the following:

- *Cause the existing baseline LOS to degrade to worse than LOS D at any signalized intersection within the Downtown; or,*
- *Cause the existing baseline LOS to degrade to worse than LOS C at any signalized intersection outside of the Downtown.*

The City of Sebastopol does not have an adopted threshold of significance for project-related impacts at intersections that are already operating, or projected to operate, at unacceptable LOS under Existing or Cumulative Conditions without the addition of any project-related traffic.

Therefore, for the purpose of such studies to determine whether a project-related impact would be significant, the following criteria have been utilized in other studies in the City of Sebastopol. Similar criteria are utilized within other jurisdictions such as the City of Napa, City of Santa Rosa, City of San Francisco, and the City of Oakland:

- *A project impact is considered significant if the proposed project would cause the average control delay at any signalized intersections to increase by five (5.0) seconds or more for intersections already operating at unacceptable LOS E or LOS F under the no project conditions.*

Existing Conditions

The Existing Conditions scenario provides an evaluation of current operation based on existing traffic volumes during the a.m. and p.m. peak periods. This condition does not include project-generated traffic volumes. Volume data was collected in December 2018 while local schools were in session.

Intersection Levels of Service

Under existing conditions, all study intersections are operating at LOS D or better. The existing traffic volumes are shown in Figure 1. A summary of the intersection level of service calculations is contained in Table 4, and copies are provided in Appendix B.

Table 4 – Existing Peak Hour Intersection Levels of Service

Study Intersection	AM Peak		PM Peak	
	Delay	LOS	Delay	LOS
1. Sebastopol Ave (SR 12)/Morris St	16.7	B	25.0	C
2. Sebastopol Ave (SR 12)/Petaluma Ave (SR 116)	24.8	C	26.1	C
3. Bodega Ave-SR 12/N Main St-S Main St (SR 116)	52.2	D	53.3	D
4. N Main St (SR 116)/McKinley St	6.2	A	8.4	A

Notes: Delay is measured in average seconds per vehicle; LOS = Level of Service

Future Conditions

The City of Sebastopol is working with Caltrans to improve signal timing at multiple intersections along SR 12 and SR 116 within the downtown core. Signal coordination at several of the studied intersections is being considered as an improvement, along with re-evaluating the existing cycle lengths. All four of the intersections included in this analysis are part of the signal improvement study. Since the planned improvements to signal timing at these locations have yet to be determined and the planned project is consistent with the site's land use designation and zoning, so reasonably anticipated in planning studies for future conditions, a future conditions scenario was not analyzed.

Project Description

The proposed project includes 18 two-bedroom townhomes to be built on a currently vacant lot, with one carport and one visitor parking space per unit. The site would be accessible where Morris Street terminates south of Sebastopol Avenue, with an additional emergency access point via Village Park. The project site plan is shown in Figure 2.

Trip Generation

The anticipated trip generation for the proposed project was estimated using standard rates published by the Institute of Transportation Engineers (ITE) in *Trip Generation Manual*, 10th Edition, 2017, for Multifamily Housing Low-Rise (ITE LU #220), as the description most closely matches the proposed project. The proposed project is expected to generate an average of 132 vehicle trips per day, including 8 trips during the a.m. peak hour and 10 during the p.m. peak hour. These results are summarized in Table 5.

Table 5 – Trip Generation Summary

Land Use	Units	Daily		AM Peak Hour				PM Peak Hour			
		Rate	Trips	Rate	Trips	In	Out	Rate	Trips	In	Out
Proposed											
Multifamily Housing	18 units	7.32	132	0.46	8	2	6	0.56	10	6	4

Trip Distribution

The pattern used to allocate new project trips to the street network was determined by reviewing existing turning movements at the study intersections as well as employment patterns for residents of the City of Sebastopol as indicated by the 2010 Census. Since traffic conditions are generally most critical during the weekday p.m. peak

LEGEND / ABBREVIATIONS

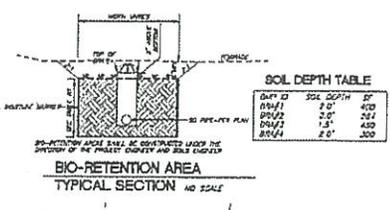
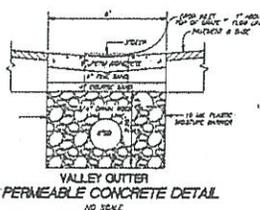
- EXISTING FEATURES SHOWN AS REFERRED
- BOUNDARY LINE
 - CONTOUR LINE
 - POSSIBLE LINE (TYPE WALL)
 - EDGE OF PAVEMENT
 - TREE SWALE
 - BLADE LINE
 - STAIN DRAIN
 - 6" SANITARY SEWER
 - SE LATERAL
 - SHADE SWALE
 - WETLAND
 - POSSIBLE PAVEMENT
 - NEW CONCRETE

TREE ABBREVIATIONS

- AC ACORN
- ALB ALBANY
- AM AMERICAN
- AN ANON
- AR ARIZONA
- AS ASH
- BA BALSAM
- BE BEECH
- BI BIRCH
- BU BURLINGAME
- CA CAJON
- CH CHERRY
- CO COCONUT PALM
- CR CRABAPPLE
- CU CUMMINS
- DA DAHLIA
- DE DEODAR
- DI DIOONIA
- DR DREW
- DU DUNE
- EA EASTERN
- EL ELDER
- EM EMERALD
- EU EUROPEAN
- FA FERN
- FL FLORIDA
- FR FRUIT
- GA GAULMUR
- GL GUM
- GR GRASS
- GU GUAVA
- HA HAWAIIAN
- HE HEAVY
- HO HONEY
- HU HUNGARIAN
- IA ILLINOIS
- IB IBRAHIM
- IC ICELANDIC
- IL ILLINOIS
- IN INDIAN
- IR IRON
- IS ISLANDIA
- JA JACOB
- KA KAWAIA
- KE KEWEE
- LA LAUREL
- LI LIPTON
- LU LUCID
- MA MAHOGANY
- MC MICHIGAN
- MD MARYLAND
- ME MEXICAN
- MI MICHIGAN
- MO MONTANA
- MS MISSISSIPPI
- MT MONTANA
- NA NORTHERN
- NE NEVADA
- NI NORTHERN
- NO NORTHERN
- NU NORTHERN
- NY NEW YORK
- OH OHIO
- OR OREGON
- PA PENNSYLVANIA
- PE PEACH
- PI PINE
- PL PLUM
- PO PINE
- PR PRUNELLA
- PS PINE
- PT PORTULACA
- RA RAHMAN
- RE REED
- RI RHODODENDRON
- RO ROSE
- RU RUSSIAN
- SA SANTA
- SC SCOTLAND
- SE SEASIDE
- SH SHALICE
- SI SIERRA
- SO SOUTH
- SP SPANISH
- ST STAMEN
- TA TANG
- TE TEXAS
- TH THORN
- TI TIBETAN
- TO TONGA
- TR TRINIDAD
- TS TULSA
- TT TULSA
- VA VIRGINIA
- VE VENEZUELA
- VI VIETNAM
- VO VOYAGER
- WA WASHINGTON
- WE WISCONSIN
- WI WISCONSIN
- WO WOOD
- WY WYOMING
- ZA ZEPHYRUS

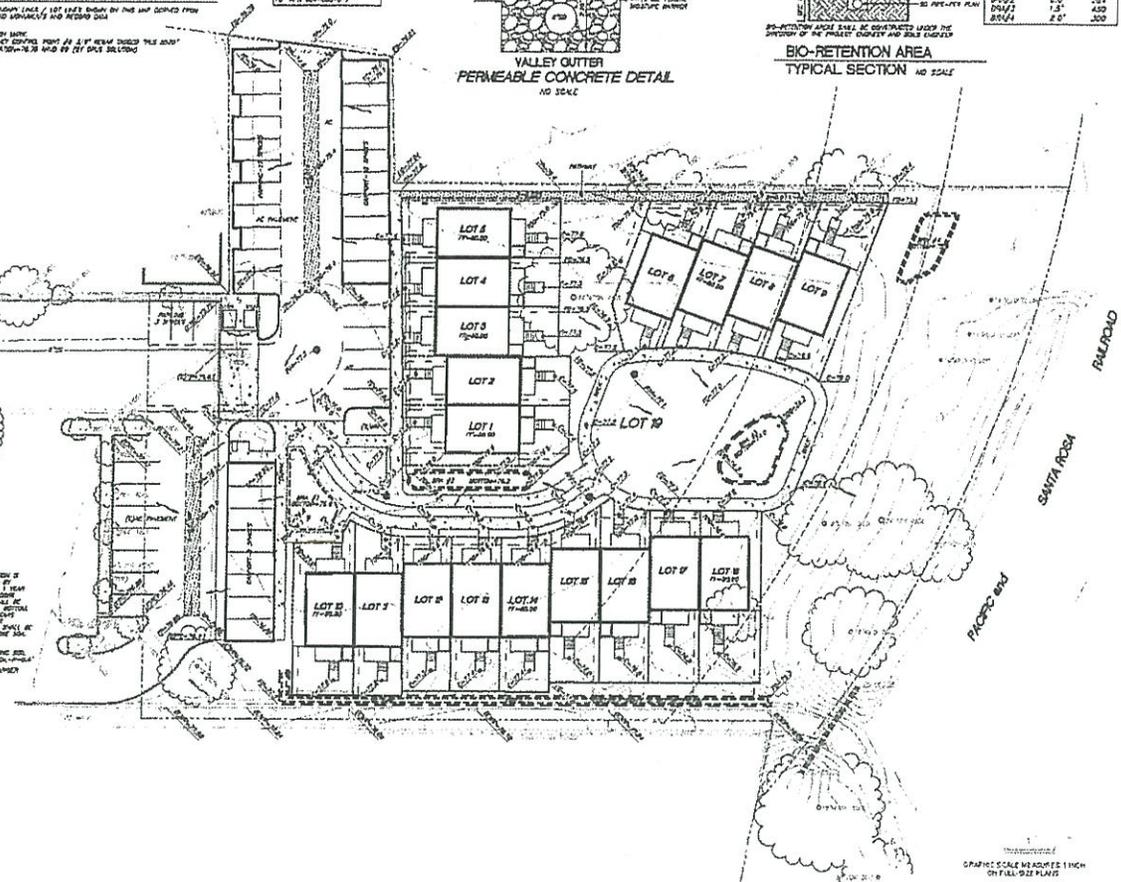
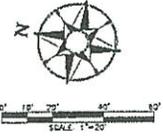
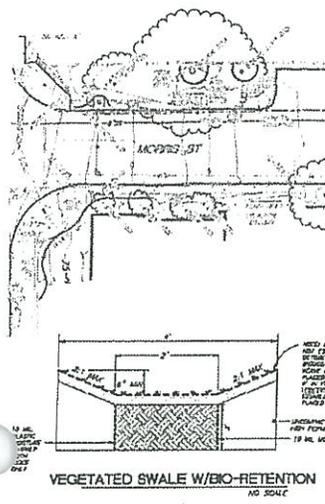
NOTES

- 1. ALL NEW CONCRETE SHALL BE 4000 PSI STRENGTH CONCRETE WITH 4% AIR ENTRAINMENT.
- 2. ALL NEW CONCRETE SHALL BE FINISHED WITH A BROOM FINISH.
- 3. ALL NEW CONCRETE SHALL BE CURIED AND MOISTURED FOR 7 DAYS.
- 4. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING CURING.
- 5. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING CONSTRUCTION.
- 6. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING DEMOLITION.
- 7. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING REPAIRS.
- 8. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING MAINTENANCE.
- 9. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING STORAGE.
- 10. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING TRANSPORT.
- 11. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING INSTALLATION.
- 12. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING OPERATION.
- 13. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING DECOMMISSIONING.
- 14. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING DISPOSAL.
- 15. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING RECYCLING.
- 16. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING REUSE.
- 17. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING REPAIRS.
- 18. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING MAINTENANCE.
- 19. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING STORAGE.
- 20. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING TRANSPORT.
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- 24. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING DISPOSAL.
- 25. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING RECYCLING.
- 26. ALL NEW CONCRETE SHALL BE PROTECTED FROM DAMAGE DURING REUSE.



SOIL DEPTH TABLE

SOIL TYPE	SOIL DEPTH (FT)	STRENGTH (PSF)
SOIL 1	2.0'	400
SOIL 2	2.0'	200
SOIL 3	1.5'	100
SOIL 4	2.0'	100



DRAWN	GG
CHECK	J.D.
APPROVED	BAT
DATE	06-13-2016
JOB NUMBER	9054.00
SHEET	2 OF 3

DAVIS TOWNHOMES
GRADING, DRAINAGE and STORM WATER TREATMENT

DANIEL DAVIS
1051 TODD ROAD, SANTA ROSA, CA. 95407

NO.	DESCRIPTION / REVISION	BY	CHK.	DATE

LACO
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hour, the distribution assumptions are primarily based on the expected trip routes during that time. The applied distribution assumptions and resulting trips are shown in Table 6.

Route	Percent	Daily Trips	AM Trips	PM Trips
US 101 (to/from the east)	35%	46	3	4
SR 116 (to/from the north)	15%	20	1	2
SR 116 (to/from the south)	35%	46	3	4
Sonoma Coast (to/from the west)	15%	20	1	2
TOTAL	100%	132	8	10*

Note: * Trips do not add up to the calculated trip generation due to rounding

Intersection Operation

Existing plus Project Conditions

Upon the addition of project-related traffic to the Existing volumes, the study intersections are expected to operate at the same service levels as without project-related traffic. These results are summarized in Table 7. Project only traffic volumes and Existing plus Project traffic volumes are shown in Figure 3.

Study Intersection	Existing Conditions				Existing plus Project			
	AM Peak		PM Peak		AM Peak		PM Peak	
	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS
1. Sebastopol Ave (SR 12)/Morris St	16.7	B	25.0	C	19.9	B	26.7	C
2. Sebastopol Ave (SR 12)/Petaluma Ave (SR 116)	24.8	C	26.1	C	24.9	C	26.2	C
3. Bodega Ave-SR 12/N Main St-S Main St (SR 116)	52.2	D	53.3	D	52.6	D	53.6	D
4. N Main St (SR 116)/McKinley St	6.2	A	8.4	A	6.2	A	8.3	A

Notes: Delay is measured in average seconds per vehicle; LOS = Level of Service

It should be noted that the level of service and delay at the intersection of Sebastopol Avenue (SR 12)/Morris Street can fluctuate with periods of more congestion due to heavy east-west traffic. The project only increases the delay by approximately three seconds during the a.m. peak hour and two seconds during the p.m. peak hour. The impact of this level of delay increase is considered less-than-significant. However, because delay generated by the southern leg could impact delay and congestion during the peak congestion periods, it would be appropriate to provide measures to reduce this impact.

As noted earlier in the report, the City is working with Caltrans to improve signal timing at multiple intersections along SR 12 including the Morris Street intersection. Based on the preliminary evaluation, recommended signal timing and resulting simulations with the updated timing plan, the intersection of Sebastopol Avenue (SR 12)/Morris Street will experience a decreased delay of approximately 5 seconds which would offset the delay created by the project. In order to provide an additional layer of reduced traffic impact, the southbound approach should be restriped from the current geometrics (18 ft. inbound lane, 12 ft. outbound lane) to two 15-foot lanes. This restriping which may require re-installation of the traffic signal detector position, will periodically allow southbound right-turn traffic to make a right on red when there is one car queued up waiting to make a left turn.

Finding – The study intersections are expected to continue operating acceptably and at the same Levels of Service upon the addition of project-generated traffic. The project’s short-term impact is therefore considered less-than-significant. However, due to the particular sensitivity of the SR 12/Morris Street intersection to congestion, the project should restripe the southbound Morris Street approach.

Recommendation – The project should arrange for restriping of the southbound Morris Street approach to create two 15-foot lanes and, if needed, re-install the traffic signal detector position.

Alternative Modes

Given the proximity to Sebastopol's downtown, the Barlow district, the Laguna open space, and transit stops surrounding the site, it is reasonable to assume that some residents would want to walk, bicycle, and/or use transit to travel to and from the project site.

Pedestrian Facilities

Sebastopol Avenue connects to the Barlow District and the downtown from the east entrance point into the City of Sebastopol. Morris Street connects neighborhoods to the Barlow District, Analy High School, the Community Center, and the Laguna Skate Park. Both roads would serve as the primary path of travel for residential pedestrian activity. Sidewalk connectivity between the project site and destinations surrounding the project site is generally adequate; however, the sidewalk is discontinuous on the west side of Morris Street north of Laguna Park Way.

Based on the proposed site plan, there is not a paved walkway/sidewalk connection between the Forest Park Trail and the townhomes and Morris Street. The Forest Park Trail is adjacent to the project site, which provides connection to the Joe Rodota Trail south of project site.

Finding – Pedestrian facilities surrounding the proposed site are generally adequate to provide connection the downtown and transit stops. There is not an existing or planned connection between the site and the Forest Park Trail.

Recommendation – The proposed project should provide pedestrian facilities between the townhomes and the Forest Park Trail.

Bicycle Facilities

Existing and planned bicycle facilities, per the *City of Sebastopol Bicycle and Pedestrian Master Plan*, will provide access for bicyclists. The City of Sebastopol Ordinance 17.110.030 requires residential uses to provide one-half bicycle parking space per proposed multifamily and attached single-family dwelling unit. Proper bicycle parking facilities are also required; each bicycle parking space shall include a stationary parking device to adequately secure bicycles.

Based on the 18 proposed multifamily dwelling units, the project is required to provide a minimum of nine bicycle parking spaces. According to the proposed project's site plan, each unit is planned to have an exterior rear porch closet with a bicycle hook to provide bicycle parking.

Finding – Bicycle facilities serving the project site are adequate. There are plans for bicycle parking for each unit.

Transit

Existing transit routes are adequate to accommodate project-generated transit trips. Existing stops are within acceptable walking distance of the site.

Finding – Transit facilities serving the project site are expected to be adequate.

Conclusions and Recommendations

Conclusions

- The proposed project is expected to generate an average of 132 daily vehicle trips, including 8 trips during the morning peak hour and 10 trips during the evening peak hour.
- The study intersections are currently operating acceptably at LOS D or better overall during both peak hours. With anticipated project related traffic added, the intersections are expected to continue operating at the same service levels as without project trips.
- With recommended signal timing modifications at the SR 12/Morris Street intersection, the intersection will experience a decreased delay of approximately 5 seconds which would offset the delay created by the project.
- Due to the particular sensitivity of the SR 12/Morris Street intersection to congestion, the project should restripe the southbound Morris Street approach to allow for vehicles to make a right-turn on red.
- Existing pedestrian, bicycle, and transit facilities are generally adequate to serve the project site. Pedestrian connectivity between the Forest Park Trail and the project site is lacking and should be considered for improvement.

Recommendations

- The project should arrange for restriping of the southbound Morris Street approach to create two 15-foot lanes and, if needed, re-install the traffic signal detector position.
- The proposed should provide a pedestrian facility connection between the project site and the Forest Park Trail.

Study Participants and References

Study Participants

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Assistant Planner	Julia Walker
Graphics	Katia Wolfe
Editing/Formatting	Alex Scrobonia
Report Review	Dalene J. Whitlock, PE, PTOE

References

2014 Collision Data on California State Highways, California Department of Transportation, 2017
City of Sebastopol Bicycle and Pedestrian Master Plan, Sonoma County Transportation Authority, 2011
City of Sebastopol General Plan, City of Sebastopol, 2008
Highway Capacity Manual, Transportation Research Board, 2000
Highway Design Manual, 6th Edition, California Department of Transportation, 2017
Statewide Integrated Traffic Records System (SWITRS), California Highway Patrol, 2014-2018
Trip Generation Manual, 10th Edition, Institute of Transportation Engineers, 2017

SEB065



Appendix A

Collision Rate Calculations

Intersection Collision Rate Calculations

Davis Townhomes

Intersection # 1: Sebastopol Ave (SR 12) & Morris St

Date of Count: Wednesday, December 5, 2018

Number of Collisions: 5
Number of Injuries: 3
Number of Fatalities: 0
ADT: 18900
Start Date: January 1, 2014
End Date: December 31, 2018
Number of Years: 5

Intersection Type: Four-Legged
Control Type: Signals
Area: Urban

$$\text{collision rate} = \frac{\text{Number of Collisions} \times 1 \text{ Million}}{\text{ADT} \times 365 \text{ Days per Year} \times \text{Number of Years}}$$

$$\text{collision rate} = \frac{5}{18,900} \times \frac{1,000,000}{365 \times 5}$$

	Collision Rate	Fatality Rate	Injury Rate
Study Intersection	0.14 c/mve	0.0%	60.0%
Statewide Average*	0.27 c/mve	0.4%	41.9%

ADT = average daily total vehicles entering intersection
 c/mve = collisions per million vehicles entering intersection
 * 2013 Collision Data on California State Highways, Caltrans

Intersection # 2: Sebastopol Ave (SR 12) & Petaluma Ave (SR 116)

Date of Count: Wednesday, December 5, 2018

Number of Collisions: 12
Number of Injuries: 6
Number of Fatalities: 0
ADT: 24900
Start Date: January 1, 2014
End Date: December 31, 2018
Number of Years: 5

Intersection Type: Four-Legged
Control Type: Signals
Area: Urban

$$\text{collision rate} = \frac{\text{Number of Collisions} \times 1 \text{ Million}}{\text{ADT} \times 365 \text{ Days per Year} \times \text{Number of Years}}$$

$$\text{collision rate} = \frac{12}{24,900} \times \frac{1,000,000}{365 \times 5}$$

	Collision Rate	Fatality Rate	Injury Rate
Study Intersection	0.26 c/mve	0.0%	50.0%
Statewide Average*	0.27 c/mve	0.4%	41.9%

ADT = average daily total vehicles entering intersection
 c/mve = collisions per million vehicles entering intersection
 * 2013 Collision Data on California State Highways, Caltrans

Intersection Collision Rate Calculations

Davis Townhomes

Intersection # 3: Bodega Ave-SR 12 & North Main St-South Main St (SR 116)
 Date of Count: Wednesday, December 5, 2018

Number of Collisions: 6
 Number of Injuries: 2
 Number of Fatalities: 0
 ADT: 21700
 Start Date: January 1, 2014
 End Date: December 31, 2018
 Number of Years: 5

Intersection Type: Four-Legged
 Control Type: Signals
 Area: Urban

$$\text{collision rate} = \frac{\text{Number of Collisions} \times 1 \text{ Million}}{\text{ADT} \times 365 \text{ Days per Year} \times \text{Number of Years}}$$

$$\text{collision rate} = \frac{6}{21,700} \times \frac{1,000,000}{365 \times 5}$$

	Collision Rate	Fatality Rate	Injury Rate
Study Intersection	0.15 c/mve	0.0%	33.3%
Statewide Average*	0.27 c/mve	0.4%	41.9%

ADT = average daily total vehicles entering intersection
 c/mve = collisions per million vehicles entering intersection
 * 2013 Collision Data on California State Highways, Caltrans

Intersection # 4: North Main St & McKinley St
 Date of Count: Wednesday, December 5, 2018

Number of Collisions: 1
 Number of Injuries: 1
 Number of Fatalities: 0
 ADT: 17700
 Start Date: January 1, 2014
 End Date: December 31, 2018
 Number of Years: 5

Intersection Type: Tee
 Control Type: Signals
 Area: Urban

$$\text{collision rate} = \frac{\text{Number of Collisions} \times 1 \text{ Million}}{\text{ADT} \times 365 \text{ Days per Year} \times \text{Number of Years}}$$

$$\text{collision rate} = \frac{1}{17,700} \times \frac{1,000,000}{365 \times 5}$$

	Collision Rate	Fatality Rate	Injury Rate
Study Intersection	0.03 c/mve	0.0%	100.0%
Statewide Average*	0.21 c/mve	0.3%	42.4%

ADT = average daily total vehicles entering intersection
 c/mve = collisions per million vehicles entering intersection
 * 2013 Collision Data on California State Highways, Caltrans

Appendix B

Intersection Level of Service Calculations

HCM Signalized Intersection Capacity Analysis

1: Sebastopol Rd & Morris St

02/05/2019

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	38	893	6	0	783	135	3	1	0	76	0	34
Future Volume (vph)	38	893	6	0	783	135	3	1	0	76	0	34
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700
Total Lost time (s)	4.7	5.1			5.1	5.1		4.7		4.7	4.7	
Lane Util. Factor	1.00	1.00			1.00	1.00		1.00		1.00	1.00	
Frbp, ped/bikes	1.00	1.00			1.00	0.98		1.00		1.00	0.95	
Flpb, ped/bikes	1.00	1.00			1.00	1.00		1.00		1.00	1.00	
Frt	1.00	1.00			1.00	0.85		1.00		1.00	0.85	
Flt Protected	0.95	1.00			1.00	1.00		0.96		0.95	1.00	
Satd. Flow (prot)	1615	1698			1700	1410		1639		1615	1375	
Flt Permitted	0.95	1.00			1.00	1.00		0.96		0.95	1.00	
Satd. Flow (perm)	1615	1698			1700	1410		1639		1615	1375	
Peak-hour factor, PHF	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Adj. Flow (vph)	40	940	6	0	824	142	3	1	0	80	0	36
RTOR Reduction (vph)	0	0	0	0	0	16	0	0	0	0	0	0
Lane Group Flow (vph)	40	946	0	0	824	126	0	4	0	80	36	0
Confl. Peds. (#/hr)			11			2			3			10
Confl. Bikes (#/hr)									1			
Heavy Vehicles (%)	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
Turn Type	Prot	NA		Prot	NA	Perm	Split	NA		Split	NA	
Protected Phases	5	2		1	6		8	8		4	4	
Permitted Phases						6						
Actuated Green, G (s)	6.0	67.6			56.9	56.9		1.6		9.9	9.9	
Effective Green, g (s)	6.0	67.6			56.9	56.9		1.6		9.9	9.9	
Actuated g/C Ratio	0.06	0.72			0.61	0.61		0.02		0.11	0.11	
Clearance Time (s)	4.7	5.1			5.1	5.1		4.7		4.7	4.7	
Vehicle Extension (s)	3.0	5.0			5.0	5.0		3.0		3.0	3.0	
Lane Grp Cap (vph)	103	1226			1033	857		28		170	145	
v/s Ratio Prot	0.02	c0.56			0.48			c0.00		c0.05	0.03	
v/s Ratio Perm						0.09						
v/c Ratio	0.39	0.77			0.80	0.15		0.14		0.47	0.25	
Uniform Delay, d1	42.0	8.2			14.0	7.9		45.3		39.4	38.4	
Progression Factor	1.00	1.00			1.00	1.00		1.00		1.00	1.00	
Incremental Delay, d2	2.4	3.6			5.0	0.2		2.3		2.1	0.9	
Delay (s)	44.5	11.8			19.0	8.1		47.7		41.4	39.3	
Level of Service	D	B			B	A		D		D	D	
Approach Delay (s)		13.1			17.4			47.7			40.8	
Approach LOS		B			B			D			D	

Intersection Summary			
HCM 2000 Control Delay	16.7	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.77		
Actuated Cycle Length (s)	93.6	Sum of lost time (s)	19.2
Intersection Capacity Utilization	70.9%	ICU Level of Service	C
Analysis Period (min)	15		
c Critical Lane Group			

HCM Signalized Intersection Capacity Analysis

2: Sebastopol Rd & Petaluma Ave

02/05/2019

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	59	537	0	0	664	139	94	478	461	0	0	0
Future Volume (vph)	59	537	0	0	664	139	94	478	461	0	0	0
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700
Total Lost time (s)	4.7	5.0			5.0			4.6	4.6			
Lane Util. Factor	1.00	1.00			0.95			0.95	1.00			
Frpb, ped/bikes	1.00	1.00			1.00			1.00	0.97			
Flpb, ped/bikes	1.00	1.00			1.00			1.00	1.00			
Frt	1.00	1.00			0.97			1.00	0.85			
Flt Protected	0.95	1.00			1.00			0.99	1.00			
Satd. Flow (prot)	1615	1700			3138			3203	1408			
Flt Permitted	0.95	1.00			1.00			0.99	1.00			
Satd. Flow (perm)	1615	1700			3138			3203	1408			
Peak-hour factor, PHF	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Adj. Flow (vph)	61	554	0	0	685	143	97	493	475	0	0	0
RTOR Reduction (vph)	0	0	0	0	13	0	0	0	24	0	0	0
Lane Group Flow (vph)	61	554	0	0	815	0	0	590	451	0	0	0
Confl. Peds. (#/hr)							3	1	2			
Confl. Bikes (#/hr)									2			
Heavy Vehicles (%)	0%	0%	2%	2%	0%	0%	0%	0%	0%	2%	2%	2%
Turn Type	Prot	NA			NA		Perm	NA	Perm			
Protected Phases	5	2			6			8				
Permitted Phases							8		8			
Actuated Green, G (s)	6.9	43.7			32.1			36.2	36.2			
Effective Green, g (s)	6.9	43.7			32.1			36.2	36.2			
Actuated g/C Ratio	0.08	0.49			0.36			0.40	0.40			
Clearance Time (s)	4.7	5.0			5.0			4.6	4.6			
Vehicle Extension (s)	2.0	3.0			4.0			3.5	3.5			
Lane Grp Cap (vph)	124	830			1125			1295	569			
v/s Ratio Prot	0.04	c0.33			c0.26							
v/s Ratio Perm								0.18	c0.32			
v/c Ratio	0.49	0.67			0.72			0.46	0.79			
Uniform Delay, d1	39.6	17.4			24.9			19.5	23.4			
Progression Factor	1.00	1.00			1.00			1.00	1.00			
Incremental Delay, d2	1.1	2.0			2.5			0.3	7.7			
Delay (s)	40.7	19.4			27.4			19.8	31.0			
Level of Service	D	B			C			B	C			
Approach Delay (s)		21.5			27.4			24.8			0.0	
Approach LOS		C			C			C			A	

Intersection Summary

HCM 2000 Control Delay	24.8	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.77		
Actuated Cycle Length (s)	89.5	Sum of lost time (s)	14.3
Intersection Capacity Utilization	94.9%	ICU Level of Service	F
Analysis Period (min)	15		

c Critical Lane Group

HCM Signalized Intersection Capacity Analysis 3: Bodega Ave/Sebastopol Rd & Main St

02/05/2019

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑	↗	↘	↑					↘	↑↗	
Traffic Volume (vph)	0	337	52	340	434	0	0	0	0	255	533	50
Future Volume (vph)	0	337	52	340	434	0	0	0	0	255	533	50
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700
Total Lost time (s)		4.7	4.7	11.7	11.7					4.7	4.7	
Lane Util. Factor		1.00	1.00	1.00	1.00					1.00	0.95	
Frbp, ped/bikes		1.00	0.96	1.00	1.00					1.00	0.99	
Flpb, ped/bikes		1.00	1.00	1.00	1.00					0.99	1.00	
Frt		1.00	0.85	1.00	1.00					1.00	0.99	
Flt Protected		1.00	1.00	0.95	1.00					0.95	1.00	
Satd. Flow (prot)		1700	1391	1615	1700					1596	3167	
Flt Permitted		1.00	1.00	0.95	1.00					0.95	1.00	
Satd. Flow (perm)		1700	1391	1615	1700					1596	3167	
Peak-hour factor, PHF	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Adj. Flow (vph)	0	359	55	362	462	0	0	0	0	271	567	53
RTOR Reduction (vph)	0	0	4	0	0	0	0	0	0	0	7	0
Lane Group Flow (vph)	0	359	51	362	462	0	0	0	0	271	613	0
Confl. Peds. (#/hr)			17							5		25
Confl. Bikes (#/hr)												1
Heavy Vehicles (%)	2%	0%	0%	0%	0%	2%	2%	2%	2%	0%	0%	0%
Turn Type		NA	Perm	Prot	NA					Perm	NA	
Protected Phases		4		3	8						2	
Permitted Phases			4							2		
Actuated Green, G (s)		16.5	16.5	16.4	37.6					24.5	24.5	
Effective Green, g (s)		16.5	16.5	16.4	37.6					24.5	24.5	
Actuated g/C Ratio		0.20	0.20	0.20	0.46					0.30	0.30	
Clearance Time (s)		4.7	4.7	11.7	11.7					4.7	4.7	
Vehicle Extension (s)		3.0	3.0	3.0	5.0					4.0	4.0	
Lane Grp Cap (vph)		342	279	323	779					476	946	
v/s Ratio Prot		c0.21		c0.22	0.27						c0.19	
v/s Ratio Perm			0.04							0.17		
v/c Ratio		1.05	0.18	1.12	0.59					0.57	0.65	
Uniform Delay, d1		32.8	27.2	32.8	16.5					24.3	25.0	
Progression Factor		1.00	1.00	1.00	1.00					1.00	1.00	
Incremental Delay, d2		62.3	0.3	86.7	1.8					1.9	1.7	
Delay (s)		95.0	27.5	119.5	18.3					26.2	26.7	
Level of Service		F	C	F	B					C	C	
Approach Delay (s)		86.1			62.8			0.0			26.6	
Approach LOS		F			E			A			C	
Intersection Summary												
HCM 2000 Control Delay			52.2			HCM 2000 Level of Service				D		
HCM 2000 Volume to Capacity ratio			0.88									
Actuated Cycle Length (s)			82.0			Sum of lost time (s)				23.1		
Intersection Capacity Utilization			94.9%			ICU Level of Service				F		
Analysis Period (min)			15									
c Critical Lane Group												

HCM Signalized Intersection Capacity Analysis

4: Main St & McKinley St

02/05/2019



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↵	↶				↕
Traffic Volume (vph)	72	558	0	0	0	782
Future Volume (vph)	72	558	0	0	0	782
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700
Total Lost time (s)	4.7	4.7				4.7
Lane Util. Factor	1.00	1.00				0.95
Frbp, ped/bikes	1.00	1.00				1.00
Flpb, ped/bikes	1.00	1.00				1.00
Frt	1.00	0.85				1.00
Flt Protected	0.95	1.00				1.00
Satd. Flow (prot)	1615	1445				3230
Flt Permitted	0.95	1.00				1.00
Satd. Flow (perm)	1615	1445				3230
Peak-hour factor, PHF	0.95	0.95	0.95	0.95	0.95	0.95
Adj. Flow (vph)	76	587	0	0	0	823
RTOR Reduction (vph)	58	0	0	0	0	0
Lane Group Flow (vph)	18	587	0	0	0	823
Confl. Peds. (#/hr)	18	6				
Confl. Bikes (#/hr)		1				
Heavy Vehicles (%)	0%	0%	2%	2%	2%	0%
Turn Type	Prot	custom				NA
Protected Phases	7	2 6 7				2
Permitted Phases						
Actuated Green, G (s)	14.5	51.8				32.6
Effective Green, g (s)	14.5	51.8				32.6
Actuated g/C Ratio	0.24	0.86				0.54
Clearance Time (s)	4.7					4.7
Vehicle Extension (s)	2.0					4.0
Lane Grp Cap (vph)	390	1247				1754
v/s Ratio Prot	0.01	c0.41				0.25
v/s Ratio Perm						
v/c Ratio	0.05	0.47				0.47
Uniform Delay, d1	17.5	0.9				8.4
Progression Factor	1.00	1.00				1.00
Incremental Delay, d2	0.0	0.4				0.3
Delay (s)	17.5	1.3				8.7
Level of Service	B	A				A
Approach Delay (s)	3.2		0.0			8.7
Approach LOS	A		A			A

Intersection Summary			
HCM 2000 Control Delay	6.2	HCM 2000 Level of Service	A
HCM 2000 Volume to Capacity ratio	0.52		
Actuated Cycle Length (s)	60.0	Sum of lost time (s)	13.4
Intersection Capacity Utilization	43.2%	ICU Level of Service	A
Analysis Period (min)	15		
c Critical Lane Group			

HCM Signalized Intersection Capacity Analysis

1: Sebastopol Rd & Morris St

02/05/2019

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	49	735	2	3	705	170	3	3	5	162	1	52
Future Volume (vph)	49	735	2	3	705	170	3	3	5	162	1	52
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700
Total Lost time (s)	4.7	5.1		4.7	5.1	5.1		4.7		4.7	4.7	
Lane Util. Factor	1.00	1.00		1.00	1.00	1.00		1.00		1.00	1.00	
Frbp, ped/bikes	1.00	1.00		1.00	1.00	0.98		0.96		1.00	0.94	
Flpb, ped/bikes	1.00	1.00		1.00	1.00	1.00		1.00		1.00	1.00	
Frt	1.00	1.00		1.00	1.00	0.85		0.94		1.00	0.85	
Flt Protected	0.95	1.00		0.95	1.00	1.00		0.99		0.95	1.00	
Satd. Flow (prot)	1583	1666		1583	1667	1385		1480		1583	1329	
Flt Permitted	0.95	1.00		0.95	1.00	1.00		0.99		0.95	1.00	
Satd. Flow (perm)	1583	1666		1583	1667	1385		1480		1583	1329	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	53	799	2	3	766	185	3	3	5	176	1	57
RTOR Reduction (vph)	0	0	0	0	0	9	0	5	0	0	0	0
Lane Group Flow (vph)	53	801	0	3	766	176	0	6	0	176	58	0
Confl. Peds. (#/hr)			20			1						14
Confl. Bikes (#/hr)			2						3			1
Turn Type	Prot	NA		Prot	NA	Perm	Split	NA		Split	NA	
Protected Phases	5	2		1	6		8	8		4	4	
Permitted Phases						6						
Actuated Green, G (s)	8.7	63.3		1.6	56.2	56.2		1.6		16.3	16.3	
Effective Green, g (s)	8.7	63.3		1.6	56.2	56.2		1.6		16.3	16.3	
Actuated g/C Ratio	0.09	0.62		0.02	0.55	0.55		0.02		0.16	0.16	
Clearance Time (s)	4.7	5.1		4.7	5.1	5.1		4.7		4.7	4.7	
Vehicle Extension (s)	3.0	5.0		3.0	5.0	5.0		3.0		3.0	3.0	
Lane Grp Cap (vph)	135	1033		24	918	763		23		252	212	
v/s Ratio Prot	c0.03	c0.48		0.00	c0.46			c0.00		c0.11	0.04	
v/s Ratio Perm						0.13						
v/c Ratio	0.39	0.78		0.12	0.83	0.23		0.26		0.70	0.27	
Uniform Delay, d1	44.1	14.2		49.5	19.0	11.8		49.6		40.5	37.6	
Progression Factor	1.00	1.00		1.00	1.00	1.00		1.00		1.00	1.00	
Incremental Delay, d2	1.9	4.3		2.3	7.3	0.3		6.1		8.2	0.7	
Delay (s)	46.0	18.5		51.9	26.4	12.1		55.7		48.7	38.4	
Level of Service	D	B		D	C	B		E		D	D	
Approach Delay (s)		20.2			23.7			55.7			46.1	
Approach LOS		C			C			E			D	
Intersection Summary												
HCM 2000 Control Delay		25.0										
HCM 2000 Volume to Capacity ratio		0.77										
Actuated Cycle Length (s)		102.0						19.2				
Intersection Capacity Utilization		70.4%										
Analysis Period (min)		15										
c Critical Lane Group												

HCM Signalized Intersection Capacity Analysis

2: Sebastopol Rd & Petaluma Ave

02/05/2019

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	100	430	0	0	640	141	146	602	434	0	0	0
Future Volume (vph)	100	430	0	0	640	141	146	602	434	0	0	0
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700
Total Lost time (s)	4.7	5.0			5.0			4.6	4.6			
Lane Util. Factor	1.00	1.00			0.95			0.95	1.00			
Frpb, ped/bikes	1.00	1.00			1.00			1.00	0.97			
Flpb, ped/bikes	1.00	1.00			1.00			1.00	1.00			
Fr t	1.00	1.00			0.97			1.00	0.85			
Fl t Protected	0.95	1.00			1.00			0.99	1.00			
Satd. Flow (prot)	1583	1667			3070			3134	1368			
Fl t Permitted	0.95	1.00			1.00			0.99	1.00			
Satd. Flow (perm)	1583	1667			3070			3134	1368			
Peak-hour factor, PHF	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Adj. Flow (vph)	103	443	0	0	660	145	151	621	447	0	0	0
RTOR Reduction (vph)	0	0	0	0	13	0	0	0	30	0	0	0
Lane Group Flow (vph)	103	443	0	0	792	0	0	772	417	0	0	0
Confl. Peds. (#/hr)						7	3		7			
Confl. Bikes (#/hr)									1			
Turn Type	Prot	NA			NA		Perm	NA	Perm			
Protected Phases	5	2			6			8				
Permitted Phases							8		8			
Actuated Green, G (s)	7.9	45.3			32.7			36.5	36.5			
Effective Green, g (s)	7.9	45.3			32.7			36.5	36.5			
Actuated g/C Ratio	0.09	0.50			0.36			0.40	0.40			
Clearance Time (s)	4.7	5.0			5.0			4.6	4.6			
Vehicle Extension (s)	2.0	3.0			4.0			3.5	3.5			
Lane Grp Cap (vph)	136	826			1098			1251	546			
v/s Ratio Prot	c0.07	0.27			c0.26							
v/s Ratio Perm								0.25	c0.30			
v/c Ratio	0.76	0.54			0.72			0.62	0.76			
Uniform Delay, d1	40.8	15.8			25.4			21.9	23.7			
Progression Factor	1.00	1.00			1.00			1.00	1.00			
Incremental Delay, d2	19.0	0.7			2.5			1.0	6.5			
Delay (s)	59.8	16.5			27.9			22.8	30.2			
Level of Service	E	B			C			C	C			
Approach Delay (s)		24.7			27.9			25.5			0.0	
Approach LOS		C			C			C			A	

Intersection Summary

HCM 2000 Control Delay	26.1	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.74		
Actuated Cycle Length (s)	91.4	Sum of lost time (s)	14.3
Intersection Capacity Utilization	94.6%	ICU Level of Service	F
Analysis Period (min)	15		

c Critical Lane Group

HCM Signalized Intersection Capacity Analysis

3: Bodega Ave/Sebastopol Rd & Main St

02/05/2019

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑	↗	↘	↑					↖	↑↓	
Traffic Volume (vph)	0	266	58	356	471	0	0	0	0	258	655	108
Future Volume (vph)	0	266	58	356	471	0	0	0	0	258	655	108
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700
Total Lost time (s)		4.7	4.7	11.7	11.7					4.7	4.7	
Lane Util. Factor		1.00	1.00	1.00	1.00					1.00	0.95	
Frbp, ped/bikes		1.00	0.86	1.00	1.00					1.00	0.96	
Flpb, ped/bikes		1.00	1.00	1.00	1.00					0.89	1.00	
Frt		1.00	0.85	1.00	1.00					1.00	0.98	
Flt Protected		1.00	1.00	0.95	1.00					0.95	1.00	
Satd. Flow (prot)		1667	1212	1583	1667					1412	2979	
Flt Permitted		1.00	1.00	0.95	1.00					0.95	1.00	
Satd. Flow (perm)		1667	1212	1583	1667					1412	2979	
Peak-hour factor, PHF	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Adj. Flow (vph)	0	277	60	371	491	0	0	0	0	269	682	112
RTOR Reduction (vph)	0	0	0	0	0	0	0	0	0	0	0	0
Lane Group Flow (vph)	0	277	60	371	491	0	0	0	0	269	795	0
Confl. Peds. (#/hr)			82							42		99
Confl. Bikes (#/hr)												2
Turn Type		NA	Perm	Prot	NA					Perm	NA	
Protected Phases		4		3	8							2
Permitted Phases			4							2		
Actuated Green, G (s)		16.4	16.4	16.9	38.0					29.9	29.9	
Effective Green, g (s)		16.4	16.4	16.9	38.0					29.9	29.9	
Actuated g/C Ratio		0.19	0.19	0.19	0.43					0.34	0.34	
Clearance Time (s)		4.7	4.7	11.7	11.7					4.7	4.7	
Vehicle Extension (s)		3.0	3.0	3.0	5.0					4.0	4.0	
Lane Grp Cap (vph)		311	226	304	721					480	1014	
v/s Ratio Prot		c0.17		c0.23	0.29							c0.27
v/s Ratio Perm			0.05							0.19		
v/c Ratio		0.89	0.27	1.22	0.68					0.56	0.78	
Uniform Delay, d1		34.8	30.5	35.5	20.0					23.6	26.0	
Progression Factor		1.00	1.00	1.00	1.00					1.00	1.00	
Incremental Delay, d2		25.6	0.6	125.2	3.4					1.8	4.3	
Delay (s)		60.4	31.2	160.6	23.4					25.4	30.3	
Level of Service		E	C	F	C					C	C	
Approach Delay (s)		55.2			82.5			0.0			29.1	
Approach LOS		E			F			A			C	

Intersection Summary			
HCM 2000 Control Delay	53.3	HCM 2000 Level of Service	D
HCM 2000 Volume to Capacity ratio	0.91		
Actuated Cycle Length (s)	87.8	Sum of lost time (s)	23.1
Intersection Capacity Utilization	94.6%	ICU Level of Service	F
Analysis Period (min)	15		

c Critical Lane Group

HCM Signalized Intersection Capacity Analysis

4: Main St & McKinley St

02/05/2019



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↖	↗				↕
Traffic Volume (vph)	210	778	0	0	0	780
Future Volume (vph)	210	778	0	0	0	780
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700
Total Lost time (s)	4.7	4.7				4.7
Lane Util. Factor	1.00	1.00				0.95
Frbp, ped/bikes	1.00	1.00				1.00
Flpb, ped/bikes	1.00	1.00				1.00
Frt	1.00	0.85				1.00
Flt Protected	0.95	1.00				1.00
Satd. Flow (prot)	1583	1417				3167
Flt Permitted	0.95	1.00				1.00
Satd. Flow (perm)	1583	1417				3167
Peak-hour factor, PHF	0.97	0.97	0.97	0.97	0.97	0.97
Adj. Flow (vph)	216	802	0	0	0	804
RTOR Reduction (vph)	92	0	0	0	0	0
Lane Group Flow (vph)	124	802	0	0	0	804
Confl. Peds. (#/hr)	116	45				
Confl. Bikes (#/hr)		2				
Turn Type	Prot	custom				NA
Protected Phases	7	2 6 7				2
Permitted Phases		7				
Actuated Green, G (s)	17.7	49.6				27.2
Effective Green, g (s)	17.7	49.6				27.2
Actuated g/C Ratio	0.30	0.85				0.47
Clearance Time (s)	4.7					4.7
Vehicle Extension (s)	2.0					4.0
Lane Grp Cap (vph)	479	1203				1475
v/s Ratio Prot	0.08	c0.57				0.25
v/s Ratio Perm						
v/c Ratio	0.26	0.67				0.55
Uniform Delay, d1	15.4	1.5				11.2
Progression Factor	1.00	1.00				1.00
Incremental Delay, d2	0.1	1.6				0.5
Delay (s)	15.5	3.1				11.7
Level of Service	B	A				B
Approach Delay (s)	5.7		0.0			11.7
Approach LOS	A		A			B

Intersection Summary

HCM 2000 Control Delay	8.4	HCM 2000 Level of Service	A
HCM 2000 Volume to Capacity ratio	0.73		
Actuated Cycle Length (s)	58.4	Sum of lost time (s)	13.4
Intersection Capacity Utilization	61.4%	ICU Level of Service	B
Analysis Period (min)	15		

c Critical Lane Group

HCM Signalized Intersection Capacity Analysis

1: Sebastopol Rd & Morris St

02/05/2019

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	38	893	7	1	783	135	6	2	2	76	0	34
Future Volume (vph)	38	893	7	1	783	135	6	2	2	76	0	34
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700
Total Lost time (s)	4.7	5.1		4.7	5.1	5.1		4.7		4.7	4.7	
Lane Util. Factor	1.00	1.00		1.00	1.00	1.00		1.00		1.00	1.00	
Frbp, ped/bikes	1.00	1.00		1.00	1.00	0.98		0.97		1.00	0.95	
Flpb, ped/bikes	1.00	1.00		1.00	1.00	1.00		1.00		1.00	1.00	
Frt	1.00	1.00		1.00	1.00	0.85		0.97		1.00	0.85	
Flt Protected	0.95	1.00		0.95	1.00	1.00		0.97		0.95	1.00	
Satd. Flow (prot)	1615	1697		1615	1700	1410		1565		1615	1374	
Flt Permitted	0.95	1.00		0.95	1.00	1.00		0.97		0.95	1.00	
Satd. Flow (perm)	1615	1697		1615	1700	1410		1565		1615	1374	
Peak-hour factor, PHF	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Adj. Flow (vph)	40	940	7	1	824	142	6	2	2	80	0	36
RTOR Reduction (vph)	0	0	0	0	0	15	0	2	0	0	0	0
Lane Group Flow (vph)	40	947	0	1	824	127	0	8	0	80	36	0
Confl. Peds. (#/hr)			11			2			3			10
Confl. Bikes (#/hr)								1				
Heavy Vehicles (%)	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
Turn Type	Prot	NA		Prot	NA	Perm	Split	NA		Split	NA	
Protected Phases	5	2		1	6		8	8		4	4	
Permitted Phases						6						
Actuated Green, G (s)	6.0	63.1		1.6	58.7	58.7		1.6		9.9	9.9	
Effective Green, g (s)	6.0	63.1		1.6	58.7	58.7		1.6		9.9	9.9	
Actuated g/C Ratio	0.06	0.66		0.02	0.62	0.62		0.02		0.10	0.10	
Clearance Time (s)	4.7	5.1		4.7	5.1	5.1		4.7		4.7	4.7	
Vehicle Extension (s)	3.0	5.0		3.0	5.0	5.0		3.0		3.0	3.0	
Lane Grp Cap (vph)	101	1122		27	1046	867		26		167	142	
v/s Ratio Prot	c0.02	c0.56		0.00	0.48			c0.01		c0.05	0.03	
v/s Ratio Perm						0.09						
v/c Ratio	0.40	0.84		0.04	0.79	0.15		0.31		0.48	0.25	
Uniform Delay, d1	43.0	12.4		46.1	13.7	7.8		46.4		40.3	39.3	
Progression Factor	1.00	1.00		1.00	1.00	1.00		1.00		1.00	1.00	
Incremental Delay, d2	2.5	6.6		0.6	4.6	0.2		6.7		2.2	0.9	
Delay (s)	45.5	19.0		46.7	18.3	7.9		53.0		42.5	40.3	
Level of Service	D	B		D	B	A		D		D	D	
Approach Delay (s)		20.0			16.8			53.0			41.8	
Approach LOS		C			B			D			D	

Intersection Summary			
HCM 2000 Control Delay	19.9	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.79		
Actuated Cycle Length (s)	95.4	Sum of lost time (s)	19.2
Intersection Capacity Utilization	70.9%	ICU Level of Service	C
Analysis Period (min)	15		
c Critical Lane Group			

HCM Signalized Intersection Capacity Analysis

2: Sebastopol Rd & Petaluma Ave

02/05/2019

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	59	537	0	0	667	139	94	478	462	0	0	0
Future Volume (vph)	59	537	0	0	667	139	94	478	462	0	0	0
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700
Total Lost time (s)	4.7	5.0			5.0			4.6	4.6			
Lane Util. Factor	1.00	1.00			0.95			0.95	1.00			
Frpb, ped/bikes	1.00	1.00			1.00			1.00	0.97			
Flpb, ped/bikes	1.00	1.00			1.00			1.00	1.00			
Frt	1.00	1.00			0.97			1.00	0.85			
Flt Protected	0.95	1.00			1.00			0.99	1.00			
Satd. Flow (prot)	1615	1700			3138			3203	1408			
Flt Permitted	0.95	1.00			1.00			0.99	1.00			
Satd. Flow (perm)	1615	1700			3138			3203	1408			
Peak-hour factor, PHF	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Adj. Flow (vph)	61	554	0	0	688	143	97	493	476	0	0	0
RTOR Reduction (vph)	0	0	0	0	13	0	0	0	24	0	0	0
Lane Group Flow (vph)	61	554	0	0	818	0	0	590	452	0	0	0
Confl. Peds. (#/hr)							3	1	2			
Confl. Bikes (#/hr)									2			
Heavy Vehicles (%)	0%	0%	2%	2%	0%	0%	0%	0%	0%	2%	2%	2%
Turn Type	Prot	NA			NA		Perm	NA	Perm			
Protected Phases	5	2			6			8				
Permitted Phases							8		8			
Actuated Green, G (s)	6.9	43.8			32.2			36.3	36.3			
Effective Green, g (s)	6.9	43.8			32.2			36.3	36.3			
Actuated g/C Ratio	0.08	0.49			0.36			0.40	0.40			
Clearance Time (s)	4.7	5.0			5.0			4.6	4.6			
Vehicle Extension (s)	2.0	3.0			4.0			3.5	3.5			
Lane Grp Cap (vph)	124	830			1126			1296	569			
v/s Ratio Prot	0.04	c0.33			c0.26							
v/s Ratio Perm								0.18	c0.32			
v/c Ratio	0.49	0.67			0.73			0.46	0.79			
Uniform Delay, d1	39.7	17.4			24.9			19.5	23.4			
Progression Factor	1.00	1.00			1.00			1.00	1.00			
Incremental Delay, d2	1.1	2.0			2.5			0.3	7.8			
Delay (s)	40.8	19.5			27.5			19.8	31.2			
Level of Service	D	B			C			B	C			
Approach Delay (s)		21.6			27.5			24.9			0.0	
Approach LOS		C			C			C			A	

Intersection Summary

HCM 2000 Control Delay	24.9	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.77		
Actuated Cycle Length (s)	89.7	Sum of lost time (s)	14.3
Intersection Capacity Utilization	95.1%	ICU Level of Service	F
Analysis Period (min)	15		
c Critical Lane Group			

HCM Signalized Intersection Capacity Analysis

3: Bodega Ave/Sebastopol Rd & Main St

02/05/2019

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑	↗	↘	↑					↘	↑↗	
Traffic Volume (vph)	0	337	52	342	435	0	0	0	0	255	533	50
Future Volume (vph)	0	337	52	342	435	0	0	0	0	255	533	50
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700
Total Lost time (s)		4.7	4.7	11.7	11.7					4.7	4.7	
Lane Util. Factor		1.00	1.00	1.00	1.00					1.00	0.95	
Frbp, ped/bikes		1.00	0.96	1.00	1.00					1.00	0.99	
Flpb, ped/bikes		1.00	1.00	1.00	1.00					0.99	1.00	
Frt		1.00	0.85	1.00	1.00					1.00	0.99	
Flt Protected		1.00	1.00	0.95	1.00					0.95	1.00	
Satd. Flow (prot)		1700	1391	1615	1700					1596	3167	
Flt Permitted		1.00	1.00	0.95	1.00					0.95	1.00	
Satd. Flow (perm)		1700	1391	1615	1700					1596	3167	
Peak-hour factor, PHF	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Adj. Flow (vph)	0	359	55	364	463	0	0	0	0	271	567	53
RTOR Reduction (vph)	0	0	4	0	0	0	0	0	0	0	7	0
Lane Group Flow (vph)	0	359	51	364	463	0	0	0	0	271	613	0
Confl. Peds. (#/hr)			17							5		25
Confl. Bikes (#/hr)												1
Heavy Vehicles (%)	2%	0%	0%	0%	0%	2%	2%	2%	2%	0%	0%	0%
Turn Type		NA	Perm	Prot	NA					Perm	NA	
Protected Phases		4		3	8						2	
Permitted Phases			4							2		
Actuated Green, G (s)		16.5	16.5	16.4	37.6					24.5	24.5	
Effective Green, g (s)		16.5	16.5	16.4	37.6					24.5	24.5	
Actuated g/C Ratio		0.20	0.20	0.20	0.46					0.30	0.30	
Clearance Time (s)		4.7	4.7	11.7	11.7					4.7	4.7	
Vehicle Extension (s)		3.0	3.0	3.0	5.0					4.0	4.0	
Lane Grp Cap (vph)		342	279	323	779					476	946	
v/s Ratio Prot		c0.21		c0.23	0.27						c0.19	
v/s Ratio Perm			0.04							0.17		
v/c Ratio		1.05	0.18	1.13	0.59					0.57	0.65	
Uniform Delay, d1		32.8	27.2	32.8	16.5					24.3	25.0	
Progression Factor		1.00	1.00	1.00	1.00					1.00	1.00	
Incremental Delay, d2		62.3	0.3	88.9	1.8					1.9	1.7	
Delay (s)		95.0	27.5	121.7	18.4					26.2	26.7	
Level of Service		F	C	F	B					C	C	
Approach Delay (s)		86.1			63.9			0.0			26.6	
Approach LOS		F			E			A			C	
Intersection Summary												
HCM 2000 Control Delay			52.6			HCM 2000 Level of Service				D		
HCM 2000 Volume to Capacity ratio			0.88									
Actuated Cycle Length (s)			82.0			Sum of lost time (s)			23.1			
Intersection Capacity Utilization			95.1%			ICU Level of Service			F			
Analysis Period (min)			15									
c Critical Lane Group												

HCM Signalized Intersection Capacity Analysis

4: Main St & McKinley St

02/05/2019



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↶	↷				↷↶
Traffic Volume (vph)	72	559	0	0	0	782
Future Volume (vph)	72	559	0	0	0	782
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700
Total Lost time (s)	4.7	4.7				4.7
Lane Util. Factor	1.00	1.00				0.95
Frbp, ped/bikes	1.00	1.00				1.00
Flpb, ped/bikes	1.00	1.00				1.00
Frt	1.00	0.85				1.00
Flt Protected	0.95	1.00				1.00
Satd. Flow (prot)	1615	1445				3230
Flt Permitted	0.95	1.00				1.00
Satd. Flow (perm)	1615	1445				3230
Peak-hour factor, PHF	0.95	0.95	0.95	0.95	0.95	0.95
Adj. Flow (vph)	76	588	0	0	0	823
RTOR Reduction (vph)	58	0	0	0	0	0
Lane Group Flow (vph)	18	588	0	0	0	823
Confl. Peds. (#/hr)	18	6				
Confl. Bikes (#/hr)		1				
Heavy Vehicles (%)	0%	0%	2%	2%	2%	0%
Turn Type	Prot	custom				NA
Protected Phases	7	2 6 7				2
Permitted Phases						
Actuated Green, G (s)	14.5	51.8				32.6
Effective Green, g (s)	14.5	51.8				32.6
Actuated g/C Ratio	0.24	0.86				0.54
Clearance Time (s)	4.7					4.7
Vehicle Extension (s)	2.0					4.0
Lane Grp Cap (vph)	390	1247				1754
v/s Ratio Prot	0.01	c0.41				0.25
v/s Ratio Perm						
v/c Ratio	0.05	0.47				0.47
Uniform Delay, d1	17.5	0.9				8.4
Progression Factor	1.00	1.00				1.00
Incremental Delay, d2	0.0	0.4				0.3
Delay (s)	17.5	1.3				8.7
Level of Service	B	A				A
Approach Delay (s)	3.2		0.0			8.7
Approach LOS	A		A			A

Intersection Summary			
HCM 2000 Control Delay	6.2	HCM 2000 Level of Service	A
HCM 2000 Volume to Capacity ratio	0.52		
Actuated Cycle Length (s)	60.0	Sum of lost time (s)	13.4
Intersection Capacity Utilization	43.2%	ICU Level of Service	A
Analysis Period (min)	15		
c Critical Lane Group			

HCM Signalized Intersection Capacity Analysis

1: Sebastopol Rd & Morris St

02/05/2019

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	49	735	6	5	705	170	5	4	6	162	1	52
Future Volume (vph)	49	735	6	5	705	170	5	4	6	162	1	52
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700
Total Lost time (s)	4.7	5.1		4.7	5.1	5.1		4.7		4.7	4.7	
Lane Util. Factor	1.00	1.00		1.00	1.00	1.00		1.00		1.00	1.00	
Frbp, ped/bikes	1.00	1.00		1.00	1.00	0.98		0.98		1.00	0.93	
Flpb, ped/bikes	1.00	1.00		1.00	1.00	1.00		1.00		1.00	1.00	
Frt	1.00	1.00		1.00	1.00	0.85		0.94		1.00	0.85	
Flt Protected	0.95	1.00		0.95	1.00	1.00		0.98		0.95	1.00	
Satd. Flow (prot)	1583	1663		1583	1667	1385		1508		1583	1328	
Flt Permitted	0.95	1.00		0.95	1.00	1.00		0.98		0.95	1.00	
Satd. Flow (perm)	1583	1663		1583	1667	1385		1508		1583	1328	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	53	799	7	5	766	185	5	4	7	176	1	57
RTOR Reduction (vph)	0	0	0	0	0	9	0	7	0	0	0	0
Lane Group Flow (vph)	53	806	0	5	766	176	0	9	0	176	58	0
Confl. Peds. (#/hr)			20			1						14
Confl. Bikes (#/hr)			2						3			1
Turn Type	Prot	NA		Prot	NA	Perm	Split	NA		Split	NA	
Protected Phases	5	2		1	6		8	8		4	4	
Permitted Phases						6						
Actuated Green, G (s)	8.7	63.5		1.6	56.4	56.4		3.4		16.3	16.3	
Effective Green, g (s)	8.7	63.5		1.6	56.4	56.4		3.4		16.3	16.3	
Actuated g/C Ratio	0.08	0.61		0.02	0.54	0.54		0.03		0.16	0.16	
Clearance Time (s)	4.7	5.1		4.7	5.1	5.1		4.7		4.7	4.7	
Vehicle Extension (s)	3.0	5.0		3.0	5.0	5.0		3.0		3.0	3.0	
Lane Grp Cap (vph)	132	1015		24	904	751		49		248	208	
v/s Ratio Prot	c0.03	c0.48		0.00	c0.46			c0.01		c0.11	0.04	
v/s Ratio Perm						0.13						
v/c Ratio	0.40	0.79		0.21	0.85	0.23		0.19		0.71	0.28	
Uniform Delay, d1	45.2	15.3		50.6	20.2	12.5		49.0		41.6	38.7	
Progression Factor	1.00	1.00		1.00	1.00	1.00		1.00		1.00	1.00	
Incremental Delay, d2	2.0	5.0		4.3	8.2	0.3		1.9		9.0	0.7	
Delay (s)	47.2	20.3		54.9	28.3	12.8		50.8		50.6	39.4	
Level of Service	D	C		D	C	B		D		D	D	
Approach Delay (s)		22.0			25.5			50.8			47.8	
Approach LOS		C			C			D			D	

Intersection Summary

HCM 2000 Control Delay	26.7	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.77		
Actuated Cycle Length (s)	104.0	Sum of lost time (s)	19.2
Intersection Capacity Utilization	70.4%	ICU Level of Service	C
Analysis Period (min)	15		

c Critical Lane Group

HCM Signalized Intersection Capacity Analysis

2: Sebastopol Rd & Petaluma Ave

02/05/2019

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	100	432	0	0	642	141	146	602	436	0	0	0
Future Volume (vph)	100	432	0	0	642	141	146	602	436	0	0	0
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700
Total Lost time (s)	4.7	5.0			5.0			4.6	4.6			
Lane Util. Factor	1.00	1.00			0.95			0.95	1.00			
Frbp, ped/bikes	1.00	1.00			1.00			1.00	0.97			
Flpb, ped/bikes	1.00	1.00			1.00			1.00	1.00			
Frt	1.00	1.00			0.97			1.00	0.85			
Flt Protected	0.95	1.00			1.00			0.99	1.00			
Satd. Flow (prot)	1583	1667			3070			3134	1368			
Flt Permitted	0.95	1.00			1.00			0.99	1.00			
Satd. Flow (perm)	1583	1667			3070			3134	1368			
Peak-hour factor, PHF	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Adj. Flow (vph)	103	445	0	0	662	145	151	621	449	0	0	0
RTOR Reduction (vph)	0	0	0	0	13	0	0	0	30	0	0	0
Lane Group Flow (vph)	103	445	0	0	794	0	0	772	419	0	0	0
Confl. Peds. (#/hr)							7	3	7			
Confl. Bikes (#/hr)									1			
Turn Type	Prot	NA			NA		Perm	NA	Perm			
Protected Phases	5	2			6			8				
Permitted Phases							8		8			
Actuated Green, G (s)	8.0	45.5			32.8			36.7	36.7			
Effective Green, g (s)	8.0	45.5			32.8			36.7	36.7			
Actuated g/C Ratio	0.09	0.50			0.36			0.40	0.40			
Clearance Time (s)	4.7	5.0			5.0			4.6	4.6			
Vehicle Extension (s)	2.0	3.0			4.0			3.5	3.5			
Lane Grp Cap (vph)	137	826			1096			1252	546			
v/s Ratio Prot	c0.07	0.27			c0.26							
v/s Ratio Perm								0.25	c0.31			
v/c Ratio	0.75	0.54			0.72			0.62	0.77			
Uniform Delay, d1	40.9	15.9			25.6			21.9	23.9			
Progression Factor	1.00	1.00			1.00			1.00	1.00			
Incremental Delay, d2	18.4	0.7			2.6			1.0	6.6			
Delay (s)	59.4	16.6			28.2			22.9	30.4			
Level of Service	E	B			C			C	C			
Approach Delay (s)		24.6			28.2			25.7			0.0	
Approach LOS		C			C			C			A	

Intersection Summary			
HCM 2000 Control Delay	26.2	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.75		
Actuated Cycle Length (s)	91.8	Sum of lost time (s)	14.3
Intersection Capacity Utilization	94.9%	ICU Level of Service	F
Analysis Period (min)	15		
c Critical Lane Group			

HCM Signalized Intersection Capacity Analysis

3: Bodega Ave/Sebastopol Rd & Main St

02/05/2019

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑	↗	↘	↑					↘	↑	↗
Traffic Volume (vph)	0	267	58	357	472	0	0	0	0	259	655	108
Future Volume (vph)	0	267	58	357	472	0	0	0	0	259	655	108
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700	1700
Total Lost time (s)		4.7	4.7	11.7	11.7					4.7	4.7	
Lane Util. Factor		1.00	1.00	1.00	1.00					1.00	0.95	
Frbp, ped/bikes		1.00	0.86	1.00	1.00					1.00	0.96	
Flpb, ped/bikes		1.00	1.00	1.00	1.00					0.89	1.00	
Frt		1.00	0.85	1.00	1.00					1.00	0.98	
Flt Protected		1.00	1.00	0.95	1.00					0.95	1.00	
Satd. Flow (prot)		1667	1212	1583	1667					1412	2979	
Flt Permitted		1.00	1.00	0.95	1.00					0.95	1.00	
Satd. Flow (perm)		1667	1212	1583	1667					1412	2979	
Peak-hour factor, PHF	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Adj. Flow (vph)	0	278	60	372	492	0	0	0	0	270	682	112
RTOR Reduction (vph)	0	0	0	0	0	0	0	0	0	0	0	0
Lane Group Flow (vph)	0	278	60	372	492	0	0	0	0	270	795	0
Confl. Peds. (#/hr)			82							42		99
Confl. Bikes (#/hr)												2
Turn Type		NA	Perm	Prot	NA					Perm	NA	
Protected Phases		4		3	8						2	
Permitted Phases			4							2		
Actuated Green, G (s)		16.4	16.4	16.9	38.0					29.9	29.9	
Effective Green, g (s)		16.4	16.4	16.9	38.0					29.9	29.9	
Actuated g/C Ratio		0.19	0.19	0.19	0.43					0.34	0.34	
Clearance Time (s)		4.7	4.7	11.7	11.7					4.7	4.7	
Vehicle Extension (s)		3.0	3.0	3.0	5.0					4.0	4.0	
Lane Grp Cap (vph)		311	226	304	721					480	1014	
v/s Ratio Prot		c0.17		c0.23	0.30						c0.27	
v/s Ratio Perm			0.05							0.19		
v/c Ratio		0.89	0.27	1.22	0.68					0.56	0.78	
Uniform Delay, d1		34.9	30.5	35.5	20.0					23.6	26.0	
Progression Factor		1.00	1.00	1.00	1.00					1.00	1.00	
Incremental Delay, d2		26.0	0.6	126.4	3.4					1.8	4.3	
Delay (s)		60.9	31.2	161.9	23.5					25.4	30.3	
Level of Service		E	C	F	C					C	C	
Approach Delay (s)		55.6			83.1			0.0			29.1	
Approach LOS		E			F			A			C	
Intersection Summary												
HCM 2000 Control Delay			53.6			HCM 2000 Level of Service				D		
HCM 2000 Volume to Capacity ratio			0.91									
Actuated Cycle Length (s)			87.8			Sum of lost time (s)		23.1				
Intersection Capacity Utilization			94.9%			ICU Level of Service		F				
Analysis Period (min)			15									
c Critical Lane Group												

HCM Signalized Intersection Capacity Analysis

4: Main St & McKinley St

02/05/2019

	↙	↖	↑	↗	↘	↓
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↙	↖				↑↑
Traffic Volume (vph)	210	779	0	0	1	781
Future Volume (vph)	210	779	0	0	1	781
Ideal Flow (vphpl)	1700	1700	1700	1700	1700	1700
Total Lost time (s)	4.7	4.7				4.7
Lane Util. Factor	1.00	1.00				0.95
Frbp, ped/bikes	1.00	1.00				1.00
Flpb, ped/bikes	1.00	1.00				1.00
Frt	1.00	0.85				1.00
Flt Protected	0.95	1.00				1.00
Satd. Flow (prot)	1583	1417				3166
Flt Permitted	0.95	1.00				1.00
Satd. Flow (perm)	1583	1417				3166
Peak-hour factor, PHF	0.97	0.97	0.97	0.97	0.97	0.97
Adj. Flow (vph)	216	803	0	0	1	805
RTOR Reduction (vph)	0	0	0	0	0	0
Lane Group Flow (vph)	216	803	0	0	0	806
Confl. Peds. (#/hr)	116	45				
Confl. Bikes (#/hr)		2				
Turn Type	Prot	custom				NA
Protected Phases	7	2 6 7				2
Permitted Phases		7				
Actuated Green, G (s)	17.8	53.3				30.8
Effective Green, g (s)	17.8	53.3				30.8
Actuated g/C Ratio	0.29	0.86				0.50
Clearance Time (s)	4.7					4.7
Vehicle Extension (s)	2.0					4.0
Lane Grp Cap (vph)	454	1218				1572
v/s Ratio Prot	0.14	c0.57				
v/s Ratio Perm						0.25
v/c Ratio	0.48	0.66				0.51
Uniform Delay, d1	18.2	1.4				10.5
Progression Factor	1.00	1.00				1.00
Incremental Delay, d2	0.3	1.4				0.4
Delay (s)	18.5	2.9				10.9
Level of Service	B	A				B
Approach Delay (s)	6.2		0.0			10.9
Approach LOS	A		A			B
Intersection Summary						
HCM 2000 Control Delay		8.3		HCM 2000 Level of Service		A
HCM 2000 Volume to Capacity ratio		0.72				
Actuated Cycle Length (s)		62.0		Sum of lost time (s)	13.4	
Intersection Capacity Utilization		61.5%		ICU Level of Service		B
Analysis Period (min)		15				
c Critical Lane Group						



**A CULTURAL RESOURCES EVALUATION OF
THE PROPOSED TOWNHOMES LOCATED AT 6737
SEBASTOPOL AVENUE, SEBASTOPOL, SONOMA COUNTY,
CALIFORNIA (APN 004-063-036)**

SUBMITTED BY

William Roop, M.A., RPA, ARCHAEOLOGICAL RESOURCE SERVICE

SUBMITTED FOR

Dan Davis, project proponent

January 16, 2019

A.R.S. Project 18-063

INTRODUCTION

As requested and authorized, Archaeological Resource Service has conducted an archaeological evaluation of the parcel described below. The following basic tasks are to be accomplished as part of this project:

1. A check of the information on file with our office and the Regional Office of the California Historical Resources Information System, to determine the presence or absence of previously recorded historic or prehistoric cultural resources,
2. A check of appropriate historic references to determine the potential for historic era archaeological deposits, and;
3. Contact with the Native American Heritage Commission to determine the presence or absence of listed Sacred Lands within the project area;
4. Contact with all appropriate Native American organizations or individuals designated by the Native American Heritage Commission as interested parties for the project area;
5. A surface reconnaissance of all accessible parts of the project area to locate any visible signs of potentially significant historic or prehistoric cultural deposits.
6. Preparation of a report describing the work accomplished, the results of the research, and making appropriate recommendations for further action, if warranted.

PROJECT DESCRIPTION

The proposed project would construct eighteen townhomes with associated parking and access within the approximately 1.7 acre property.

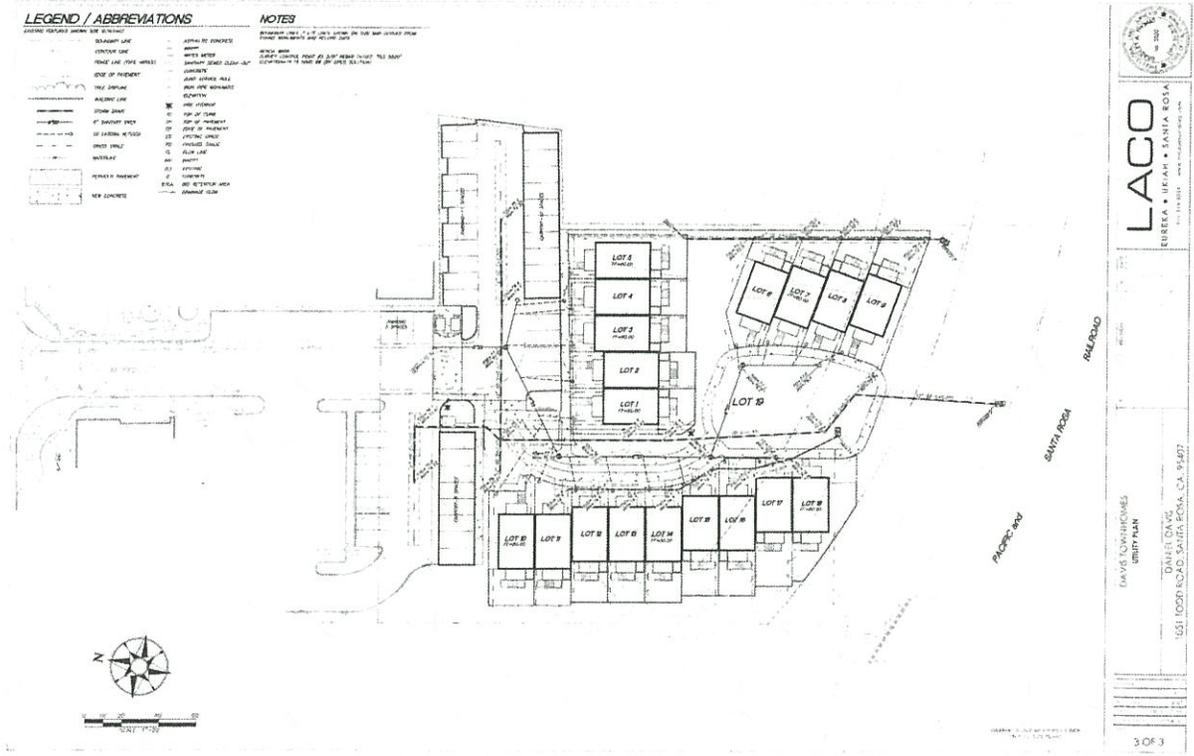
PROJECT LOCATION

The project area is located at 6737 Sebastopol Avenue, Sebastopol, Sonoma County, California. The parcel consists of about 1.7 acres of open land bounded by a parking lot to the north, a densely vegetated area to the south, vacant commercial land to the east, and a hotel complex to the west.

The project area lies in the southwest quarter of the southeast quarter within Section 35 of Township 7 North, Range 9 West, Mt. Diablo Base and Meridian, as shown on the USGS 7.5' Sebastopol Quadrangle Map (1954; photorevised 1968). The Universal Transverse Mercator

California Environmental Quality Act (CEQA) (14 CCR 15064.5 and PRC 21084.1). Significant historical resources need to be addressed before environmental mitigation guidelines are developed and approved. A "significant historical resource" (including both a prehistoric and historic resource) is one that is found eligible for listing in the California Register of Historical Resources. As per Title 14, California Code of Regulations Section 15064.5, historical resources are those that are:

- Listed in, or eligible for listing in, the California Register of Historic Resources (Public Resources Code 5024.1, Title 14 CCR, Section 4850 et. seq.);
- Listed in, or eligible for listing in, the National Register of Historic Places (CRHR);
- Included in a local register of historical resources, as defined in an historical resource survey meeting the requirements of Section 5024.1(g) of the Public Resource Code; or
- Any object, building, structure, site, area, place, record, or manuscript which a lead agency determines to be historically significant or significant in the architectural, engineering, scientific, economic, agricultural, educational, social, political, military, or cultural annals of California, provided the lead agency's determination is supported by substantial evidence in light of the whole record.



The 18 individual units and the associated parking and access are shown here. This is the evaluated plan. Additionally, historical resources and historic districts designated or listed as city or county landmarks or historic properties or districts pursuant to any city or county ordinance can also be listed in the California Register, if the criteria for listing under the ordinance have been determined by the Office of Historic Preservation to be consistent with California Register criteria adopted by the commission (pursuant to Section 5024.1(e) of the PRC).

A resource may be listed as an historical resource in the California Register if it has integrity and meets any of the following National Register of Historic Places criteria:

- 1) Is associated with events that have made a significant contribution to the broad patterns of our history; or
- 2) Is associated with the lives of persons important in our past; or
- 3) Embody the distinctive characteristics of a type, period, or method of construction, or represent the work of a master, or possesses high artistic values, or represent a significant and distinguishable entity whose components may lack individual distinction; or
- 4) Has yielded, or may be likely to yield, information important in prehistory or history.

CEQA (PRC 21083.2) also distinguishes between two classes of archaeological resources: archaeological sites that meet the definition of a historical resource as above, and “unique archaeological resources.” A “unique archaeological resource” has been defined in CEQA as an archaeological artifact, object, or site about which it can be clearly demonstrated that, without merely adding to the current body of knowledge, there is a high probability that it meets any of the following criteria:

- 1) Contains information needed to answer important scientific research questions and that there is a demonstrable public interest in that information,
- 2) Has a special and particular quality such as being the oldest of its type or the best available example of its type, or
- 3) Is directly associated with a scientifically recognized important prehistoric or historic event or person.

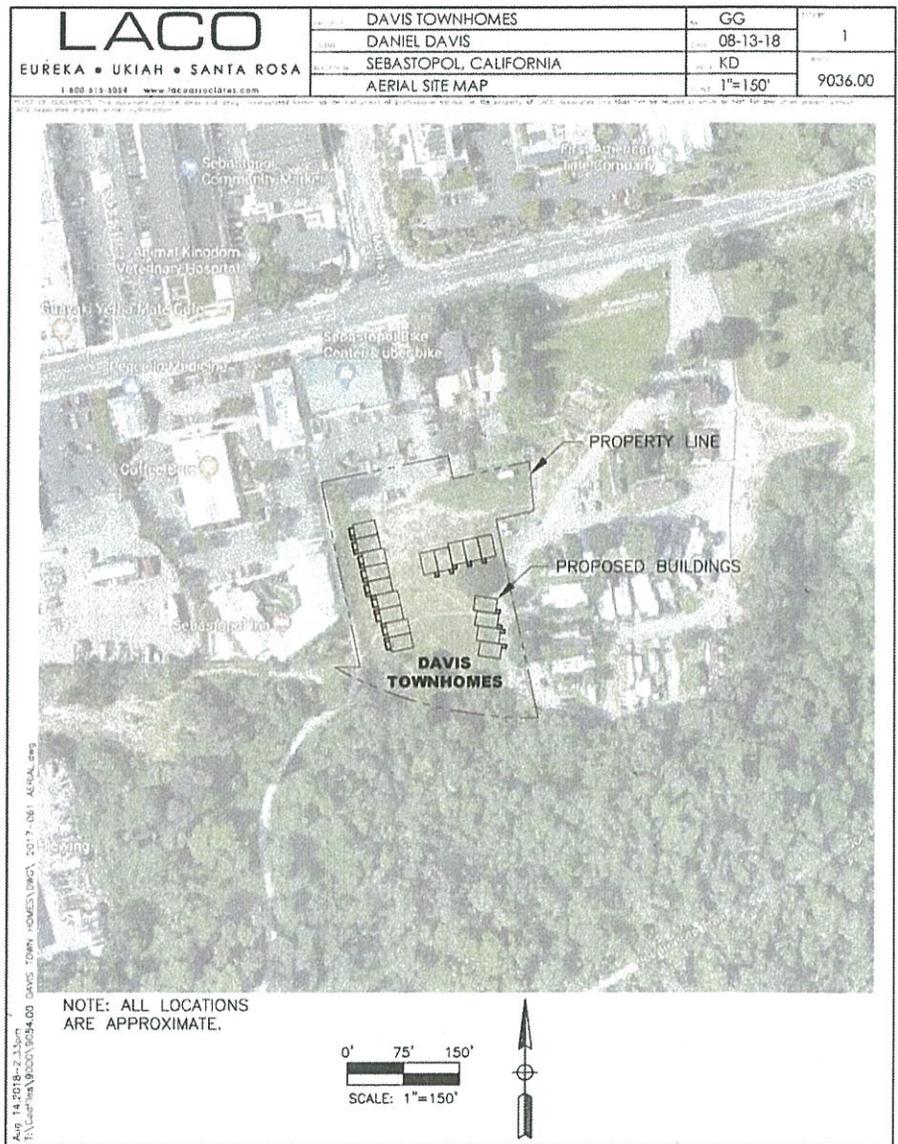


FIGURE 3 -- THE EVALUATED PLAN ON GOOGLE EARTH

Buildings, sites, structures, objects, and districts representative of California and United States history, architecture, archaeology, engineering, and culture convey significance when they also possess integrity of location, design, setting, materials, workmanship, feeling, and association. A resource has integrity if it retains the characteristics that were present during the resource's period of significance. Enough of these characteristics must remain to convey the reasons for its significance.

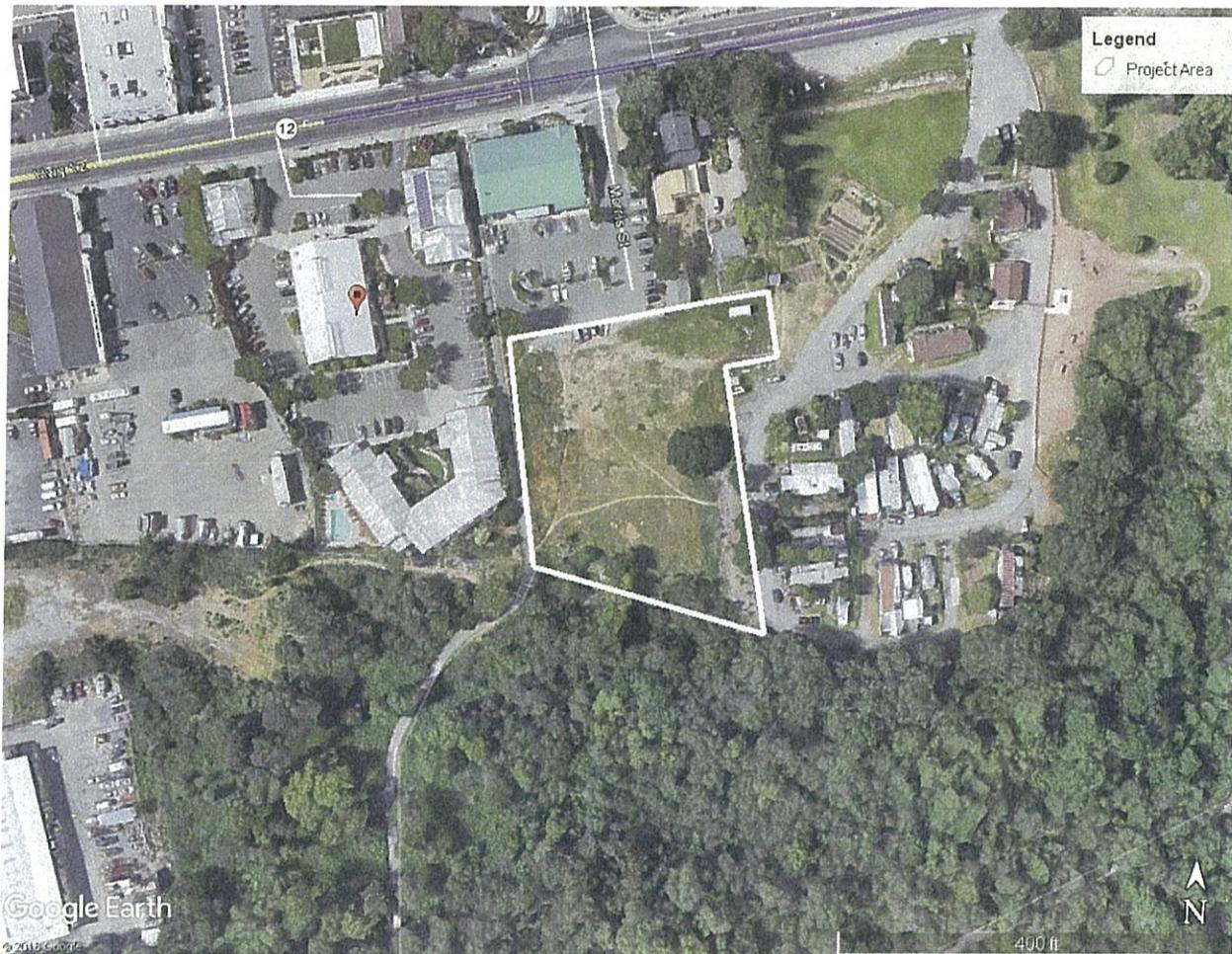


FIGURE 4 -- ANOTHER VIEW OF THE PROJECT AREA FROM GOOGLE EARTH

This represents the condition of the area during the archaeological inspection.

As of July 2015, two new classes of resources have been defined. Tribal cultural resources and Tribal cultural landscapes can be any of a variety of cultural sites as defined by the individual tribe. These resources, once identified, are treated as significant resources under CEQA.

The fact that a resource is not listed in, or determined to be eligible for listing in the CRHR, or included in a local register of historical resources (pursuant to Section 5020.1(k) of the PRC), or identified in an historical resources survey (meeting the criteria in Section 5024.1(g) of the PRC) does not preclude a lead agency from determining that the resource may be an historical resources as defined in PRC sections 5020.1(j) or 5024.1.

SACRED LANDS INVENTORY / NATIVE AMERICAN CONSULTATION

The California Native American Heritage Commission (NAHC) works to identify, catalogue, and protect places of special religious or social significance, graves, and cemeteries of Native Americans per the authority given the Commission in Public Resources Code 5097.9. A check with the NAHC was done to determine if there are sites listed in the Sacred Lands file located within or near to the current project area.

No response has been received from the agency. It is recommended that the lead agency contact any tribes that have indicated a wish to be consulted on planning projects. The Native American Heritage Commission has previously indicated that the Federated Indians of Graton Rancheria are the appropriate group to consult for this location. The December 5 letter to Katherine Austin from the City of Sebastopol Planning Department notes that this consultation has been initiated.

RESULTS OF LITERATURE CHECK

Our search began with an archival review of the computerized listing of archaeological reports maintained by Archaeological Resource Service. This study indicated that archaeological studies in the area around Sebastopol have consisted of mostly small parcels ranging from barely one acre to greater than 150 acres. The majority of the reports were negative, although historic properties have been identified in one or two cases and a few prehistoric sites have been identified in several other parcels. None of the presently recorded prehistoric or historic sites would be affected by the planned project.

A literature check performed at the Northwest Information Center (NWIC) in Rohnert Park (File Number 18-1331) provided additional information on the general area. No significant or potentially significant resources were identified in the subject property.

The literature check indicates that several archaeological evaluations have been undertaken in the general vicinity since these studies were first required in the 1970's. No previous evaluation has included the present project area. The available data indicates that the shoreline of the Laguna de Santa Rosa, a seasonably variable water body, was highly attractive to Native American settlement and/or exploitation. The laguna provided a variety of plant materials, migrating birds and other wildlife, as well as fish and freshwater shell fish. The shore of the laguna is considered to be highly sensitive to the potential presence of Native American artifacts or sites. Two previous evaluations are particularly significant to the current study.

In 1997 the property at 6761 Sebastopol Avenue was evaluated by Tom Origer (Origer 1997). The evaluated area lay between the western property line of the current project area (the existing bicycle path), and the former railroad spur that is now marked by a curved line of vegetation along the western edge of the project area. The Origer project extended north to Sebastopol Avenue and about as far south as the current project being reported here. Origer found no indication of an intact archaeological site in his survey area. He did, however, find an extensive deposit of disturbed midden (culturally modified soil) that had been displaced by the former railroad. Due to the presence of cultural soils, Origer executed a series of backhoe trenches to determine the presence or absence of subsurface deposits, and the general condition of the encountered soils. After determining the results of the surface and subsurface examinations, Origer determined that:

No surface or subsurface evidence of intact prehistoric or historic archaeological sites was found. However, the ground surface along the railroad bed is littered with prehistoric midden soils that contain marine shellfish fragments and obsidian flakes. Often such materials indicate the presence of potentially important archaeological sites; however, I performed a

study in the area in 1976 for the Sebastopol Route "E" Bypass, and it was clear then, when visibility was excellent, that these materials represented a secondary deposit (Origer 1976). My April 16, 1997, re-examination of the place where these materials are distributed resulted in the same conclusion that they were redeposited from elsewhere. Although these midden soils do not represent an intact archaeological site, Recommendation #2 (below) addresses the potential that they could contain important materials.

Origer made the following recommendations regarding the deposits (Recommendation 1 involves architectural resources):

2. *The midden soils along the railroad tracks that border the south and southwestern portion of the property should only be disturbed with a qualified archaeologist present. Although the soils do not mark an intact archaeological deposit, there is a possibility that human skeletal remains could be present. I recall when conducting the 1976 Route "E" Bypass study accounts that human graves were disturbed by early railroad construction that passed through nearby archaeological sites, sites from which these soils could have originated. Three archaeological sites (CA-SON-873, CA-SON-874, and CA-SON-1347) are known to exist between Petaluma Avenue and the Laguna de Santa Rosa, and at least two (CA-SON-874 and CA-SON-874) were cut by the railroad.*
3. *If any buried archaeological remains are discovered during construction work (e.g., grading, utilities trenching), work should stop at the place of discovery until a qualified archaeologist is contacted and has completed an evaluation of their importance. Buildings that formerly stood on the parcel no longer are visible; however, archaeological remains could be present. Potentially important archaeological site indicators can include but not necessarily be limited to locally darkened soil (midden) that could contain a combination of shellfish and bone fragments, chipped or ground stone tools (e.g., arrow or spear tips, mortars and pestles), chipping debris (e.g., obsidian or chert flakes/pieces), fire-affected stone pieces (often fist sized and with angular edges), old bottles, ceramic and metal (e.g., square nails) objects.*

Also in 1997, Archaeological Resource Service evaluated the property between 6691 and 6721 Sebastopol Avenue (Roop 1997), including at least part of the present project area. Fill soil was noted covering most of the evaluated area, generating a recommendation for a subsurface examination using a backhoe. The results of the surface examination were reported as:

In summary, the surface reconnaissance has revealed the presence of a fill soil over the parcel that hides any potential for subsurface prehistoric deposits. The fill soil could contain significant historic era deposits associated with the commercial endeavors that have been practiced there. The remaining building, a large residential structure, is clearly over fifty years of age. A potential exists for buried deposits associated with the past uses of this building. Such deposits, if of sufficient age and integrity, would be considered historically significant (Roop 1997:8).

The remaining potential for significant archaeological features led to a recommendation for a backhoe test, similar to that proposed by Origer earlier the same year. A total of four trenches were excavated. No indication of prehistoric archaeological features or artifacts was observed during the test procedure, although disturbed historic era materials were observed. The following conclusions were drawn:

No prehistoric deposits are apparent within the project area. A potential exists for discovery of buried prehistoric deposits beneath the fill soils of the parcel. The test excavation has demonstrated that this potential is low within the area to be impacted by the proposed building.

Some of the fill layers may consist of large amounts of historic debris which, if of sufficient age and integrity, would constitute significant archaeological deposits. Where they are encountered in the construction process, steps should be taken to insure the preservation of the information contained in them. Historic deposits could be associated with any of the past uses of the parcel, or its neighboring areas. Expected deposits could come from any or all of the following sources:

- 1. The Chinese settlement that existed along the Laguna de Santa Rosa up to the 1930's;*
- 2. The rail yard that extended into Sebastopol from just south of the parcel after the early 1890's, and its associated hobo settlement;*
- 3. The fruit processing operation that existed through much of the 20th century; or,*
- 4. It is not uncommon to find wetlands, and the land bordering them, used as dumping areas by 19th century settlements. Given the close proximity to the settlement of Sebastopol and lack of formal land use until this century, the parcel may have served as an informal, early, Sebastopol dump.*

The two reports discussed above, as well as several other studies conducted in the project vicinity speak to the generally high sensitivity to archaeological resources on the eastern side of Sebastopol, where the resources of the Laguna de Santa Rosa attracted Native settlement.

Several large prehistoric and/or ethnographic habitation sites are located above the flood plain and near the shore of the Laguna de Santa Rosa or on tributary streams that flow into the Laguna. At least one of these recognized village locations is thought to be the ethnographic *Konhomtara* village of *Kacintui*. According to the archival research conducted by Adrian and Mary Praetzelis of the Anthropological Studies Center at Sonoma State University in 1977, this apparent ethnographically named village seems to have been formally recorded as one or two separately identified prehistoric archaeological sites that were designated as either CA-SON-490 or CA-Son-722 (Praetzelis and Praetzelis 1977).

According to research conducted in this same general area by archaeologist Suzanne Stewart, there are several lithic scatter sites that are located below the 200-foot contour (S. Stewart 1982:2). These sites consist mainly of artifactual material that is related to manufacture and/or repair of basic tools such as projectile points, scrapers, and similar chipped stone tools types and appear to contain little else in the way of modified soil deposits or items such as specialized artifacts made of shell or bone (beads, pendants, or awls) that seem to be unrelated to basic subsistence activities (such as hunting or gathering foodstuffs or material like native stone used in tool manufacture.. The prehistoric site known as CA-SON-1102 is yet another example of the tool and flake scatter type of site often found in the highlands away from the old, aboriginal shore of the Laguna. Although some of these sites are located over 1 mile south of the current project area and thus would not be possibly affected by the proposed development, the Son-1102 site was found along Atascadero Creek (that once flowed into the Laguna) (Eisenmann 1978). Rather than being a tool scatter that is well-developed and exposed on the present ground surface, the site was found to have been buried underneath 1 meter of alluvium (Eisenmann 1978).

PREHISTORIC SETTING

The area surrounding the project property has been inhabited for thousands of years by Native American populations. The earliest people occupying this area were probably speakers of the *Yukian* language (S. Stewart 1985). Around 7000 years ago *Hokan* speakers began to migrate from the southern California and Great Basin areas northward (Stewart 1985). Some of these people settled in the Clear Lake region where the Proto-Pomo language developed (Stewart 1985). About four to five thousand years ago, these Pomo-speaking people began to migrate

into the Russian River Valley and the valleys along it as far south as Cotati (Origer and Fredrickson 1980).

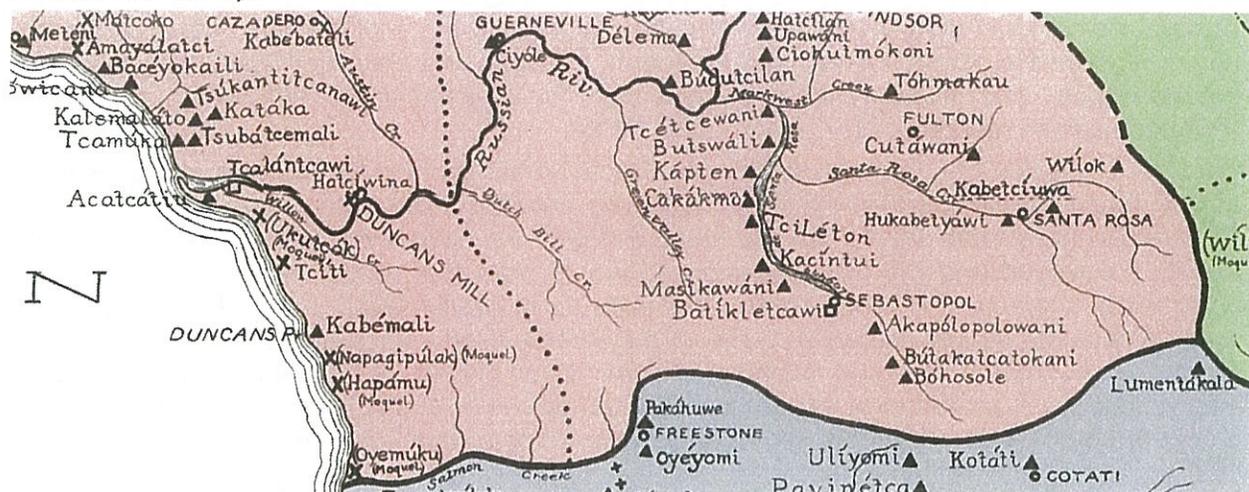


FIGURE 5 – ETHNOGRAPHIC VILLAGES NEAR SEBASTOPOL (BARRETT 1908)

Filled triangles indicate old village sites, hollow squares indicate inhabited (1908) modern village sites. Pink areas were in Pomo territory, green in Yukian (Wappo) territory, and purple indicates Moquelumnan or Coast Miwok territory.

The habitation sites representative of early prehistoric settlement often consist of small mounds of cultural soil (midden) containing a moderate to dense scatter of obsidian and Franciscan chert flakes and tools and sporadic ground stone tools such as mortars, pestles, manos, and other functional categories (Flynn 1990:3). Such sites also might contain the remains of foodstuffs such as marine or terrestrial mollusks (from such taxa as clams, mussels, oysters, abalone, chiton, or snails), butchered animal and bird bone, fire-cracked rock, and baked clay, as well as organic materials like charcoal, ash, and plant fibers.

The general project area lies on the western edge of the former Laguna de Santa Rosa, a large semi-permanent lake and surrounding seasonal wetland in prehistoric time. Today the area is characterized by as a low, seasonally filled basin with small ponds and pools (called vernal pools since they usually form in the spring due to poor drainage) within the surrounding upland areas. The Santa Rosa Plain today is principally covered by introduced European grasses and forbs that have replaced the native, annual grasses and plant species that once were found in the basin and along the banks of the intermittent streams that fed into the lake. The Santa Rosa flood control channel now cuts in an east/west direction through the northern part of the Laguna.

The Laguna de Santa Rosa was a valuable resource to prehistoric residents of this region. Numerous archaeological sites are located within it and around its periphery (Praetzellis and Praetzellis 1977; Origer and Fredrickson 1977, 1980). The Praetzellis and Praetzellis archaeo-environmental study of the *Konhomtara* Pomo, an ethnographic group who lived around a portion of the Laguna, including the present project area, indicates that sites of varying sizes and presumably varying activities are located in and around the Laguna (Praetzellis and Praetzellis 1977). Many of the larger prehistoric sites mentioned in this study and those that were possibly correlated with ethnographically occupied villages (those occupied through the mid to late 1800s and known to historians) were located on the edges of the Laguna, including the hilly land that comprised the historic center of Sebastopol.

Research indicates that few ethnographically named Pomo villages have been identified through archaeological fieldwork, although certain types of artifacts such as very small projectile

The Santa Rosa Plain was a diverse environment where native peoples in both the ethnographic and prehistoric periods could have gathered abundant plant and animal resources (Praetzelis and Praetzelis 1977; Flynn 1990). Short term gathering forays could have been made from these upland village sites into the grass-covered lower plain and marsh/vernal pool areas to collect seasonally available plants or to hunt various game animals and birds. Accordingly being situated in a slightly more upland setting, the project area might contain hunting trails or temporary campsites marked by concentrations of lithic materials or other kinds of artifacts related to basic subsistence activities.

Barrett (19078) described the village of *Batikletcawi* or *Batinkletcawi* as an inhabited modern village site in Sebastopol. His description of the community is as follows:

Bati'kletcawi or *batiñkletca'wi*, from *bati'*, alder, *Alnus rhombifolia*, *Kale'*, tree, and *tca'wi*, house, or *totolagotca* (Western Moquelumnan dialect name), from *to'tola*, elderberry, and *go'tcha*, house, in the southern part of the town of Sebastopol. There is at present but a single house with about seven inhabitants here, but this was once a populous village. This house is located on the site of the old village which also bore the name *Bati'kletcawi*. At a point about a mile east of the town of Sebastopol there is another family of about 10 individuals, and there are several other places within the limits of the dialectic area where Indians may be found at times, as on the ranches near the towns of Windsor, Healdsburg and Cloverdale; but the sites at Sebastopol were the only ones found which are inhabited regularly and permanently. The total number of Indians, excluding those at the town of Sebastopol, regularly residing within this dialectic area, is not greater than twenty-five (Barrett 1908:213-214).



FIGURE 7 – SEBASTOPOL IN 1876

HISTORIC SETTING

The town of Sebastopol began as the community of Pine Grove in 1855 (Thompson 1877). Settlement of the general area began about 1846 with Joaquin Carillo, owner of the Rancho Llano de Santa Rosa. James M. Miller and John Walker opened a store about a mile south of the intersection of the Petaluma Road and the Santa Rosa Road (Highway 116 at Highway 12) in 1850. In 1855, J.H.P. Morris procured a building from Miller and Walker and moved it onto a tract of government land where Sebastopol is now, and opened a saloon and grocery store. He then deeded a lot to John Dougherty on condition that a store be erected on it. The town of Pine Grove apparently achieved its present name during the siege of Sebastopol on the Crimean Peninsula. A fight between two locals, Jeff Stevens and a man named Hibbs, ended when Hibbs sought refuge in Dougherty's store. Dougherty forbade Stevens to enter the store and the locals referred to the place as "Hibbs Sebastopol." The name apparently stuck and

Pine Grove was left to history. The next business established in town was a blacksmith shop and wagon manufactory run by George Jacobs. A hotel was the next business in town, followed by several more enterprises, raising the local population to about 300 by 1877 ((Munro-Fraser 1877). In 1877 the town supported "three stores, one hotel, one blacksmith shop, one shoe shop, one saloon, one livery stable, one meat market, and three



FIGURE 8 – SEBASTOPOL ON THE 1942 15' QUADRANGLE MAP

physicians, one of whom is a lady." (Munro-Fraser 1877:176).

Examination of several early maps indicates that the project area was outside of the Sebastopol business district up to the cusp of the 20th century. The opening of the Northwest Pacific branch line in 1890 and the electrically powered Petaluma and Santa Rosa rail line in 1903 introduced new activities on and near the project area. An impromptu community of Chinese, presumably railroad workers, developed on the southern edge of the present parcel. The Chinese community slowly died out, to be

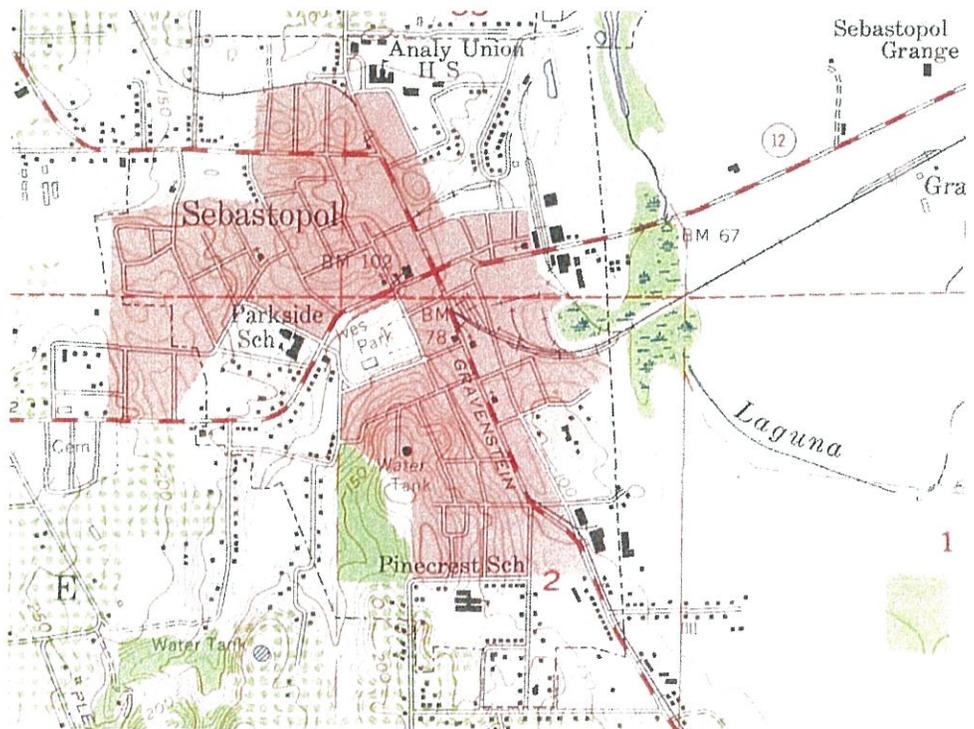


FIGURE 9 -- SEBASTOPOL IN 1954

replaced in the area by a later hobo camp. A homeless camp continues to be active. These uses are related by local tradition, although I have found no written record. One description of 20th century Chinese experience in Sebastopol does not mention the property, but gives a feeling for the condition of the Chinese American community in the first and early second quarters of the 20th century. The following quote is part of the story of John Wall, a Chinese American from Sebastopol. His father, Ginn Wall, was a tenant farmer on an apple orchard near Sebastopol:

There were about three hundred Chinese farmworkers up there, and they were old men. I asked my dad about it and he told me they had come over here about the same time he did and they were working on the railroad. Then he began to talk a little about the railroad, something happened after they were through building it, but I didn't listen to him carefully enough then. I just knew that after the railroad was over, these guys worked on a lumber mill for a while doing shingle work, and then when the lumber mill shut down, they went from there to farming. Naturally they've been doing this type of stuff most of their lives anyway (Ginn in Brett and Nee 1973:26).

Ginn Wall spent his life savings to bring his wife, Johnny's mother, from China. She lived there for many years, the only woman in the Chinese community of Sebastopol, until she died in May 1929:

Well, there they were, with three hundred Chinese workers, and except for my mother, not a single woman. That was the whole Chinese settlement in Sebastopol. All those old guys



FIGURE 10 -- THE PROJECT AREA FROM GOOGLE EARTH, LOOKING SOUTH

This is the appearance of the project area when the evaluation was undertaken.

thought about was how they wanted to go back to China. But there's only about six months work in the year on apples, so they never saved a thing. And the only other thing besides work was gambling. Gambling was the social life, and gambling was the pastime. Everybody hoped to make a few bucks so they could go home in the easy way. The others lost their money and got stuck from year to year. And the reason there's no Chinese in Sebastopol today (1973) is that eventually they all died off because there was no reproduction (Ginn in Brett and Nee 1973:26-27).

RESULTS OF SURFACE EXAMINATION

The cultural resource evaluation has resulted in a negative finding. A negative result indicates that no artifacts or potentially significant cultural features were observed.

The entire project area was examined in a series of pedestrian transects running north-south between the parking area and the southern edge of the property. It was observed that a layer of fill soil appears to cover most of the examined area.

Recent trash mixed with some older material was observed over the property. Fragments of white ceramic, apparently from broken sinks and other fixtures were observed in the fill material toward the northern side of the property. These materials demonstrate no distinct pattern or deposit and do not constitute significant or potentially significant cultural resources.



FIGURE 11 -- LOOKING SOUTHWEST ACROSS THE PROPERTY

The bicycle\pedestrian path that marks the parcel boundary on the west can be seen in front of the neighbor's building. The white flecks in the foreground are largely fragments of ceramic fixtures.

A hand trowel was used to scrape up a soil sample and examine the material for any signs of Native American settlement or use. No shell or bone fragments, flakes of knappable stone, burned rock, or other indicators of the presence of a Native American settlement were observed at any location within the examined area.

Historic materials, including some modern trash, were more densely distributed toward the northern part of the property than toward the Laguna to the south. As discussed in the 1997 report that partially included the present project area (Roop 1997), this material is potentially associated with railroad construction and use, apple processing, Chinese settlement in the area, hobo camps in the area and possibly casual dumping by local residents in a relatively unused area. Any of these deposits is likely to have been disturbed by subsequent human activity, and is not likely to retain the integrity of deposit that is required to identify potential significance in the archaeological record.



FIGURE 12 -- LOOKING SOUTHEAST ACROSS THE PROPERTY

The mobile home community is on the neighboring property.

CONCLUSIONS

No significant or potentially significant artifacts, archaeological deposits or features have been identified within the project area. Previous evaluations in the vicinity have identified potentially significant historic era features in fill soils associated with former fruit processing operations. Such deposits have a potential to be classified as significant if they retain integrity of place, composition and condition. Badly disturbed deposits lose integrity and value in the

interpretation of past events. Previously identified historic era deposits in the area are associated with either the railroad or fruit processing. Both of these are considered significant in the development of Sebastopol as a community. The previously identified deposits have been badly disturbed and retained little interpretive value. A similar situation is likely to prevail in the current project area.

It must be concluded that there is a potential for the discovery of historic era artifacts and/or features identified with historic era activities in Sebastopol. Previously discovered features have been disturbed to the point of destruction by later activities in the same place. Such features are not significant and do not require further attention from the permitting agency or the project proponent. Potentially, previously undisturbed features could be present in the project area, although the evidence to date is not particularly supportive of this. Should such features be encountered, they would represent a significant contribution to the history of Sebastopol and should be treated accordingly.

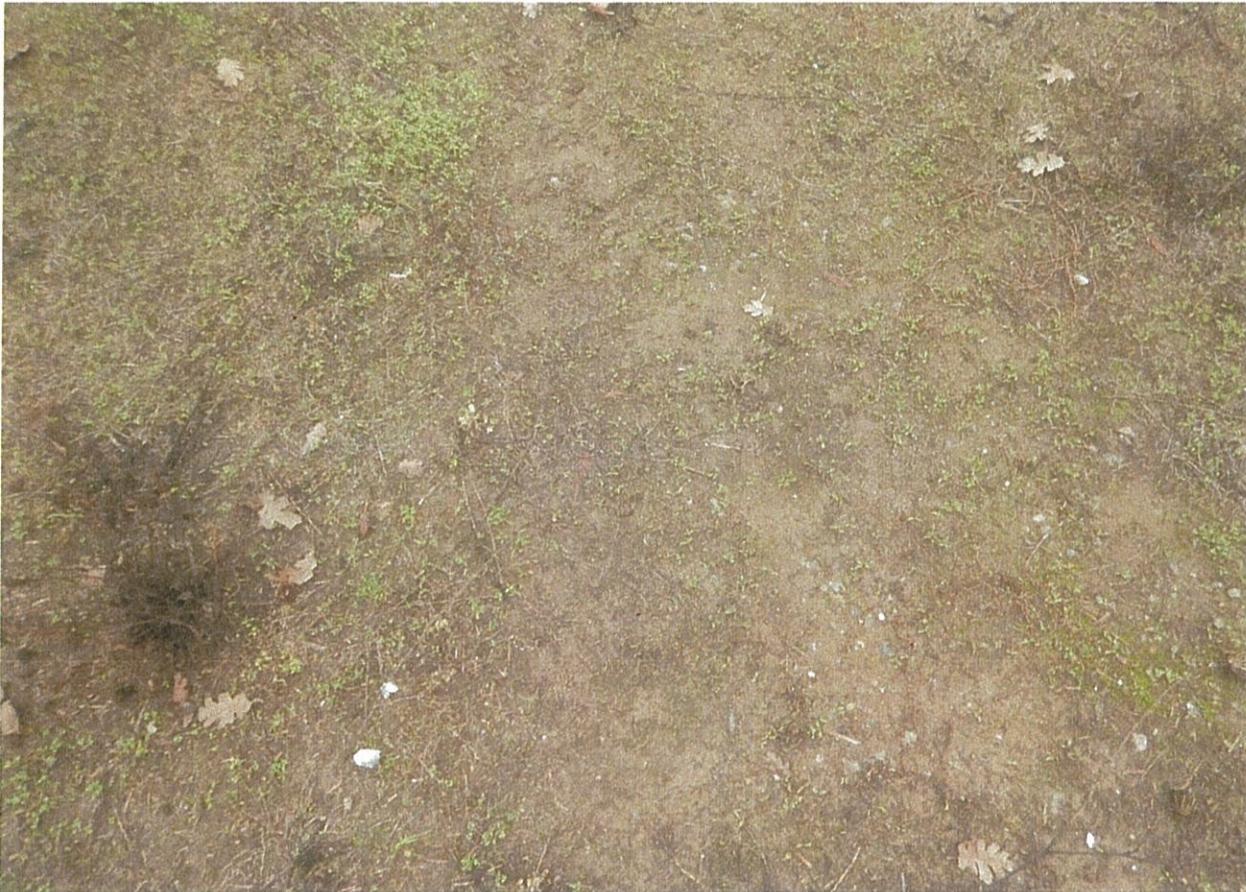


FIGURE 13 -- TYPICAL SURFACE SOILS OF THE PROJECT AREA

The white flecks are ceramic fragments. These appear to be fill soils and not the native soils of the area.

RECOMMENDATIONS

In the unlikely event that any unanticipated artifacts or cultural features are discovered during future grading or underground excavation for foundations or utility lines, Archaeological Resource Service recommends that all work in the vicinity of the find be stopped until the discovery area can be evaluated by an archaeologist. Depending on the extent and cultural

composition of the discovered materials, it may be advisable to have subsequent excavation monitored by an archaeologist, who should be ready to record, recover, and/or protect significant cultural materials from further damage. If the observed artifacts are historic and determined to be potentially significant as a deposit, the feature can be removed by archaeological field technicians, or the project can be modified to leave the feature in place. Historic era features tend to occupy less space than prehistoric features, making them more economical to recover, if necessary.

According to California Health and Safety Code 7050.5, the discovery of human skeletal remains anywhere within a project area requires that work be discontinued in the vicinity of the discovery, while the county coroner is contacted. If the skeletal remains are found to be prehistoric, Native American and not modern, then the coroner must call the Native American Heritage Commission (NAHC) in Sacramento within 24 hours. The NAHC will designate the "Most Likely Descendant" of the remains and the Most Likely Descendant will be responsible for recommending the disposition and treatment of the remains. Although the likelihood of encountering human skeletal remains in the project area seems very slight, it is important to have a procedure for alternate tasks that can be put into effect quickly in the event that human remains are discovered. This allows construction work to continue while the remains are investigated.

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APPENDIX 1— SIGNIFICANCE IN THE EVALUATION OF CULTURAL RESOURCES AS HISTORIC PROPERTIES

To be significant an archaeological site must qualify for registration as an “historic resource” the following criteria must be met for this listing:

An archeological site may be considered an historical resource if it is significant in the architectural, engineering, scientific, economic, agricultural, educational, social, political, military or cultural annals of California (PRC § 5020.1(j)) or if it meets the criteria for listing on the California Register (14 CCR § 4850). CEQA provides somewhat conflicting direction regarding the evaluation and treatment of archeological sites. The most recent amendments to the CEQA Guidelines try to resolve this ambiguity by directing that lead agencies should first evaluate an archeological site to determine if it meets the criteria for listing in the California Register. If an archeological site is an historical resource (i.e., listed or eligible for listing in the California Register) potential adverse impacts to it must be considered, just as for any other historical resource (PRC § 21084.1 and 21083.2(l)). If an archeological site is not an historical resource, but meets the definition of a “unique archeological resource” as defined in PRC § 21083.2, then it should be treated in accordance with the provisions of that section.

If an archaeological site does not qualify for listing, the directive is clear. The Public Resources Code states:

(4) If an archaeological resource is neither a unique archaeological nor an historical resource, the effects of the project on those resources shall not be considered a significant effect on the environment. It shall be sufficient that both the resource and the effect on it are noted in the Initial Study or EIR, if one is prepared to address impacts on other resources, but they need not be considered further in the CEQA process.

APPENDIX 2 – PROFESSIONAL STANDARDS FOR CONSULTANTS

Secretary of the Interior's Standards

The minimum professional qualifications in archeology are a graduate degree in archeology, anthropology, or closely related field plus:

1. At least one year of full-time professional experience or equivalent specialized training in archeological research, administration or management;
2. At least four months of supervised field and analytic experience in general North American archeology; and
3. Demonstrated ability to carry research to completion.

In addition to these minimum qualifications, a professional in prehistoric archeology shall have at least one year of full-time professional experience at a supervisory level in the study of archeological resources of the prehistoric period. A professional in historic archeology shall have at least one year of full-time professional experience at a supervisory level in the study of archeological resources of the historic period.



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PLANNING COMMISSION
MEETING OF: October 23, 2018
SEBASTOPOL YOUTH ANNEX
425 MORRIS STREET

APPROVED MINUTES

PLANNING COMMISSION
CITY OF SEBASTOPOL
MINUTES OF OCTOBER 23, 2018

SEBASTOPOL YOUTH ANNEX
425 MORRIS STREET

PLANNING COMMISSION:

The notice of the meeting was posted on October 18, 2018.

ANNOUNCEMENT: Please turn off all cell phones and pagers during the meeting.

1. CALL TO ORDER: Chair Douch called the meeting to order at 7:01 p.m.

2. ROLL CALL:

Present: Chair Douch, and Commissioners Glaser, Wilson, Fernandez, Doyle and Fritz
Absent: Vice Chair Jacob and Commissioner Kelley (excused)
Staff: Kari Svanstrom, Planning Director
Dana Morrison, Assistant Planner
Rebecca Mansour, Planning Technician

3. APPROVAL OF PLANNING COMMISSION MINUTES: There were none.

4. COMMENTS FROM THE PUBLIC ON ITEMS NOT ON AGENDA: This is for items *not* on the agenda, but that are related to the responsibilities of the Planning Commission or City Council. The Commission and Council receive any such comments, but under law, may not act on them. If there are a large number of persons wishing to speak under this item, speaking time may be reduced to less than 3 minutes, or if there is more than 15 minutes of testimony, the item may be moved to the end of the meeting to allow agendaized business to be conducted.

Chair Douch asked for comments from the public on items not on tonight's agenda.

Linda Berg, a resident of Sebastopol, commented:

- Respectfully requested that the Chair asked those in attendance to power off their cellphones for health and safety.

Chair Douch responded in the affirmative and asked those in attendance to power off their cellphones as requested by Ms. Berg.

Ms. Berg comments continued:

- Objects to being cooked alive.
- We're all being fried by our wireless devices.
- This is a trillion-dollar industry that is based on the ignorance of consumers.
 - They depend on the continued ignorance of consumers to keep that money rolling in.
 - This comes at the expense of our public health.
- The cell tower at the hospital has caused a cancer cluster at Petaluma Avenue Homes.
 - A number of people eloquently spoke about this issue at the last City Council meeting.
 - This cell tower should be removed/relocated at least 1,500 feet away from any residence.
 - The City is going to adopt a resolution and write a letter of support for these people.
 - The hospital gets about \$3,000 per month from Crown Castle for hosting that tower.
 - This is just the beginning.
- The City needs to start looking at everything that comes before it for a permit through the lens of how much microwave radiation and magnetic fields the project will bring.
- Demonstrated use of an EMF meter and spoke on the high levels of microwave radiation in the meeting room.
- Thanked the Commission for their time.

Hearing nothing further, Chair Douch closed the public comment period.

5. STATEMENTS OF CONFLICTS OF INTEREST: There were none.

6. PLANNING DEPARTMENT REPORT (Update on Future Agendas, Action of Other Boards and City Council)

Director Svanstrom provided the following updates:

- The Zoning Ordinance was approved by the City Council on October 16, 2018 and will go into effect on November 15, 2018.
 - The second reading will take place on October 30, 2018.
 - Some additional items (errors and omissions) are included in an errata as well as a couple of other minor edits that were raised during the last Commission meeting.
- At the City Council meeting on October 30, 2018, the Council will review:
 - A telecom urgency ordinance establishing a moratorium for any new small cell sites in the public right-of-way. This is intended to be in effect while the telecommunications ordinance is being updated.
 - The last update to our telecommunications ordinance occurred in 1996.

- An initial discussion of the telecommunications ordinance is expected to be before the Commission in late November.
- On Sunday, November 4, a Housing Fair is being held at the Community Center from 1:30 p.m. to 5:00 p.m.
 - Commissioner Fritz will be on the panel.
- Invited Commission members to notify her of any anticipated absences, etc. for purposes of the draft City Council liaison list that she has been working on.
- A revised workplan will be returning to the Commission as well.
- Sonoma State University's Annual Planning Commissioners Conference is being held on Saturday, December 2.
 - Requested that Commissioners let her know if they plan on attending, if they haven't done so already, so she can register everyone later this week.

Commissioner Glaser commented:

- Planning Commission education on telecom is greatly needed.

Director Svanstrom responded in the affirmative and noted that that was being worked out with the City Attorney and consulting firm, Meyers | Nave.

Commissioner Fernandez commented that he plans on attending the Commissioners Conference at Sonoma State University.

7. CONSENT CALENDAR (PUBLIC HEARING IF REQUESTED): (none)

8. PUBLIC HEARING:

- A. **ESOS [ENVIRONMENTAL AND SCENIC OPEN SPACE] MODIFICATION REQUEST:** This is an application requesting either: a) an exemption for the project from the required ESOS requirements or; b) a reduction in the scope of the ESOS resource analysis study for the site located at 6737 Sebastopol Avenue. The request for exemption is due to the existing character of the property, which was previously developed with industrial uses and remains relatively void of environmental resources. This request is a contemplated request under the ESOS regulations, Section 17.46.070 of the Zoning Ordinance. The application is also requesting a reduction in the 100' setback from the Railroad Forest to 50', as allowed by Section 17.46.050.B(1).

Director Svanstrom presented the staff report and was available for questions.

The Commission asked questions of Director Svanstrom and Assistant Planner Morrison.

Hearing no further questions, Chair Douch asked if the applicant wished to make a presentation.

The applicant, Dan Davis, gave a brief presentation and was available for questions.

The Commission asked questions of Mr. Davis.

Hearing nothing further, Chair Douch asked if members of the public wished to speak on this item.

Ms. Berg commented:

- Expressed feeling sick from having to be in this environment with unhealthy levels of microwave radiation while waiting for her turn to comment on this application.
 - Demonstrated use of an EMF meter and spoke on the high levels of microwave radiation in the meeting room.
 - A growing number of people are sensitive to microwave radiation.
 - Requested that members of the public be allowed to comment earlier in the process.

Chair Douch asked Ms. Berg to limit her comments to those related to the Davis Townhomes application.

- The entire Laguna is an ideal habitat for ticks.
 - Has collected 400 ticks.
 - Has been dealing with Lyme disease for nearly thirty years.
 - Conducted tick tock Tuesday meetings for over four years. More than 1,300 people who were desperately ill participated.
 - There is no cheap and easy treatment for Lyme disease.
 - If this project is built, one can assume that a lot of the residents will have dogs and will use the wonderful trails along the Laguna.
 - Unfortunately, the trails are filthy with ticks.
- Levels of microwave radiation should also be considered.
- If we don't pay attention to these things, we'll end up with more cancer clusters and more Lyme disease.
- Dr. Kenneth Stoller states that more than fifty million Americans have Lyme disease.
- Does not think that this project should be built.
- Has an inactive contractor's license herself.
- There are ways to protect from radiation.
- Thanked the Commission for their time.

Lynn Deedler, a resident of Sebastopol, commented:

- This lot could be a community treasure.
- This lot should be owned by the City of Sebastopol.
- This lot is at the vertex of the two most beautiful lots in all the Laguna.
- This is a really scenic site.
- Sebastopol has considered itself a steward of the Laguna.
- From hotel to hotel, the City Sebastopol has turned its back on the Laguna.
- This site provides the last opening that we have to the prettiest part of the Laguna within our city limits.
- Sebastopol has a lot of public facilities that are aging and too small.
- This would be a location for something that is open to the public.
- This site would be more valuable if owned by the City.
- This project will put two-story buildings along the prettiest part of the Laguna.
- There are many alternative developments that come to mind for him.
- While he is not against this project, he would prefer that the City find a way to make it a public piece of property that we can all share and enjoy.
- Thanked the Commission for their time.

Hearing nothing further, Chair Douch closed the public hearing and brought it back to the Commission for discussion.

Director Svanstrom noted that the Planning Department received one written comment from a member of the public which was provided to the Commission prior to the start of tonight's meeting.

Chair Douch asked for Commission discussion.

Commissioner Glaser commented:

- Supports what has been requested so far.
- A 50' setback seems like more than enough.
- When looking back over the last twenty years you can see that the site has been impacted before.
- It can take a very long time, if ever, before a site can return to its natural state.
- This project gets the site closer to its natural state, in some respects.
- Has additional questions of the applicant, with regards to the project itself, as this project moves forward through the process.
- In looking at aerials and pictures, unlike this project, all adjoining sites run right up against the Laguna.

Commissioner Wilson commented:

- While this property is close to the Laguna, in terms of appearance, this property does not look like a treasure.
- The city desperately needs housing.
- This is a good proposal.
- There are not many 1.7-acre properties available that can accommodate housing inside city limits.
- Reiterated the need for housing.
- Agrees with staff's recommendations on the conditions for approval.

Commissioner Fritz commented:

- Concurred with fellow Commissioner comments.
- While this property is adjacent to the Laguna, it is not in good shape.
- The property is fairly devoid of native habitat.
- It will be important to make sure that the storm water treatment facilities for this parcel are appropriate and adequate to filter any additional runoff water before it gets into the Laguna.
- Agrees with staff's recommendation to move the bioretention area out of the 50' setback.
- We desperately need housing, and this is an appropriate location for it.
- As an advocate for higher density, he'd like to see more than 18 units.
- This project is decently laid out and provides a good amount of open space for the residents to use.
- This will enhance the site over its existing condition.
- Expressed having concerns regarding lot #18 as it slightly encroaches into the 50' setback.
 - In addition, the drop off in landscape in the backyard is not viable as it would render the backyard fairly unusable.
 - It may make sense to move lot #18, and perhaps the lot next to it, forward to the east a little bit.
 - Doing so could make the backyard more usable and would remove it from being within the 50' setback.

- Reducing the depth of lot #18 could accomplish the same thing as it is deeper than some of the other lots.
- Supports staff's recommendation for the 50' setback and modifications to the ESOS study.

Commissioner Fernandez asked staff during which months the nesting period takes place.

Director Svanstrom responded that the nesting period usually takes place from April to November.

Commissioner Fernandez commented:

- Shares concerns on lot #18.
- Concurs with fellow members of the Commission on this request being appropriate.
- However, unfortunately 2-story units are not viable for seniors.
- Oftentimes enforcement of conditions can be difficult; wants clear provisions that can be realistically monitored and enforced.
- Suggested signage to notify public of sensitive areas near the Laguna.
- Supports 50' setback. A 50' setback will wind up better than requiring 100' and leaving the area as is.

Commissioner Doyle commented:

- Thanked staff for a thorough and clear staff report.
 - The findings were well described and the conditions of approval well written.
- Supports the 50' setback.
- Expressed concern over lot #18 encroaching into the 50' setback.
 - The design of lot #18 is not consistent with the recommended conditions of approval.
 - Seems like there is room in the site to move lot #18.
 - If the applicant considered taking lots #6, 7, 8, and 9 and turning them back to the grid with the other units in the same diagonal grid, lot #18 could be moved next to lot #9 and there would still be space to move the bioretention area (#4) over the line.
 - As it is right now, sees an opening for someone to contest the project.
- Other than the issue he spoke of he expressed being fully supportive of the project.

Chair Douch commented:

- Echoes comments made by fellow Commissioners.
- Supports this application as conditioned.
- Seconded Commissioner Doyle's comments on the staff report being very well done.
- The last time the Commission looked at this application, the Commission requested additional information.
 - Clarifications and additional information within the staff report give him confidence in his sense that the 50' setback is appropriate in this case.
- The issues raised regarding lot #18 need to be remedied for consistency.
- Supports this project.

Commissioner Glaser asked clarifying questions of staff.

Director Svanstrom responded.

Commissioner Doyle made a motion to adopt the conditions of approval as recommended by staff with the following modification:

- Requested the following be inserted as a new b), after a); *b) Lot #18 shall be relocated to be outside of the 50' setback area.*

Commissioner Glaser asked a clarifying question of Commissioner Doyle.

Commissioner Wilson commented:

- Whether or not the ESOS study can be limited to a visual study is what is before the Commission at this time.
- The other issue for the Commission to decide is whether or not this project is entitled to a 50' setback with the conditions as outlined as staff.
- What isn't before the Commission at this time is the tentative map or the conditions of the map.
 - Will defer his questions on the tentative map until the appropriate time.
- Suggested amended language for the motion.

Commissioner Doyle amended his motion to adopt the conditions of approval as recommended by staff with the following modification:

- Requested the following be inserted as a new b), after a); *b) No residential lots, improvements (trails, fences, picnic benches, stormwater facilities, etc.) or structures shall be permitted inside the 50' setback buffer area.*

Director Svanstrom clarified that the motion was to approve the limited ESOS review and the reduction of the 50' setback subject to the recommended conditions of approval with the modification as stated by Commissioner Doyle.

Commissioner Doyle concurred.

Commissioner Glaser seconded the motion.

Commissioner Fernandez commented:

- Supports the motion.
- Wants to ensure systematic monitoring of conditions of approval in the future, not just during the initial stage.

Director Svanstrom spoke on the City's process in terms of monitoring compliance.

Commissioner Glaser commented that that type of general concern, in terms of systematic monitoring, should apply to any project, not just this one.

AYES:	Chair Douch, and Commissioners Glaser, Wilson, Fernandez, Doyle, and Fritz
NOES:	None
ABSTAIN:	None
ABSENT:	Vice Chair Jacob and Commissioner Kelley

Commissioner Wilson commented, for Brown Act purposes, comments or questions should be held until the appropriate point in the process in terms of what is on the agenda.

Director Svanstrom suggested that Commission members submit comments on issues they'd like the applicant to address to her which she could then forward to the applicant. Doing so would not be a Brown Act issue so long as it is done on an individual basis.

9. DISCUSSION: There were none.

10. WRITTEN COMMUNICATIONS: There were none.

11. ADJOURNMENT: Chair Douch adjourned the meeting at 8:27 p.m. The regularly scheduled Planning Commission meeting on Tuesday, November 13, 2018 has been canceled. The next meeting of the Commission will be held on Tuesday, November 27, 2018, at 7:00 p.m. at the Sebastopol Youth Annex, 425 Morris Street, Sebastopol, CA 95472.

Respectfully Submitted By:

Kari Svanstrom
Planning Director



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APPROVED MINUTES

DESIGN REVIEW BOARD
SPECIAL MEETING
CITY OF SEBASTOPOL
MINUTES OF November 14, 2018

SEBASTOPOL CITY HALL
CONFERENCE ROOM
7120 BODEGA AVENUE
4:00 P.M

DESIGN REVIEW BOARD:

The notice of the meeting was posted on November 08, 2018.

1. CALL TO ORDER: Chair Luthin called the meeting to order at 4:00 P.M.

2. ROLL CALL:

Present:	Ted Luthin, Chair Cary Bush, Vice Chair Lars Langberg, Board Member Gregory Beale, Board Member Christine Level, Board Member Ron Hari, Board Member
Absent:	None
Staff:	Kari Svanstrom, Planning Director Dana Morrison, Assistant Planner Rebecca Mansour, Planning Technician

3. APPROVAL OF MINUTES: October 17, 2018 Tree Board & Design Review Board

Board Member Level amended the Design Review Board minutes of October 17, 2018.

Vice Chair Bush made a motion to approve the Tree Board minutes of October 17, 2018 as submitted, and the Design Review Board minutes of October 17, 2018 as amended.

Board Member Beale seconded the motion.

AYES: Vice Chair Bush, Board Member Beale, Board Member Langberg, and Board Member Hari

NOES: None

ABSTAIN: Chair Luthin and Board Member Level

ABSENT: None

4. PLANNING DEPARTMENT UPDATE ON MATTERS OF GENERAL INTEREST:

Director Svanstrom provided the following updates:

- The City has advertised for three openings on the Design Review Board and two openings on the Planning Commission.
 - Encouraged the three incumbents (Chair Luthin and Board members Bush and Beale) whose terms are ending to reapply.
 - The deadline to apply is coming up soon, in the event that enough applications are not received, the deadline will be extended.
- The City Council recently adopted the new Zoning Ordinance which goes into effect tomorrow, November 15, 2018.
- Two Public Arts Committee meetings were held last week to hear from the finalists for the art piece in front of the library.
 - The maquettes for both pieces will be on display at the Sebastopol Library through the Thanksgiving weekend.
 - Comment cards will be available and collected.
 - Encouraged interested persons to stop by, check out the maquettes, and provide their input on a comment card.

Board Member Langberg, who serves on the Public Arts Committee as well, commented:

- After over twenty applicants, the Committee chose three finalists.
 - One of the finalists backed out.
 - The two remaining artists and proposals are very different.

Director Svanstrom updates continued:

- The Housing Fair that was held on Sunday, November 4, 2018 saw about 150 attendees. City staff, County staff, and other organizations were present to help residents who may be interested in accessory dwelling units, junior accessory dwelling units, and other home-sharing opportunities to try to address the housing needs of our community.

The Board had no questions for Director Svanstrom.

5. COMMENTS FROM THE PUBLIC REGARDING ITEMS NOT ON THE AGENDA: There were none.

6. STATEMENTS OF CONFLICT OF INTEREST: There were none.

7. CONSENT CALENDAR: There were none.

8. REGULAR AGENDA:

A. PLANNED COMMUNITY POLICY STATEMENT AND DEVELOPMENT PLAN REVIEW: Review of the Policy Statement and Development Plan for a Planned Community referred to as the Davis Townhomes project. Project proposes to develop eighteen (18); 1,180 square foot, 2-bedroom, 1.5 bathroom, 2-story townhomes. The proposed development would occur on a vacant lot located at the end of Morris Street, south of Sebastopol Avenue/Hwy 12 (6737 Sebastopol Avenue).

Assistant Planner Morrison noted that an updated handout had been provided to the Board prior to the start of this meeting and presented the staff report.

The Board asked questions of Assistant Planner Morrison as well as Director Svanstrom.

The Board asked questions of the applicant as well.

Chair Luthin asked if the applicant wished to make a presentation.

Kathy Austin (Project Architect) introduced Parker Smith (Landscape Architect), Dan Davis and Mrs. Davis (Property Owners) and gave a presentation. She was available for questions.

Mr. Smith gave a brief presentation as well.

Ms. Austin made closing remarks and expressed that she, Mr. Smith, and Mr. and Mrs. Davis were available for questions.

The Board asked questions of the applicant.

Director Svanstrom made a clarifying statement on process.

The Board asked additional questions of Director Svanstrom and the applicant.

Chair Luthin asked if members of the public wished to speak on this item.

The property owner of 6681 Sebastopol Avenue commented:

- Her property abuts the property that is proposed for development.
- Uses this property as a rental.
- Brought her property at 6681 Sebastopol Avenue back to pristine condition after it was once a homeless encampment.
 - She did this specifically to earn money for her retirement.
- Expressed concern over the impacts that the lights from the parking lot may have on one or more of the units on her property.
 - Needs to have her units rented.
 - Spoke on the financial impact that not being able to rent one or more of her units could have.
 - Requested that the lights be adjusted so that they won't be shining on her property.
- Would like to have a solid fence, preferably not wood, between this project and her property for privacy.
- The proposed location for trash collection is very closer to the back of the fence.
 - Expressed concern over the amount of trash that will collect there with a total of eighteen units.
 - Requested that trash collection occur elsewhere, away from her fence line.
- Expressed concerns over the traffic that this project will bring.
 - Automobile lights from cars that are parking at night will shine through into her rental areas.
- Thanked the Board for hearing her concerns.

Board Member Beale asked a clarifying question of the public speaker.

Ms. Austin responded:

- Thanked the public speaker for her comments and encouraged her to get in touch with the property owner so he could coordinate with her as this project moves forward.
- Considering a concrete fence in the area but wants to work with the neighboring property owner to ensure that it meets what she would like to see.
- Wants to be good neighbors and wants to address concerns.

- Is considering using a light fixture that includes a baffle, so the light would not project backwards.
- With regards to the trash enclosure, the developer must work with the local trash hauler to confirm exactly what they want.
 - The proposal is consistent with what she has designed in the past for similar projects and is subject to change based on feedback from the trash hauling company.

The property owner of 6681 Sebastopol Avenue responded:

- Would appreciate the opportunity to discuss fencing.
- Because lighting is such a concern, she would like to see what the proposed light would look like and how it would project.

The Board asked clarifying questions of Ms. Austin.

Lynn Deedler, a resident of Sebastopol, commented:

- All in all, this is a good project.
- The proposed density of the project is very appropriate.
- The units are comprised of three large blocks which allows for a huge common area.
 - Perhaps the large blocks are an appropriate tradeoff for the big common area.
 - Goes back and forth on that.
- These units do tend to look a lot alike.
 - Painting them strong, different colors may or may not mitigate that.
 - Suggested architectural distinction.
- Considers the concerns he expressed to be minor but worth considering.
- In looking at the color samples, the colors are very appropriate for the environment that they are in with one exception.
 - The off-white with the gray looks very tract home residential and clashes with the surrounding environment.

An unidentified man commented:

- This is a very attractive project.
- This project is very needed in our town.
- Suggested factoring in the common area when calculating FAR.
 - Suspects that owners of the units will have an undivided proportionate ownership of the common areas.

Hearing nothing further, Chair Luthin closed the public comment period and brought it back to the Board for discussion.

Chair Luthin spoke on process and asked to hear from the Board on the proposed Policy Statement.

The Board asked procedural questions of staff.

Board Member Langberg commented:

- Generally, the map, site plan, and density works and feels pretty good for the site.
- The Policy Statement seems accurate.
- Echoed Commissioner Fritz's comment on this project being able to be more dense.
- Doesn't have a problem with this development becoming more dense, however, it seems to work as proposed.
- Likes the common spaces and openness of the site.

- Has additional comments about the character and design of the project, orientation of buildings, etc., however, those are separate from the matter of the Policy Statement.

Vice Chair Bush commented:

- Feels the same as Board Member Langberg.
- The application is great.
- The proposal takes advantage of a rather narrow aperture.
- The project is well sited, has adequate circulation, and nice common areas.
- Would love for the project to have a denser quality as housing is such a huge need here.
- This is a good use for the site.

The Board asked clarifying questions of staff.

Board Member Level commented:

- Supports the proposed density.
 - It's nice to talk about higher density but we need to ensure that we have the infrastructure to support it first.
- The incorrect statement in the staff report on height needs to be clarified.
- Supports the 40' height as allowed in the Zoning Ordinance.
- Expressed being in support of the Policy Statement.

Board Member Beale commented:

- Sebastopol needs more housing, more density.
- This is a unique property.
- Adding more than the proposed amount of density could eliminate the character that has been carefully created by the applicant team.
 - What the applicant has come up with makes a lot of sense and shows their care for the community and town.
- Feels positive about what the applicant is trying to achieve here.

Board Member Hari commented:

- Concurs with Board Member Beale on the matter of density.
- Most people come to Sebastopol for a slight amount of open space.
 - People don't come to Sebastopol looking for high density development.
- Likes the proposal.
- Is a traditionalist in terms of style.
- Likes the traditional style of this project and thinks it fits well here.
- Hopes for a fairly high pitched roof.

Chair Luthin commented:

- Concurs with fellow Board Member comments.
- This is an appropriate project for where it is proposed.
- Going higher density with the intersection and easements would be very challenging and probably wouldn't fit the property.
- This project has been carefully crafted and he expressed being in support of it.
- Served on the General Plan Advisory Committee.
- Supports lower FAR in this case.
- Concurred with Board Member Level on height in that the applicant can go up to 40' if they wish.
- It is important to understand and respect the floodplain issues.
- The Downtown Core Planned Community chart that was provided was very helpful.

- The application itself is nicely organized and understandable.
- Reiterated his support for this project.

The Board asked a clarifying question of staff on item number four in the Policy Statement.

Chair Luthin surmised the following:

- The timeline, as indicated in item number five, looks reasonable.
- The Board is unanimously in support of this project.
- The Board supports the lower FAR.
- The Board supports a height limit of up to 40' as allowed per zoning.

The Board concurred.

Board Member Level commented:

- The 30' height limit is self-imposed by the applicant.
- Zoning allows for a height of up to 40'.

Chair Luthin asked to hear from the Board on this project in terms of preliminary Design Review.

Board Member Langberg commented:

- Because this is a very unique site in Sebastopol it begs an opening to a different kind of architecture than can be seen elsewhere in Sebastopol.
- The proposed style of architecture, that can also be seen in our downtown, does not feel appropriate.
 - It will probably work and many people who like a more traditional style will be happy with it.
 - Trying to replicate the character of what we have in other parts of town feels like a missed opportunity.
- This site is wide open and has some very eclectic buildings and beautiful landscape around it.
- The pallet could be much looser and the design more wide open.
 - Encouraged exploration of that.
- Does not see individuality in these units.
 - The differences between each unit are much too subtle.
 - At first glance the units look pretty much the same to him.
 - The use of different colors will help some.
 - Color should not be leading the design intention, it should be enhancing and reinforcing it.
- Understands the bike path issue, but the hierarchy between the front and back of the units does not feel right.
- The common space is beautiful and open.
- The rear yard feels uninviting.
- The façades of the buildings out to the courtyard and to the Railroad Forest could be much more open and inviting.
 - The design should encourage community.
- The cluster of buildings around the courtyard is nice and feels good.
- A majority of the units don't have views of the Railroad Forest.
 - It would be nice if it could open up more in that direction.
- Ms. Austin's work at The Barlow combined styles (industrial, barn, agricultural) in a very interesting way.
 - Without replicating it, this project could go there in a smaller scale.
- Architecture wise, the proposed design doesn't really feel like what this site calls for.

Vice Chair Bush commented:

- Concurred with Board Member Langberg.
- This is a unique site which presents a great opportunity for something more inventive.
- This proposal includes three really big buildings that have a lot of ornamentation.
 - In looking at this from the perspective of somebody looking to buy an affordable home, the ornate details that are being proposed could really drive up the price.
- These seem like row houses.
- The proposal doesn't contextually fit the site.
- In looking at everything, the plan looks very busy.
 - It will be very hard to let your eyes settle into the space especially as an endemic connection to the Laguna.

Board Member Level commented:

- Doesn't see much differentiation between the units.
- These look like row houses.
- Concurred with Board Member Langberg and Vice Chair Bush on this being an opportunity for something more bold.
 - Thanked Board Member Langberg and Vice Chair Bush for their comments which were very well stated.
- Carports always seem to appear as an afterthought.
 - Would like the carports to tie-in to the buildings in some manner.
 - Does not want to see basic, prefabricated carports used here.
- Wants to see definite specifics when the applicant returns for Design Review that way the Board can be certain on what it's approving.

The Board asked clarifying questions of Ms. Austin.

Board Member Beale commented:

- Lots one through five that run parallel to Sebastopol Avenue provide noise and visual buffering, from the intersection down through the parking lot, for the courtyard which is a benefit.
- Concerned with lots ten through eighteen in terms of their relationship with Sebastopol Inn which is a two-story hotel.
- Likes that the project is more oriented towards the south and the Laguna.
- The fact that this site is in the Downtown Core while being removed from the main drag makes it unique.
- It makes sense to orient housing away from the downtown.
- This process is not intended to feel like an interrogation.
 - The fact that it has felt like an interrogation at times is not appropriate.
 - Understands the importance of being specific about what's critical, what's important, and what is not.
 - Asked fellow Board members to do things more positively.

Board Member Hari commented:

- Reiterated that he is a traditionalist.
- What the applicant is proposing will cost a lot of money.
- When thinking of affordable or moderate housing, it's hard to be creative and save money.
- This is not a very visible site.
 - If this site were highly visible, he may be more inclined to want to see it spiffed up a bit.

- The Board should not put anything extra on the applicant if this is what they've worked out because it will wind up costing them more money.
 - It's not appropriate for the Board to require changes that will wind up costing the applicant more money.

Board Member Beale commented:

- We're not designing and building the vineyard home on the top of a hill.
- There is a purpose behind this development.
 - The Board's input should be cognizant of that.
- This development will be seen most from the bicycle trail.
 - The fact that there is less of a wall of structure facing the bicycle trail is a positive.

Board Member Level commented:

- Does not object to the design if it is what the applicant wants to use.
- This is not an affordable housing project, this is a market rate housing project with the possibility of inclusionary units.
- What they decide to spend on this project will affect the future value.

Chair Luthin commented:

- The townhomes that are behind the historic homes on South Main Street totally make sense there.
 - That makes a little less sense here, in this location.
- Concurred with Board Members Level and Hari on some people being traditionalists, which is okay with.
- There is nothing objectionable about the proposal, if this is what the property owner wants.
- While he understands that people will want their private space, he expressed concern over the 6' fence that will be facing the common area on lots one through five.

Ms. Austin responded that the fence could be 4' solid with an additional 2' of lattice.

Chair Luthin commented:

- Likes the idea of 4' solid with 2' of lattice better than a solid 6'.
- Could support a 6' solid fence for the areas that do not face the common area.

The Board asked a clarifying question of Ms. Austin.

Chair Luthin commented:

- Interested in seeing a real rendering of what the units will look like with the proposed color scheme.
- In general, he does not like relying on color to give individual units distinction.
- These feel like miniature single family homes that are connected.
- Agrees are there missed design opportunities here, but there usually are.
- The design is not inappropriate for Sebastopol.
- Supports this project.

Vice Chair Bush commented:

- Two escaped exotic species are on the landscape plan which makes him nervous from a contextual and endemic standpoint being close to the Laguna.
- Some of the trees in the tree legend are short lived trees.
- The style of the homes is the applicant's decision.
- How the homes fit with the landscape is vital.

- Some of the roofing positions are awkward.
- The mass of the buildings, in terms of the whole profile, strikes him as a little less than consistent.
- Agrees with the rest of the Board in there being a project here.
- There's a lot going on here and the project will be pretty impactful.
- The huge long plant list will only contribute to the project being even more impactful.
- Suggested quieting the site down.
- As proposed, the maintenance of this project will be huge.
 - Easy maintenance is desirable.

Board Member Beale commented:

- With the landscaping plan, carports, and orientation of the different buildings, it's almost impossible in reality to stand at any given point and see the elevation the way it's being shown by the applicant in this submittal.
 - At best, you'll be looking down the face of the buildings more along it versus at it from most places.
- Encouraged the applicant to provide different perspectives when they return for Design Review.

Ms. Austin thanked the Board for their comments and asked a clarifying question of staff.

Hearing nothing further, Chair Luthin concluded Board discussion of this item.

Chair Luthin adjourned the meeting at 5:55 p.m. for a brief break.

Chair Luthin reconvened the meeting at 6:03 p.m.

B. DESIGN REVIEW, MAJOR: This is a Major Design Review application, proposing a revision to the approved Design for the French Garden Inn (an Inn proposed to be located at 8060 Bodega Ave, next to the former French Garden [now Gravenstein Grill]). New owners have purchased the property and decided to proceed with changes to the proposed Inn, including reducing the number of hotel rooms from 18 to 11 and reducing the project to a one-story design.

Assistant Planner Morrison presented the staff report.

The Board asked questions of Assistant Planner Morrison as well as Director Svanstrom.

Chair Luthin asked if the applicant wished to make a presentation.

Gannon Tidwell, project sponsor/property owner, made a brief presentation and was available for questions. He also introduced co-sponsor/co-owner, Rich Springer.

Paul Nyulassie, project architect, made a brief presentation and was available for questions.

The Board asked questions of Mr. Tidwell, Mr. Nyulassie, and Mr. Springer.

The Board also asked additional questions of staff.

Chair Luthin asked if members of the public wished to speak on this item.

ila Benavidez-Heaster, a resident of Sebastopol, commented:

- The prior developer promised her that a hostel would be included.

- Requested consideration of a hostel here too.
- Encouraged incentives/discounts for guests who arrive on a bicycle.
- Understands the issues that may come with adding a hostel.
- Reiterated her request for consideration of a hostel within this project.
- Thanked the applicant for giving her request some thought.

Mr. Tidwell and Mr. Springer asked clarifying questions of Ms. Benavidez-Heaster and commented that they would consider the feasibility of adding a hostel room and giving incentives/discounts for guests who arrive on a bicycle.

Chair Luthin commented:

- Ms. Benavidez-Heaster made the same request for a hostel of the owners and operators of Hotel Sebastopol and they were able to add them into their project.
 - One of the reasons that they were able to include them was based on the tremendous number of bicycle tour groups, where unrelated people want to share a room, that stay at their other hotel, Hotel Healdsburg.

Hearing nothing further, Chair Luthin closed the public comment period.

Board Member Hari commented:

- Expressed concern over the last-minute nature of this request and the pressure that is being placed on the Board to decide on this project during this meeting.
 - While he doesn't object to the Board having to decide on the project during this meeting, it doesn't seem to make a lot of sense.
 - Doesn't object to this project at all, however, he does have several questions about this application.

Assistant Planner Morrison and Director Svanstrom responded on process. The applicant spoke on process and timing as well.

The Board asked clarifying questions of staff.

Chair Luthin asked for Board discussion on this application.

Board Member Beale commented:

- Concerned because the information that has been provided by the applicant has a lot of inconsistencies and some of them could present major issues.
- Loves this concept.
- This is a huge improvement from what was previously proposed.
- Really like this approach.
- Spoke on the difficulty of this process.
- This project may benefit from going through the complete process because at this point, if the Board were to approve this project, it would be based on a lot of assumptions or a humongous list of conditions.
- Works in the design and construction industry himself.
- As difficult as this process is making things for the Board it may also be doing a disservice to the applicant.
- Has additional, more specific comments on design which he will save until later.

Board Member Level commented:

- In addition to Board Member Beale's comments, trying to process this application in this fashion is a bit deceptive to the public as this is clearly not a revision to the prior proposal.

The Board asked clarifying questions of staff.

Board Member Level commented:

- There are a lot of issues with the thoroughfare fronting this project.
- There are members of the public that would want to comment on this proposal.
- Supports the basic proposal and concept in general.
- Concerned by internal inconsistencies over site, landscape and pathway issues.

The applicant asked a clarifying question of Board Member Level.

Board Member Level commented:

- Uncomfortable by the fact that this plan doesn't feel resolved or final and yet the Board is being asked to act on it.

Chair Luthin commented:

- In his recollection of the process for the prior application, the main issues were height and privacy (the neighbors behind the development were concerned due to the two-story height of the project).
 - In general, the neighbors were pretty supportive of the project.
 - Does not remember a lot of opposition.
 - Public sentiment could have changed between now and then.

Board Member Level commented:

- Concerns have to do with transparency.
- The public commented on the prior project, not on this one.
- Shouldn't give off the perception of favoritism.
- Not expecting a problem with this application.
- Not having a proper process could be a pitfall for the City.

The Board asked clarifying questions of staff.

Board Member Beale commented:

- What is unique here is the fact that the potential impact on load, use, and scale went down significantly but this is an entirely different project from what was originally approved.

Board Member Level commented:

- Issues could arise that could present liability issues for the City, particularly without there being management onsite 24/7.

Vice Chair Bush commented:

- Thanked the applicants for bringing this project to the Board.
- A lot of work has gone into this.
- The Board wants to make sure that they understand what they're being asked to approve and how it all relates to each other.
- While he understands where other Board members are coming from, he does not object to the applicant bringing this application forward at the last minute because they are following a set process.
- The proposal is not all that clear.
- The proposed edible garden will need a maintenance plan.
 - The reality of an edible landscape is that they can be really tricky.
- There is a huge ADA compliancy issue that comes with using crushed gravel/rock for the flatwork, if that is what the applicant wants to use.

- Would like to see a calendar of operations with regards to how this will all be maintained, not just when it comes to landscape.
- If landscape lighting is desired, that would be a huge component that the Board would also like to review.
 - Landscape lighting can add a great flavor for the type of client you're trying to attract.
- The plant legend looks great.
 - Does not like how the rosemary is shrubbed up around the buildings.
 - Almost all of the buildings are shrubbed up by various plantings.
 - Almost all of the plants that are around these buildings will reach up to the eaves.
 - The plant legend says that no pyrophytic plants are proposed but rosemary is a pyrophytic plant.
 - Water use is great.
- The plant legend does not reflect the look of what was presented in the rendering of the courtyard.
 - Prefers the open, airy feel of the courtyard in the rendering.
- Likes that smaller units, which people are starting to gravitate towards, are being proposed.
- Really likes this project.
 - This project is different, unique and inventive.
 - The applicant has created a place and that is what creates a really good project.
 - This looks like an exciting environment that could become a community space.
- Accessibility is important, all people have to be included.
- Approves of this project in general.
- The Board would have to attach a lot of conditions if it were to try to approve this project tonight.

Board Member Langberg commented:

- Vice Chair Bush's comments were well said.
- This submittal feels more like Preliminary Design Review.
- This application is not complete enough for the Board to approve it.
- Coming up with an extensive list of conditions would almost equate to the Board designing the project for the applicant which is not appropriate.
- In general, the project looks great and has a really nice feel to it.
- Having to come up with a long list of conditions in order to grant final Design Review approval of this application does not feel like what the Board is charged to do.

Chair Luthin asked if there was a part of the project the Board felt it could approve, like the buildings.

Board Member Langberg commented:

- The buildings cannot be approved without ADA.
- The windows are not decided.
- The lighting plan is incomplete.

Board Member Beale commented:

- While some this is final, much of what was submitted feels preliminary.

The Board asked procedural and clarifying questions of staff.

Board Member Hari commented:

- The Board is trying to do the right thing.

- The consensus of the Board appears to be that granting final approval tonight is premature.

Chair Luthin commented:

- There are too many unknowns here.
- The Board has nothing significant that it can approve at this time.
 - The site plan cannot be approved because it may need to change due to ADA.
 - The building architecture cannot be approved because there are missing details and it may change.
 - The landscape plan and renderings presented are incompatible and need to be synchronized to accurately reflect the proposal.
- Supports a continuance.
- Would like to see the following:
 - The buildings need to be resolved.
 - Specifics are needed.
 - The design of the fence.
 - Whole site elevations.
 - A reduction of the rear gate height.
 - Bicycle parking needs further thought and development.
 - Signage.
 - Accessibility.
 - Solar.
 - Mechanical.
 - Landscape.
- The Board is excited about this project.
- Expressed being totally on board with this project.
- Reiterated that the Board needs something that it can approve.
- The Board has approved vague applications before and oftentimes doing so has resulted in issues.

The Board concurred.

The Board discussed process.

The applicant asked clarifying questions of the Board and staff.

The Board was in consensus on continuing this application pending approval of a new Use Permit.

Board Member Beale commented:

- The fact that this is a reduced scale project seems to have unrightfully set expectations for the applicant's in terms of what the Board would be looking for in order to be able to approve this application today.

The Board concurred.

Hearing nothing further, Chair Luthin asked for a motion.

Vice Chair Bush made a motion to continue this application to allow the applicant to go through the standard Use Permit process and eventually return to the Board with additional details on the following:

- Landscape plan in coordination with the site plan,
- Site work (flat work and ADA compliance),

- Lighting,
- Signage,
- Fence and gate details and elevations,
- Whole site elevations with long broad perspectives,
- Bike parking,
- Solar placement,
- Mechanical equipment placement (anything exterior, including the HVAC condensing unit),
- Trash enclosures and shed,
- Deck details,
- Barn door details

The applicant asked clarifying questions of the Board.

Chair Luthin commented that interior matters are outside of the Board's purview.

Board Member Level seconded the motion.

Chair Luthin asked for discussion of the motion.

Hearing none, the Board voted on the motion as follows:

AYES: Chair Luthin, Vice Chair Bush, Board Member Beale, Board Member Langberg,
Board Member Level and Board Member Hari

NOES: None

ABSTAIN: None

ABSENT: None

9. DISCUSSION ITEMS: There were none.

10. REPORTS FROM THE BOARD/STAFF: There were none.

11. ADJOURNMENT: Chair Luthin adjourned the meeting at 7:45 p.m. The next regularly scheduled Tree Board/Design Review Board meeting will be held on December 05, 2018 at 4:00 p.m., at the Sebastopol City Hall, 7120 Bodega Avenue, Sebastopol, CA.

Respectfully Submitted By:

Dana Morrison
Assistant Planner



City of Sebastopol
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PLANNING COMMISSION
MEETING OF: April 09, 2019
SEBASTOPOL YOUTH ANNEX
425 MORRIS STREET

UNAPPROVED DRAFT MINUTES

PLANNING COMMISSION
CITY OF SEBASTOPOL
MINUTES OF April 09, 2019

SEBASTOPOL YOUTH ANNEX
425 MORRIS STREET

PLANNING COMMISSION:

The notice of the meeting was posted on April 04, 2019.

ANNOUNCEMENT: Please turn off all cell phones and pagers during the meeting.

1. CALL TO ORDER: Chair Douch called the meeting to order at 7:00 p.m.

2. ROLL CALL:

- Present:** Chair Douch, and Commissioners Glaser, Doyle, Kelley, Wilson, Fritz and Oetinger
- Absent:** Commissioner Fernandez (excused)
- Staff:** Kari Svanstrom, Planning Director

3. ELECTION OF OFFICERS: The Commission will elect a new Chair and Vice Chair.

Chair Douch asked for nominations for Chair.

Commissioner Fritz nominated Commissioner Wilson to serve as Chair.

Commissioner Oetinger seconded the nomination.

The Commission voted as follows:

- AYES: Commissioners Douch, Doyle, Kelley, Glaser, Fritz and Oetinger
- NOES: None

ABSTAIN: Commissioner Wilson
ABSENT: Commissioner Fernandez

Commissioner Douch introduced Commissioner Wilson as Chair.

Chair Wilson requested that Commissioner Douch continue as Chair for the duration of this meeting.

Commissioner Douch responded in the affirmative.

Commissioner Douch asked for nominations for Vice Chair.

Commissioner Douch nominated Commissioner Doyle to serve as Vice Chair.

Chair Wilson seconded the nomination.

The Commission voted as follows:

AYES: Chair Wilson and Commissioners Douch, Doyle, Kelley, Glaser, Fritz and Oetinger
NOES: None
ABSTAIN: None
ABSENT: Commissioner Fernandez

4. APPROVAL OF PLANNING COMMISSION MINUTES: There were none.

5. COMMENTS FROM THE PUBLIC ON ITEMS NOT ON AGENDA: This is for items *not* on the agenda, but that are related to the responsibilities of the Planning Commission or City Council. The Commission and Council receive any such comments, but under law, may not act on them. If there are a large number of persons wishing to speak under this item, speaking time may be reduced to less than 3 minutes, or if there is more than 15 minutes of testimony, the item may be moved to the end of the meeting to allow agendized business to be conducted.

At the request of a member of the public, Commissioner Douch asked those in attendance to please turn off their cell phones if willing.

Chair Douch asked for comments from the public on items not on tonight's agenda.

Linda Berg, a resident of Sebastopol, commented:

- Thanked Commissioner Douch for making that request.
- There is a measurable difference between silencing cell phones and powering them off.
 - Silencing a cellphone does nothing in terms of reducing the microwave radiation that they emit.
- Showed how a dosimeter works.
- There is a growing number of people who are electrosensitive.
- The Commission should pay attention to what the City Council is doing to block 5G.
 - The Council is doing some good work that the Commission has no clue about.
- The Commission has not been very responsive about this issue.
- Spoke on her participation, through conference calls over the past six months, with efforts to stop 5G, etc.

- The issue of Lyme Disease needs to be addressed and the public needs to be educated.
- Thanked the Commission for their time.

Hearing nothing further, Commissioner Douch closed the public comment period.

6. STATEMENTS OF CONFLICTS OF INTEREST: There were none.

7. PLANNING DIRECTOR'S REPORT (Update on Future Agendas, Action of Other Boards and City Council)

Director Svanstrom updated the Commission on the following:

- The Council is reviewing an urgency ordinance for telecommunications.
 - They began a public hearing on April 2nd.
 - There were several comments from the public, including Verizon.
 - Staff and the City's outside counsel are reviewing those comments in order to determine whether modifications to the urgency ordinance should be made.
 - This item will be back before the Council on April 16th for further review.
 - The Council will be adopting both a regular and urgency telecom ordinance.
 - Both of those will be interim until the planning Commission completes its review.
 - The draft ordinance that Council is being asked to adopt will become the draft ordinance that the Planning Commission will use as a starting point.
- The IPA 10k special event run, which is in its third or fourth year, will be on the April 16th Council agenda as a consent item.
- An appeal was filed against the Planning Commission's approval of a vacation rental.
 - The date for the Council to hear the appeal has not yet been set.
- The Design Review Board issued the final design review approval that was required for Hotel Sebastopol and that was for their solar carports over the parking lot area.
 - This approval occurred at their last meeting which took place on April 3rd.
 - The applicant is working on the building permit process as well.
 - The developer is hoping to break ground early this summer.
- The Public Arts Committee will be having a retreat on May 1st.
- Happy to answer any questions the Commission may have.

The Commission asked questions of Director Svanstrom.

Commissioner Fritz commented:

- Bothered by the fact that CVS doesn't keep their trash bins inside their trash enclosure area.

Director Svanstrom responded that she would look into that.

8. CONSENT CALENDAR (PUBLIC HEARING IF REQUESTED): (none)

8. PUBLIC HEARING:

- A. USE PERMIT AMENDMENT:** ~~This is an application submitted by Richard Prather for Occidental Road Cellars requesting approval to amend their existing Use Permit to allow for the existing tasting room and production facility to be shared with a new wine brand. In addition, the applicant is requesting an increase to their allowed production. Their current Use Permit allows 2,000 cases per year; the~~

~~applicant is requesting an additional 8,000 cases per year, for a total of 10,000 cases per year over the next 10 years. No proposed changes to the existing operating hours are proposed, and no physical changes are proposed to occur to the actual production or tasting room facility. This application has been continued to the Planning Commission meeting of April 23, 2019.~~

- B. PLANNED COMMUNITY DESIGNATION, USE PERMIT, TENTATIVE MAP, MITIGATED NEGATIVE DECLARATION/INITIAL STUDY:** This is an application submitted by Kathy Austin and Dan Davis requesting approval for the Davis Townhomes project, proposing development of eighteen (18); 1,180 square foot, 2-bedroom, 2-story townhomes. The proposed development would occur on a vacant lot located at the end of Morris Street, south of Sebastopol Avenue/Highway 12, 6737 Sebastopol Avenue (APN 004-063-036). The application includes the following entitlements: Planned Community designation; Use Permit for a residential-only project in a commercial zone; a Tentative Map for a subdivision of more than 4 parcels; and, review of the Mitigated Negative Declaration/Initial Study. Further review by the City Council will be required, in addition to review by the Design Review Board.

Director Svanstrom presented the staff report.

The Commission asked questions of Director Svanstrom.

Commissioner Douch asked if the applicant wished to make a presentation.

Kathy Austin, project architect, gave a presentation and was available for questions.

Project Manager, Ken Cohen was present and available for questions.

Property owners, Dan and Robin Davis were also present and available for questions.

Dev Goetschius, Housing Land Trust of Sonoma County was also present and available for questions.

The Commission asked questions of Ms. Austin and Ms. Goetschius. The Commission asked additional questions of Director Svanstrom as well.

Hearing no further questions, Commissioner Douch asked if members of the public wished to speak on this item.

Marsha Sue Lustig, a resident of Sebastopol, commented:

- Spoke on this application when it first came before the Commission.
- Was very excited about this project for a variety of reasons.
- One of the main reasons she was excited about it was due to the relationship with the Housing Land Trust.
- Has experienced what happens when working with Housing Land Trust.
 - Property owners receive the benefit if price values increase.
- Has not observed this same success with deed restricted units.
 - The owner(s) will usually walk away with nothing.
- Appreciates the relationship between this property and the emergency services with Park Village.
- Would prefer for the emergency vehicle access (EVA) area to become a proper street as that would be best for the community.

- Barring that, hopes that the EVA will serve as a pedestrian link for people to be able to move freely.
- Appreciates seeing the area that is now open for native plantings.
- This is a good project.
- This project doesn't have a street frontage, if it did she may have additional design-related comments to make.
- Working with the Housing Land Trust is even better than relying on our local government.

ila Benavidez-Heaster, Bodega Avenue, commented:

- Last week at the Design Review Board meeting solar panels and how they interact with shade was discussed.
 - This may be something worth looking into given Ms. Austin's comment about shading from existing trees in some areas.

Hearing nothing further, Commissioner Douch closed the public comment period.

Commissioner Douch adjourned the meeting at 9:02 p.m. for a brief break.

Commissioner Douch reconvened the meeting at 9:10 p.m.

Members of the Commission discussed the process for their discussion.

Commissioner Douch asked to hear from the Commission on the request for a Use Permit to allow solely residential in a commercial zone.

The Commission agreed on being in support of this request.

Commissioner Douch asked to hear from the Commission on the request for a Planned Community rezone with associated development standards.

Commissioner Douch commented:

- The design presumes the approved plan will be the full build-out.
- Wished to add a condition that would prohibit additions.

Director Svanstrom commented:

- The applicant noted that the prohibition of additions could be in the CC&R's.
- One issue with CC&R's is that an HOA can change them without the City being made aware of said change.
- It would be better to make it a condition of approval in the project.

Ms. Austin commented:

- Would like extension of the rear porch or a deck to be allowed as it is not living area.

Commissioner Douch commented:

- Typically, a back porch would be allowed under this condition.
- A porch can extend into a setback.

Ms. Austin commented:

- Happy to not have a living space addition.
- Would be nice to allow adding on to a rear porch or deck.

Director Svanstrom commented:

- The allowance for an uncovered porch addition is up to 8' into the setback.
- The allowance for a covered porch addition is up to 2' into the setback.

Ms. Austin commented:

- That would be good.
- Vice Chair Doyle pointed out that she had done a 20' rear yard setback from the house, not the porch, so that should be amended to be correctly reflected.

Vice Chair Doyle commented:

- Aside from the issue with the back deck, the Commission could recommend approval of the design as proposed so the setback issue that Ms. Austin mentioned would just read as a typo which she could remove in future submittals.

Ms. Austin commented:

- Suggested that the language be revised to say that the rear yards are 14' instead of 20' due to the porches being 6'.
 - Lot #18 would be the exception.

Director Svanstrom commented:

- Read from the Zoning Ordinance.
- Table 17.100 on page 17-66 does allow projections into required yards.
- In terms of Planned Communities; if it's not called out as a specific restriction or requirement it would fall under the normal Zoning Ordinance and be treated that way.
- Patio roofs and similar elements projecting from and serving a residential facility, if such element does not exceed 12' in height and has open, unwallied sides along more than 50 percent of its perimeter can extend 8' into the rear yard.
- Uncovered porches having a mean height above grade of more than 2' can extend 10' into the rear yard.

Ms. Austin commented:

- They will be over by 2' on almost every case just to get out of the floodplain.
- Unsure on the best way to maneuver this.
- On the column where she lists the setbacks, it says 5' because it's referring to lot #18.

Vice Chair Doyle commented:

- Suggested that the skewed line of lot #18 be considered a side property line, not a rear property line.
- The applicant could specify that the steep property line of lot #18 shall be considered the side property line for purposes of setback.

Director Svanstrom commented:

- If the Commission moves forward with this, staff can revise the development table to include language that states that the sloped property line for lot #18 shall be considered the side yard for purposes of setback.

Ms. Austin commented:

- Responded in the affirmative.
- Somewhere in the project description 20' rear yards are mentioned.
 - That should change due to the porches being included within the setback.

Director Svanstrom asked Ms. Austin if the porches were covered and more than 12' tall.

Ms. Austin responded that they would be more than 12' tall in some cases to be out of the floodplain.

Director Svanstrom responded that the rear yard would need to be modified to include the minimum rear yard of the porches to be consistent.

Commissioner Douch commented:

- Suggested leaving the setbacks as 5' with a condition of approval that says that conditioned space additions are not allowed.
- In this case decks would be allowed.

Ms. Austin commented:

- Supports Commissioner Douch's suggestion.

Vice Chair Doyle commented:

- Doing as Commissioner Douch suggested would work for him, although, he was thinking of a 10' rear yard setback to be consistent.

The Commission and staff concurred on changing the rear yard setback to 10' with a condition of approval that say that conditioned space additions are not allowed.

Vice Chair Doyle commented that lot #6 could be moved forward 1' to still meet the front yard setback while meeting the required rear yard setback of 10'.

Ms. Austin concurred.

Commissioner Fritz commented:

- Expressed being fine with requiring a 5' rear yard setback rather than 10'.

Ms. Austin commented:

- Suggesting keeping the rear yard setback as 5' with the caveat that conditioned space cannot be extended or added on to.

The Commission was in consensus on the following:

- The rear yard setback development standard will stay at 5'.
- No conditioned space can be added.

Commissioner Fritz commented:

- The applicant shows a side yard corner setback of 5' and no other side yard setbacks.
 - The side yard setback is 0'.
 - Perhaps that should be clarified.

Vice Chair Doyle commented:

- Concurred with Commissioner Fritz.
- The following language under Small Lot Subdivisions should be added here too for consistency;
 - "C. Interior Side Yard Setbacks. The minimum side yard setback for a single parcel shall be four feet except for structures sharing common walls."
 - Happy to go with that.
 - If the applicant wants to use five feet that's okay too.

The Commission was in consensus on using the language as read by Vice Chair Doyle.

Director Svanstrom responded in the affirmative and commented:

- Staff can replace references to "side yard corner setback" with, "interior side yard setback".
- Staff can change the setback to five feet, instead of four feet.

The Commission concurred.

Vice Chair Doyle commented:

- With regards to height; suggested a maximum height of 30' above the lowest allowable floor level height which is 80.
 - Thus meaning, maximum floor level height would be at elevation 110.

Ms. Austin commented:

- Would rather a maximum height of 40' to give herself flexibility.
- Hopes the Commission will trust her enough to give her the flexibility she's requesting.
- Intends to stay as low as reasonable while being able to have a nice roof pitch.

Vice Chair Doyle commented:

- It should be specified as a 40' maximum height if that is what the applicant is asking for.

Ms. Austin responded in the affirmative.

The Commission was in consensus on specifying 40' as the maximum height in the development standards.

Director Svanstrom confirmed that the development standards already include a 40' maximum height. She noted that the words, "as listed in this table" could be deleted.

The Commission concurred.

Commissioner Douch asked to hear from the Commission on approval of the ESOS Visual Resource Analysis as well as discussion of allowed uses within the required setback.

Commissioner Glaser commented:

- The report is fine.
- There should be allowed uses in the setback, especially if the area is left open.
 - In this case the area essentially becomes a public commodity.
- The area is currently unkempt.
- Perhaps this will force people to look at it differently, to want to clean up the greater area.
- Would support trails and benches within the setback area.
- Would not support play structures and similar items within the setback area.

Commissioner Oetinger commented:

- There is very little connectivity and walkability with the other uses in the neighborhood, particularly with Tomodachi Park and the City-owned property which someday will be a park.

- Concerned as the City has been trying to increase walkability for health, safety, economic vitality, less need for transportation, and for reducing our carbon footprint.
- As nice as this project is, it's right between some uses with little to no connectivity between them.
- Wants to hear what the Commission thinks about what she's said.
- Has a couple of suggestions; one or both could be considered.
- Suggested creating a public access easement within the 50' setback (adjacent to this project's borderline fence).
 - People already walk back there and will continue to more and more.
 - Would be smart to create a public easement to someday have connection from a possible pedestrian extension of Abbott Avenue all the way down to the new Tomodachi Park and eventually Park Village which is expected to become a park at some point in the future.
- Suggested a couple of alternative paths continuing along from the bicycle access down to the Joe Rodota trail.

Commissioner Oetinger discussed her suggestions with Ms. Austin.

Commissioner Oetinger commented:

- We don't seem to have many opportunities to create connectivity within the community.
- Because the land to the east is City-owned and will someday be a park, it makes sense to create some sort of a public easement.
- There are existing footpaths that run through this property due to a gate that hasn't been locked.

Commissioner Doyle commented:

- Immediately to the south of the strip that Commissioner Oetinger is talking about is a City-owned property.
- Suggested putting the path on that City-owned property, rather than on private property.

Commissioner Oetinger commented:

- The area that she is talking about is on the other side of the waterway and is more pedestrian accessible from this property and the future park.
- Can envision a promenade from the park all the way into town on this side of the waterway.
 - Without an easement the potential to do that will not exist.

Commissioner Glaser commented:

- The easement would benefit the residents of Davis Townhomes if located where Commissioner Oetinger is suggesting.
- Should check to ensure that the applicant can add a public easement given the ESOS designation.
- This kind of accommodation would bring all of this housing into the community.

Commissioner Oetinger commented:

- Concurred with Commissioner Glaser.
- There are places in the ESOS section that mention that restriction and it should be eliminated.

Mr. Davis commented:

- A Commissioner who is not in attendance this evening expressed concern during a prior hearing about an area, which he pointed to on a map, that used to be full of homeless encampments.
 - The area has since been cleaned up quite a bit.
 - Homeless encampments are still found in the nearby Railroad Forest area.
 - Wants to improve the area for the residents without attracting homeless activity.
 - Wants to ensure the safety of the residents.

Commissioner Glaser commented:

- Doesn't want the applicant to be limited from being able to access the area at all should conditions change.
- The residents and community would be able to use the public easement area and it would be safe.
- Wouldn't want to, as a Commission, approve conditions that would mean that the applicant couldn't do something like this in the future.

Commissioner Oetinger commented:

- The area within the development should be walkable and accessible, just like it would be if it were fronting a public street in the downtown.

Commissioner Douch asked the applicant to respond to Commissioner comments on having the area within the development as walkable and accessible for both the residents as well as the public.

Mr. Davis asked a clarifying question.

Commissioner Douch commented:

- Two matters are being discussed;
 - A public easement within the setback and a sidewalk along the frontage.

Mr. Davis commented:

- Could be opening a can of worms as we don't know how some surrounding properties may be used.

Ms. Austin commented:

- The City owns the area that would be needed to extend the sidewalk [at the front of the development] in the desired location so it would be up to the City to make that happen.
- The sidewalk goes up to the property line on the project site.
- Doesn't think a public easement would be necessary.

Director Svanstrom commented:

- What Commissioner Oetinger is saying makes sense as there is a condition of approval requiring that there be a sidewalk from the trail connecting to the west.
 - That would then connect through to the eastern property line right where the flag portion of the property starts.
 - The City could then elect to connect through on the City side of it as desired.
 - That would then serve as the connection through this project site.
- Suggested a condition of approval that would require that the sidewalks within the development be part of the public access easement that is on the property that way it will remain open and cannot be gated off at some future time.

Ms. Austin commented:

- Understands Mr. Davis' concerns for safety, especially due to the problems with homeless encampments over the years.
- Allowing the public to walk on the sidewalk in front of the houses is fine, however, putting a public easement on it seems excessive.
 - The area is already accessible and open without the need for a public easement.
- Likes the idea of having some flexibility to do something within the 50' setback.

Commissioner Glaser commented:

- Doesn't like the idea of not allowing the applicant to improve an area, especially when the improvement would mean that people could visit and enjoy looking at the Laguna which is something the City wants to encourage.

Director Svanstrom commented:

- Suggested modifying the condition so that passive recreation elements can be allowed with some type of City review.
 - Passive recreation elements are usually trails and benches.
 - Play equipment would be considered an active recreation element.
- The intent of the ESOS isn't just the visual nature of the Laguna, it's the buffering to the biological area for the species as well.
- The City is currently looking for funding to help cleanup Railroad Forest as it is in need of it.

The Commission concurred with Director Svanstrom's suggested condition modification.

In response to a question, Director Svanstrom commented that the suggested modification would occur under Condition 5b.

Commissioner Douch asked to hear from the Commission on the matter of public access.

Chair Wilson commented:

- If the City chose to put in the sidewalk and create that connection, couldn't the City add a provision which would prevent the Davis property from having a fence there which would block that connectivity?

Commissioner Oetinger commented:

- That is the essence of having an easement; if there's an easement there's access, otherwise it can be treated as private property.
- Doesn't envision the easement being used until a date quite a way into the future.
- This would be more of a cautionary easement for the future.

Director Svanstrom commented:

- The City has supported Park Village as affordable housing.
- Doesn't foresee Park Village going away.
- The City is putting money into Park Village for use as affordable housing.
- The area on the northwest side has been designated as an expansion area for Tomodachi.
- There could still be pedestrian access added between the two properties, however.

Ms. Austin commented:

- Suggested negotiating an easement at a future date, should plans for connectivity come into play.

- The property owner doesn't want an easement attached to the property at this time.
- Could support a way of keeping it open as a possibility for when it makes sense as opposed to now.

Director Svanstrom commented:

- The applicant could make a non-revocable offer of easement that the City Council could choose to accept or not if it doesn't ever make sense.
- The only time the City can require a condition like this is at this approval stage in the process, not at some future date.

Ms. Austin commented:

- Reiterated her comment about Mr. Davis being very concerned about safety.
 - Mr. Davis experienced problems with people coming in and trashing this property when the gates were left open.
 - Understands that may change once the property is developed as it is currently a vacant lot.
- Understands the desire for connectivity.
- Reiterated that Mr. Davis would prefer not to have an easement at this time due to security concerns.
- Understands staff's perspective.

Director Svanstrom commented:

- Offers of easement and those types of things are generally implemented at the time of Final Map.
- If it's a non-revocable offer of easement, the City isn't required to accept it at the time that it is offered.
 - Being non-revocable means that the applicant cannot take back the offer of easement, and that the City could defer acceptance of it until a date in which it makes sense to do so (i.e. conditions change, security concerns are alleviated, etc.)

Commissioner Glaser asked Director Svanstrom if the applicant could condition the non-revocable offer of easement to require that the City have a plan for constructing a sidewalk connection through the site.

Director Svanstrom responded that this was possible.

Chair Wilson commented:

- It could also have a time element to it which would say that the City can't accept the offer within a certain number of years thus giving the applicant time to let things develop.
- Other conditions could also be attached such as; the City shall not accept this non-revocable offer of easement without plans to do certain improvement(s).

Director Svanstrom commented that she would work on crafting a condition, consistent with their discussion, for future consideration by the City Council.

Commissioner Fritz commented:

- Hears Ms. Austin's concerns regarding adding easements.
- Hesitant to require those types of things.
- The owner of the nearby hotel limits public access to a nearby path of travel and it's frustrating.

- The owner wouldn't be able to do this if there was a public access easement.
- Whether or not there is an easement, people will walk through this project.

The Commission asked additional questions of Director Svanstrom.

Vice Chair Doyle commented:

- Like Commissioner Fritz, he would be hesitant on requiring a public easement given that there is no current plan to develop that connection.
- This is coming up at the last minute.
- This is private property.
- Doesn't like the idea of requiring the applicant to provide a public sidewalk in this case.

Chair Wilson commented:

- There may be a way for the applicant to come forward with a non-revocable offer of easement that could be conditioned in a way that the applicant would feel comfortable should the City accept their offer.
- Feels reluctant to fundamentally change the project at the last minute by requiring a public access easement.
- Perhaps there is a compromise.

Commissioner Douch commented:

- Feels similarly to Chair Wilson.
- Does not feel particularly comfortable recommending an easement for Council consideration.
- If a conversation surrounding an easement evolves in a way that is acceptable to all parties, he'd be okay with it.
- This project has been before the Commission before and this has never come up.
- Sympathetic to the thought surrounding a public easement and considers it an important discussion.
- Requiring a public easement feels like an onerous ask on this project.
- This is a project that the City wants to see happen.
- Hesitant to add another layer of entitlement and encumbrance.

Vice Chair Doyle suggested that the Commission take a straw poll on the matter of recommending a public access easement.

Director Svanstrom responded that that could be helpful.

Commissioner Douch asked for a straw poll on approval of a condition for some form of public access.

AYES: Commissioners Oetinger and Kelley

NOES: Chair Wilson, Vice Chair Doyle, and Commissioners Douch, Glaser and Fritz

Commissioner Oetinger commented:

- It is unfortunate that this topic didn't come up sooner in the process as the City's General Plan features and prioritizes walkability.

Chair Wilson commented:

- This could still come up at Council.
- The applicant could share their concerns with the Council and perhaps the easement could be qualified in a way that satisfies all involved.

- The applicant may find this discussion helpful in the sense that they could think about and possibly prepare and offer up a satisfactory solution if this is raised at Council.

Commissioner Douch asked for any additional comments on the ESOS Visual Resource Analysis.

Hearing none, Commissioner Douch asked to hear from the Commission on the Mitigated Negative Declaration.

Hearing none, Commissioner Douch asked to hear from the Commission on the Tentative Map.

Hearing none, Commissioner Douch asked to hear from the Commission on design.

Commissioner Douch commented:

- Because this project will be subject to additional review by the Design Review Board, he did not feel the need to comment on design.

Commissioner Glaser commented:

- Encouraged the applicant to plumb for a lot of electric vehicle charging.

Director Svanstrom commented:

- The applicant is required to do the rough-in for 20% of the vehicle spaces as required by Code.

Commissioner Glaser commented:

- Encouraged doing the rough-in for more than 20% of the vehicle spaces.

Director Svanstrom commented:

- With regards to design, she suggested modifying COA #1 under the Recommended Conditions of Approval as follows;
 1. Plans and elevations shall be in substantial conformance with plans prepared by Katherine Austin, and date-stamped received on December 19, 2018, and on file at the City of Sebastopol Planning Department, except as modified herein *or by the Design Review Board*. Any modifications to the plans shall be reviewed and approved by Staff prior to modification.

The Commission expressed support for the revision as stated by Director Svanstrom.

Members of the Commission asked clarifying questions of Director Svanstrom.

In response to a question the Commission agreed to modify COA #27 b. [now #28 b] under the Recommended Conditions of Approval as follows;

27. b. "A 20 foot setback for fences greater than 3 feet in height is required on all street frontages and corner lots *if required by the City Engineer.*"

Director Svanstrom commented that she would check with the City Engineer on whether that condition is needed.

In response to a question about COA #40 [now 41] and to what portion of Morris Street this applied. Under the Recommended Conditions of Approval; Director Svanstrom

commented that she would check with the City Engineer on it being specific to failed portions of Morris Street near or south of Sebastopol Road.

Vice Chair Doyle asked Ms. Austin if she has any concerns on any of the recommended conditions that had not been discussed.

Ms. Austin responded that she did not have concerns regarding any of the recommended conditions that had not been discussed.

Commissioner Fritz commented:

- Recommended COA #53 is worded strangely.

The Commission discussed modifying Recommended Conditions of Approval #53 [now #54] by striking the second sentence.

Commissioner Glaser referred to Recommended Conditions of Approval #33 and asked a clarifying question.

Hearing nothing further, Commissioner Douch asked for a motion regarding the resolution.

Commissioner Fritz made a motion to adopt the resolution as amended per the Commission's discussion.

Commissioner Glaser seconded the motion.

Director Svanstrom commented that the modifications that she understood were as follows:

- The typographical corrections noted for the Mitigated Negative Declaration.
- Condition 1 shall include, "or as modified by the Design Review Board."
- Delete the restrictive covenant language in Condition 10 for inclusionary housing and simply refer to the regulatory agreement.
- Development standards to include a condition that conditioned space additions are not allowed.
- The rear yard setback shall be changed to 5'.
- Lot 18 will be clarified to be treated as a side yard.
- What is now called the side yard corner setback will be changed to be consistent with the small lot subdivision language.
- Condition 5 b will be modified to read that passive recreation may be allowed with approval of a Planning Director approved Use Permit.
- The typographical corrections made to the Recommended Conditions of Approval.
- The second sentence shall be deleted from Condition #53 [now COA #54].

The Commission concurred with the amendments as stated by Director Svanstrom.

Commissioner Douch asked for any discussion of the motion.

Hearing none, Commissioner Douch asked for a vote on the motion.

AYES: Chair Wilson and Commissioners Douch, Doyle, Kelley, Glaser, Fritz and Oettinger

NOES: None

ABSTAIN: None

ABSENT: Commissioner Fernandez

Commissioner Douch thanked Mr. Davis and Ms. Austin.

Mr. Davis and Ms. Austin thanked the Commission.

Director Svanstrom announced that this item is tentatively scheduled to go before the Council on May 7th.

10. DISCUSSION: (none)

11. ADJOURNMENT: Commissioner Douch adjourned the meeting at 10:11 p.m. The next regularly scheduled Planning Commission meeting will take place on Tuesday, April 23, 2019, at 7:00 p.m. at the Sebastopol Youth Annex, 425 Morris Street, Sebastopol, CA 95472

Respectfully Submitted By:

Kari Svanstrom
Planning Director

Agenda Report Reviewed by:
City Manager: _____

**CITY OF SEBASTOPOL
CITY COUNCIL
AGENDA ITEM**

Meeting Date: Meeting of May 7, 2019
To: Honorable Mayor and City Councilmembers
From: Dana Morrison, Assistant Planner
Kari Svanstrom, Planning Director
Subject: Use Permit for solely-residential in a commercial zone; Planned
Community Zoning designation; ESOS Visual Resource Analysis; Tentative
Map for a subdivision of more than 4 parcels; Mitigated Negative
Declaration (CEQA) Determination
Recommendation: Approve the Use Permit, ESOS Visual Resource Analysis and Tentative
Map Adopt the Mitigated Negative Declaration, and Waive First
Reading and Introduce an Ordinance Rezoning the Parcel
Applicant/Owner: Kathy Austin / Dan Davis
File Number: 2018-82
Address: 6737 Sebastopol Avenue
CEQA Status: Mitigated Negative Declaration
General Plan: CC – Central Core
Zoning: CD – Downtown Core

Introduction:

In August of 2018 the City received an application for the Davis Townhomes project, proposing development of eighteen (18); 1,180 square foot, 2-bedroom, 2.5 bathroom, 2-story townhomes. The proposed development would occur on a vacant lot located at the end of Morris Street, south of Sebastopol Avenue/Hwy 12.

This application includes multiple entitlements which require hearings by different bodies. The entitlements include: 1) A request to modify the Environmental and Scenic Open Space (ESOS) study requirements (approved by PC on 10/23/2018); 2) a Use Permit for a solely residential project in a commercial zone; 3) a Planned Community Zoning designation; 4) ESOS Visual Analysis Study; 5) Tentative Map for a subdivision of more than 4 parcels; 6) Environmental review (California Environmental Quality Act, or CEQA, review); and, 7) Design Review for a subdivision of three or more units (to be completed by the Design Review Board).

The Planning Commission held a public hearing for this application at their April 9, 2019 meeting and adopted a Resolution recommending approval to the City Council for items 2-6 above. More details pertaining to this meeting will be discussed later in this Staff Report.

Summary of Entitlements:

Use Permit to Allow Residential Only

The applicant is proposing to develop a fully-residential project in the Downtown Core Zoning District. Per the Zoning Ordinance permanent residential-only projects are a permitted use, when not part of a mixed-use development, with the approval of a Conditional Use Permit.

Planned Community Zoning Designation

The applicant is proposing a Planned Community (PC) District designation for the site, which is a zoning designation that allows for specific development standards to be assigned for the development of a site that varies from the base zoning district. In this case, the property is currently zoned CD (Downtown Core), and the applicant is requesting it to be rezoned to PC. The PC zoning designation had a preliminary review by the Design Review Board.

ESOS Study Requirement – Visual Resource Analysis

Due to the zoning of this site, which is in close proximity to the Laguna Wetlands, a special study under the City’s Environmental and Scenic Open Space (ESOS) zoning regulations is required. At their October 23, 2018 meeting the Planning Commission approved the reduction of the required ESOS to require only visual analysis per SMC 17.46.090.

Tentative Subdivision Map

The applicant proposes to develop an 18 Lot subdivision with a common area (Lot 19), with lots ranging from 1260 to 1801 square feet.

CEQA Determination

The project required an Initial Study, which found that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions and mitigations in the project have been made by or agreed to by the project proponent. A Mitigated Negative Declaration was prepared.

Project History/Prior Review:

The Planning Commission conducted a Preliminary Review of the proposed project at their March 13, 2018 meeting prior to the formal submittal of the project. The Commission at that time was supportive of the project, including the development as a residential-only project, and reductions to the ESOS requirements given the prior development of the site. On October 23, 2018 the Planning Commission approved the reduction of the required ESOS to require only visual analysis per SMC 17.46.090. The Commission also approved the reduction of the required 100’ setback buffer to the sensitive habitat (Railroad Forest) to 50’ per SMC 17.46.050.B(1).

On November 14, 2018, the Design Review Board conducted an initial review of the Planned Community project and the Board was unanimously in support of the project as solely residential, and with the lower FAR and reduced lot sizes. The Board discussed the proposed height of the project and while some members thought that the height of the development could go as high as the base zoning district allows (40 feet), the applicant adjusted the submitted Policy Statement Table to show 40 feet as the proposed height; however, the proposed elevations in the plans still show the height to be ~30 feet from 6" below finish floor to ridge; only 2-stories are proposed. It should be noted that the final height may vary due to flood plain requirements.

The Board also provided general comments regarding the overall design and landscaping of the project but limited this discussion as the item will be returning for official Design Review at a later date. Minutes from the November 14, 2018 DRB meeting are located in the Attachments section of this Staff Report.

On April 9, 2019 the Planning Commission held a public hearing to review the proposed project's Use Permit (for solely residential and to rezone the property as a Planned Community), Tentative Map, ESOS Visual Analysis, and Initial Study/Mitigated Negative Declaration. The Planning Commission felt that the Use Permit for solely residential and the Planned Community Rezone were appropriate given the nature of the development and its location, and recommended approval. The Commission reviewed and recommended approval of the Visual Resource Analysis to the City Council. The Commission also reviewed the Tentative Map. They had no comments or issues and recommended approval to the City Council. Finally, the Planning Commission reviewed Initial Study/MND and recommended that City Council adopt the MND.

Other items of note, which are discussed in the Analysis section, include:

The Planning Commission discussed if the bike/pedestrian east-west sidewalk (the Joe Rodota Trail east across the site to the shared fence with Park Village) should connect all the way through to Tomodachi Park.

The Planning Commission felt the applicant should be encouraged to use the 50' setback from the Railroad Forest property for passive recreation (benches, minor trails, etc.). They modified Condition of Approval (COA #6b) to allow passive recreation uses to reflect this. They felt a Use Permit reviewed by the Planning Director was the appropriate level of review.

Site and Surrounding Uses:

The site is located at the end of Morris Street. The site itself is relatively sheltered from view; this is due to the physical separation from Sebastopol Avenue, in combination with commercial, retail and residential development along Sebastopol Avenue and the mobile home park to the east. The proposed development would be 2-stories, comparable to the single-family house along the northeastern boundary and the Sebastopol Inn located to the west. As such, the size would not be out of character with the surrounding uses. Given the site location, the proposed

development, would only be visible from the public right-of-way at the intersection of Morris Street and Sebastopol Avenue.

Project Description:

The proposed town homes are clustered around a central open space, in three clusters. Lot sizes vary from ~1250 to ~1800 square feet. The town homes are proposed to have 6-foot-deep front and rear porches (which will have a storage closet). In addition, there will be private rear yards of a minimum 20 feet deep by 20 feet wide with rear gates.

The project will include 18 carport spaces along with 18 assigned surface parking spaces and two (2) visitor parking spaces, for a total of 38 parking space. Four (4) of the spaces are proposed to be electric car charging stations.

The proposed development also includes gated driveway access to Park Village (to be used in case of emergency), fire and emergency vehicle access, storm water retention areas, and new landscaping including the addition of 56 new trees, trash and recycling enclosure, and one ganged mail box.

Further information regarding lighting, materials and colors are detailed in the project description submitted by the applicant, which is attached to the end of this staff report. The style of the homes are proposed to be farmhouse or late craftsman to compliment much of the architecture found throughout Sebastopol. Roof pitches and front and rear porch roofs will be varied; and all porches will have decorative railings which will be painted to coordinate with the individual color trim of the home. The design of the units and landscaping will be reviewed in further detail by the Design Review Board.

The applicant indicated that the exact structure heights from grade cannot be confirmed until a grading plan is completed. Depending on the finish grade the homes are approximately 30½' high at the ridge from 6" below the finish floor. Due to the fact that the project is located in a flood plain all homes are required to have a finish floor at 80' elevation, which is 2' above the 78' 100-year flood level.

General Plan Consistency:

The General Plan Land Use designation for the site is CC: Central Core and the ESOS overlay.

Due to the ESOS overlay an ESOS study is required for any development occurring on the project site. At their October 23, 2018 Planning Commission hearing the Commission approved a reduction to the required ESOS study to only require a staff level Visual Resources Analysis. The Visual Resource Analysis was reviewed by the Planning Commission at their April 9, 2019 meeting, and recommended approval to the City Council. With City Council approval of the Analysis the project will be compliant with the ESOS overlay.

The General Plan describes Central Core as the following: *“This designation applies to portions of Sebastopol’s Downtown and nearby areas. The Central Core designation allows office, commercial, and retail uses, as well as mixed-use residential developments. Residential uses are allowed at a density of 15.1 to 44 units per acre.”* The Zoning Ordinance allows for solely residential in the CD district, with approval of a Conditional Use Permit.

The following provisions of the General Plan relate to this project:

Policy LU 1-7: Encourage new development to be contiguous to existing development, wherever possible.

The project is consistent with this provision as the site is located at the edge of the Downtown Core District and is surrounded on three sides by existing residential and commercial developments.

Policy LU 6-2: Promote compact urban form that provides residential opportunities in close proximity to jobs, services and transit.

The project is consistent with this provision as it provides much needed housing located in close proximity to various community services and transit.

Policy COS 12-9: Encourage the protection and incorporation of existing, native, mature, non-orchard trees and areas of natural vegetation as part of new development.

The application is consistent with this provision as only one tree is slated for removal. Additionally, Conditions of Approval have been added in regards to the 50’ Railroad Forest setback which require this area to be maintained in its natural state, and which requiring buffer plantings / mitigation planting in said 50’ setback. To meet this the applicant shall work with a biologist to develop a planting plan to identify and propose plant species to provide a buffer to the identified natural resource.

Policy COS 12-12: Encourage clustered development that preserves a sense of openness, particularly in areas adjacent to open spaces and scenic resources.

The application is consistent with this provision as the development is clustered in the center of the project site, with the townhomes arranged in the three clusters with open space located in the center, and around the perimeter of the property. The southern edge of the site will have the largest open space, with a 50’ setback from the Railroad Forest property. A Condition of Approval has been added to require an open space easement on this section of the property which would prevent disturbance of this area with the exception of restoration and removal of invasive species.

Policy CD 1-7: *Promote a compact urban form and infill development with increased densities to be located in areas that are readily accessible by pedestrians and bicyclist, served by transit, and allow for convenient access to daily activities.*

The project is consistent with this provision as it is an infill development which provides much needed housing located in close proximity to pedestrian and bicycle trails and has convenient access to local amenities and transit. As conditioned, the project would be modified to include a bicycle/pedestrian connection to the existing trail located at the west side of the site.

Policy CD 3-4: *Require new development to avoid the disruption of cultural, archeological, and historical resources.*

The project was subject to a Cultural Resource Evaluation (located in Attachments) which found that the site did not contain any known or evident cultural, archeological or historic resources. A Condition of Approval was added to require that, in the event that any unanticipated artifacts or cultural features are discovered during grading or underground excavations all work in the vicinity of the find shall be stopped until the discovery area can be evaluated by an archaeologist and appropriate actions are taken. As such, the application is consistent with this provision.

Additionally, the proposal is consistent with the City's Housing Element, in that the site is identified as a site in the Housing Site Inventory as land suitable for residential development. The proposal is consistent with the City's Housing Element, in that, as conditioned, the proposal would meet the City's Inclusionary Housing requirements by providing on-site Inclusionary Unit(s) and providing housing opportunities at a variety of income levels as discussed further in this staff report.

Overall, staff believes that with City Council approval of a Use Permit for solely residential in a commercial district and a Planned Community zoning designation, along with approval of the associated Tentative Subdivision Map and Initial Study/Mitigated Negative Declaration, this project will be consistent with various provisions of the General Plan.

Zoning Ordinance Consistency:

The proposed location for the Davis Townhomes is on one parcel located at the edge of the Downtown Core District (CD). *The CD District is intended to create, preserve, and enhance the downtown area as the historic retail core of Sebastopol. Solely residential development is a permitted use (with a Use Permit) in the CD district. As mentioned earlier, the applicant is proposing a Planned Community Overlay for this property as it does not meet the minimum FAR and is proposing a smaller lot size than is required by the CD district or for a Small Lot Subdivision (reduced lot size with attached houses in Small Lot Subdivision can be approved by the Planning Commission).*

Allowed Uses Table

Allowed Uses	Downtown Core Requirements	Planned Community Proposed
Permanent residential uses that are allowed in the R7 District when not part of a mixed-use development	Conditional Use Permit	Conditional Use Permit
Nonresidential uses except for office uses	35,000 sf	Home office uses, subject to SMC 17.210
Common Area Accessory buildings		Conditional Use Permit
Residential Accessory buildings	-	Permitted, subject to 17.08.030, but limited to 80 sq. ft. in size

Development Standards Table

Development Standards	Downtown Core Requirements	Planned Community Proposed
Minimum lot area	6,000	1,260 – 1,801 sq. ft.
Maximum building height: Buildings and other facilities	40 ft., 3 stories/ 50 ft. 4 stories (1)	2 stories, and 40 ft. However per plans ~30 ft. (total may vary due to flood plain restrictions).
Front Yard	0 ft.	5 ft.
Interior Side Yard (except for structures sharing a common wall)	0 ft.	5 ft. ¹
Rear Yard, Main Building	0 ft.	5 ft.
Rear Yard, Main Building when abutting residential district	20 ft.	5 ft.
Maximum residential density	1 DU/1,743 SF lot area (CD) 1 DU/ 1,000 SF lot area (RM-H)	18 residential units on 75,794.4 SF lot = 1 DU/4,210.8 SF lot
Maximum Floor area ratio, not including residential except for purposes of calculating minimum FAR	Minimum (new buildings); 1.0 Maximum; 2.5	Net FAR 0.39 FAR ²
Minimum residential density	1 DU/ 3,600 SF lot area	Net Density: 18 residential units on 54,103 SF lot = 1 DU/3,005.72 SF lot
Minimum usable open space	50 sq. ft. per DU	280 sq. ft. per DU in rear yard + front and rear porches and common area
Parking Residential	2 parking spaces per 2 or 3 bedroom units. 18 units x 2 = 36	2 spaces per unit (36 spaces), one of which is in a carport, and 2 visitor spaces. 38 spaces total

Bicycle Parking Spaces	20% required vehicle parking requirement. 36 x 0.20 = 8 bicycle parking spaces.	Hook in rear closet space for a bike for each unit, 18 bike spaces.
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¹ Lot 18, which has an angled rear property line, angled lot line shall be considered a side yard

² No additions to habitable space shall be permitted

Generally, the proposed project is consistent with the base zoning requirements of the Downtown Core District. The Planned Community overlay is being requested as the project does not meet the minimum required FAR and minimum lot sizes. However, the Planned Community overlay allows for consideration of the unique site characteristics (discussed in the Analysis section of this Staff Report); and, if approved, the application would be consistent with the Zoning Ordinance.

Solely residential development is also an allowed use in the CD Zoning District with approval of a Use Permit; if approved the project would be consistent with the Zoning Ordinance.

As noted above, an ESOS visual analysis is required due to the overlay. This is included in the Analysis section of this Report. This was reviewed by the Planning Commission at their April 9, 2019 meeting, and they recommended approval of the Analysis to the City Council.

This application will require Design Review by the DRB which will occur at a later date (tentatively scheduled for the May 15, 2019 meeting).

Inclusionary Housing:

The project is subject to the city’s Inclusionary Housing requirements, which requires a certain percentage of units be reserved as Affordable housing in perpetuity for projects of more than five dwelling units. The level of affordability is indexed to the Area Median Income for Sonoma County (AMI) for the year the units are sold/rented.

During the recent Zoning Ordinance update, the Inclusionary Housing requirements were modified to provide additional flexibility to an applicant by requiring a different percentage/number of units be provided based on level of affordability.

For this project, the number of units to be provided are as follows (note, the fraction of a unit requirement can be fulfilled through either paying the corresponding Inclusionary Housing In-lieu fee, or by rounding up the number of units provided):

120% of AMI (Moderate Income) requires 15% of units, or 2.7 units (2 units plus .7 of a unit, or 3 units provided)

80% of AMI (Low Income) requires 10% of units, or 1.8 units

50% of AMI (Very Low Income) requires 5% of units (Very Low Income), or .9 units

These units will be reserved as affordable through a deed-restriction and affordable housing agreement between the City, Applicant and the steward of the units.

Sebastopol, like most other smaller communities, does not have a housing authority or dedicated staff to administer its affordable housing units. Instead, the City partners with a qualified housing land trust or qualified affordable housing provider to monitor (“steward”) these units, including any marketing, pre-qualification of buyers, etc. The City has identified the Housing Land Trust of Sonoma County (HLT) as the likely steward of these units and is working with the applicant and the HLT to review these requirements.

The applicant is likely to provide units at the 80% of AMI (Low Income) level, and is considering providing two units rather than paying the inclusionary fee. However, the applicant is requesting flexibility to determine the final number of units/level of affordability at a later time. This designation would need to be completed, and all agreements executed, prior to approval of the Final Map. Staff has added a Condition of Approval for the project that the applicant provide a proposal to the City regarding how they will meet these requirements, including identification of which lots/units would be the inclusionary units which will need to be approved by City Council prior to recording of the Final Map. Staff has also included other conditions to ensure that the design and construction meets the requirements of the Section 17.250 (Exhibit B, COA #10 a-g).

Staff will continue to work with the HLT and applicant to ensure the proposal meets all of the requirements of the Inclusionary Housing Ordinance (including distribution of the units throughout the site, required agreements, language for the deed restrictions, etc.) prior to the submittal of the Final Map.

Public Comment:

The Planning Department did the following to comply with Section 17.460 of the Zoning Ordinance: (1) Provided written notice to all property owners within 600 feet of the external boundaries of the subject property; (2) provided a written notice that was published in the Sonoma West Times; and (3) posted three written notices publicly on and within vicinity of the subject property. Additionally, pursuant to CEQA Guidelines §15070, staff prepared a Draft Mitigated Negative Declaration (MND) which was filed with the State Clearinghouse and the Sonoma County Clerk/Recorder (Attachment 3). The Draft Initial Study/Mitigated Negative Declaration (CEQA) was circulated for public review March 7, 2019 through April 8, 2019.

The Planning Department has not received any comments from the public as of writing this report. The State Clearinghouse confirmed that no state agencies (including CalTrans) had any comments on the CEQA Initial Study/Mitigated Negative Declaration.

City Departmental Comment:

The application was routed to the Building, Engineering, Fire, Police and Public Works Department, along with the City Manager's office for comments and conditions. Comments and Conditions from various departments have been integrated into the Conditions of Approval.

Required Findings:

Use Permit: Conditional Use Permits are discretionary and shall be granted only when the review authority determines that the proposed use or activity complies with all of the following findings:

- a. The proposed use is consistent with the General Plan and all applicable provisions of this title.
- b. The establishment, maintenance, and operation of the use applied for will not, under the circumstances of the particular case (location, size, design, and operating characteristics), be detrimental to the health, safety, peace, morals, comfort, or general welfare of persons residing or working in the area of such use or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the City.

Tentative Map for Subdivision:

1. In recommending approval or conditional approval or in approving or conditionally approving a tentative map, the Planning Commission or City Council as applicable shall find:
 1. That the proposed subdivision, together with the provisions for its design and improvement, is consistent with the General Plan, and other applicable provisions of this code including SMC Chapter 16 and the State Subdivision Map Act (SMA); and
 2. That the design of the proposed subdivision provides, to the extent feasible, for future passive or natural heating or cooling opportunities in the subdivision, as described in the State Subdivision Map Act and any guidelines promulgated by the City Council.

ESOS Visual Resource Analysis:

- a. the following aspects of the proposed Project were considered by City Staff in regards to the visual resource analysis, will the Project:
 1. Have a substantial adverse effect on a scenic vista;
 2. Substantially damage scenic resources, including, but not limited to: trees, rock outcroppings, and historic landscape;
 3. Substantially degrade the existing visual character or quality of the site and its surroundings; or,
 4. Create a new source of substantial light or glare which would adversely affect day or nighttime views of the area.

Key Issues:

The Planning Commission reviewed the following key issues, and their deliberations and recommendations are included below in the Analysis section of this report.

Key issues for the City Council to consider include:

1. Use Permit for solely residential. Is the proposed use of solely residential appropriate?
2. Planned Community Rezone. Are the proposed changes from the base district (reduced FAR and lot sizes) appropriate based on the overall project design and site location? Are the Policy Statement and Development plan for the proposed project appropriate?
3. Tentative Map. Is the proposed subdivision layout appropriate?
4. CEQA compliance. Does the Council find the Mitigated Negative Declaration adequate?
5. Design. Is the design suitable for the project site?
6. Conditions. Are there any modifications to the recommended conditions of approval?
7. Overall project. Considering all aspects of the project, does it merit approval?

Analysis:

Solely Residential Development in a Commercial Zone:

The Zoning Ordinance allows for solely residential development on a commercially zoned parcel, provided it does not detract from the intent of the commercial spaces on the street. Overall, the proposed project appears appropriate for the site. The proposal is for an attached single-family housing development consisting of multiple lots and one large lot of common space that contains a common outdoor area, as well as covered parking and surface parking.

The 1.74-acre site is located at the end of Morris Street behind an existing commercial center. The commercial frontage is poor due to the physical separation from Sebastopol Avenue, in combination with commercial, retail and residential development along Sebastopol Avenue and the mobile home park to the east. Given the properties location, situated behind existing commercial and the residential development to the northeast and east, the proposal appears appropriate for the site. The site is not well suited for commercial development but does appear to be an appropriate location for much needed housing.

Overall, the project site is unique as it is located on the edge of an urbanized area and is considered part of the City's Downtown but abuts an environmentally sensitive habitat. The project will result in the removal of 1 existing tree and result in the planting of an additional 50+ trees, as currently proposed. The proposed site is located within walking distance of existing commercial, and recreational uses, and will provide additional housing opportunities for locals.

The Planning Commission reviewed and concurred with Staff's assessment that residential only development is an appropriate use for the site for the above reasons.

Planned Community:

The key reason why a Planned Community zoning designation was proposed for this property is due to the fact that the proposal would not meet the minimum Floor Area Ratio (FAR) or subdivision size as required by this zoning district or for a Small Lot Subdivision. Applying for a Planned Community Zoning will allow for a reduction in these and make the project viable. The City Council will need to review the appropriateness of the deviations from the standard CD development standards, such as:

- Appropriateness of smaller lot sizes
 - Under the new SMC 17.230: Small Lot Subdivisions the minimum lot size is 1,500 square feet. However, a reduced lot size may be allowed for attached single family development.
 - The development proposes lots sizes as small as 1,260 square feet.

Both the Planning Commission and the Design Review Board discussed this aspect during their reviews of the Planned Community and considered the proposed lot sizes reasonable given the site and site design. Staff concurs with this given the ample common area spaces proposed.

The required minimum FAR for the CD District is 1.0; the FAR of project is 0.39. Staff has identified several factors that support the development at a smaller FAR, including:

- The site is unique in that it abuts a sensitive habitat but is considered part of the City's Downtown Core.
- This site would not be appropriate for the 4 stories which the district allows given the ESOS and development guidelines for Laguna area development.
- Smaller size of the units is better suited for the unique character of site.
- Parking requirements are being met as proposed.
- Floodplain makes garden apartments unfeasible.

The Commission and Board reviewed these aspects as well and felt that the reduced FAR for the property was appropriate. Staff concurs with this as the smaller unit size provides for sensitivity to the environmental context of the site which is near the Laguna de Santa Rosa and provides for "affordability by design" in that the proposed housing units will be more affordable than larger units would be.

In reviewing these standards the Planning Commission discussed how to handle any additions, given the small lot sizes. The applicant is proposing the project to be built as proposed, with no additions to habitable space permitted (i.e. additions to the homes). However, they did want to allow for potential additions to expand decks, porches, or add a small accessory structure (garden sheds, etc.). The Planning Commission has added a Condition of Approval that no habitable space additions will be allowed.

Visual Impact Analysis Report:

The purpose of this Visual Resources Analysis (VRA) is to assess the potential visual impacts of the Davis Townhomes project (heretofore referred to as the 'Project'), under the required ESOS

study requirements, and to propose measures to avoid, minimize, or mitigate potential adverse visual impacts associated with construction of the proposed Project on the surrounding visual environment.

Key issues evaluated by the Planning Commission included the Project's potential to adversely impact the existing visual character or quality of the affected properties and/or the physical or natural surroundings. Potential visual effects are considered from public roadways and other public vantage points in and around the neighboring community. Project design attributes; the potential to remove, change, or add features that contribute to the existing quality of the visual landscape; and, potential conflicts with applicable plans or policies relating to visual resources are considered.

The proposed development area would be potentially visible from several principal viewpoints within the City of Sebastopol, as follows:

- Tomodachi Park and Park Village (located to the east of the proposed development).
- Vehicle/Pedestrian Bridge crossing the Laguna De Santa Rosa channel at the eastern entrance to the City.
- Views from the Morris Street/Sebastopol Avenue intersection.
- Multi-use connector trail from Sebastopol Avenue to the Joe Rodota Trail.

Due to area topography, views to the site are generally restricted. Limited views may occur from surrounding residential and/or Tomodachi Park, but would generally be screened visually due to intervening vegetation and/or existing development. The key views identified above are considered in this report, and the Project's potential to alter or affect existing views from these public vantage points within the viewshed are limited.

Visual Resource Analysis (VRA) Project Location/ Surrounding Uses:

The site is located at the end of Morris Street at 6731 Sebastopol Avenue. The subject property is behind and to the south of commercial properties that face onto Sebastopol Avenue and an existing parking lot that contains approximately 32 parking spaces. A bike path and the Sebastopol Inn are located to the west. The Railroad Forest is located to the south. A residential property, Park Village, and Tomodachi Park are located to the north and east.

The site itself is relatively sheltered from view, due to the physical separation from Sebastopol Avenue, in combination with commercial, retail and residential development along Sebastopol Avenue and the mobile home park to the east. The proposed development would be 2-stories, comparable to the single-family house along the northeastern boundary and the Sebastopol Inn located to the west. As such, the size would not be out of character with the surrounding uses. Given the site location, the proposed development, would only be visible from the public right-of-way at the intersection of Morris Street and Sebastopol Avenue, and from the Joe Rodota trail connector to Sebastopol Avenue.

VRA Project Description:

The complex will have 18, 2-story townhouse units, arranged in three clusters. In addition, the project will include 18 carport spaces along with 18 assigned surface parking spaces and two visitor parking spaces; 4 spaces are proposed to be electric car charging stations.

The proposed development also includes gated driveway access to Park Village (to be used in case of emergency), fire and emergency vehicle access, storm water retention areas, and new landscaping including the addition of 56 new trees. Further information regarding exterior lighting, materials and colors are detailed in the project description submitted by the applicant which is attached to this staff report.

Lighting:

Any lighting proposed shall comply with the General Plan Policy COS 11-7 and COS 11-8 which consider minimizing light pollution and up cast lighting. This includes the use of downcast (“dark sky compliant”) lighting and conscientious placement of any proposed lighting. The detailed lighting plan will be reviewed by the Design Review Board for compliance with this requirement. This had been added as a Condition of Approval.

Signage:

Any signage will be minimal and will meet the City’s Sign Ordinance guidelines as set forth in SMC 17.120. Signage lighting shall be restricted to external illumination (if illumination is proposed). This has been added as a Condition of Approval.

Fencing and Landscaping:

Fences and walls are used to provide security, visual privacy, and/or define a space. Walls and fences shall be designed to be compatible with the surrounding landscape and architectural concept.

Construction Access (visual impact during construction):

All materials for the Project’s construction would be delivered to the site by truck. The majority of truck traffic would occur on designated truck routes and/or major streets (e.g., Highway 12/Sebastopol Avenue). Traffic resulting from construction activities would be temporary and may occur along area roadways as workers and materials are transported to and from the Project area.

Long Term Access/Circulation:

Permanent access to the site would occur from Morris Street and Sebastopol Avenue. Emergency access will be available through a gate located at the eastern portion of the property where it abuts the existing mobile home park. This would not be an access road open for public or everyday resident use, but would be available to both sites for emergency use.

Visual Analysis:

The following aspects of the proposed Project were considered in regards to the visual resource analysis, will the Project:

- a) Have a substantial adverse effect on a scenic vista;
- b) Substantially damage scenic resources, including, but not limited to: trees, rock outcroppings, and historic landscape;
- c) Substantially degrade the existing visual character or quality of the site and its surroundings; or,
- d) Create a new source of substantial light or glare which would adversely affect day or nighttime views of the area.

As described above in the Project Description, the Project site is undeveloped and relatively flat. The Project site is surrounded by existing residential and commercial development to the north, east and west. Visual resources are primarily limited to those located adjacent to the Project site due to the existing developments in the surrounding areas. To the south of the Project site is the Railroad Forest property, which is owned by the City and designated as Wetlands/Scenic Open Space. An existing bike connector path is located just to the west of the Project site and views currently exist (from this path) of the Railroad Forest property. The Project includes a 50' setback from the Railroad Forest and required restoration of vegetation within this setback. While the proposed development may partially block the view from the bike path, the mitigation requiring restoration and further plantings in the 50' abutting the Railroad Forest property will likely increase the scenic value along this section of the path.

The Planning Commission found that the proposed Project will not have a substantial adverse effect on a scenic vista as the Project site is not viewable from any panoramic vistas located nearby, and given the proposed 2-story height of the project. The site does not contain any rock outcroppings or notable natural features; only one tree on the Project site is proposed for removal. The increase in development in the area the Project site would be difficult to discern within the greater fabric of the surrounding development.

The two-story Project would not interfere with the skyline and horizon line that are available from the intersection of Highway 12 and Morris Street and is consistent with the 2-story buildings to the east of the site. Furthermore, the Project will be subject to a number of Conditions of Approval regarding height, color selection and screening which will reduce potential impacts on any scenic vistas. Any project lighting is subject to review and approval by the Design Review Board and would be in compliance with the City's General Plan policies related to light pollution.

The Project would not result in the introduction of features that would significantly detract from or contrast with the visual character of the surrounding community by conflicting with visual elements or quality of an existing area (i.e., through conflicting style, size, coverage, scale, building materials, etc.) given that the proposed project is of a similar size and scale to the surrounding development. The Project would not result in the removal of or substantial adverse change to one or more features that contribute to the valued visual character or image of the Project area, including but not limited to designated landmarks, historic resources, trees, or rock outcroppings in that there are no designated landmarks, historic resources or rock outcroppings located on the property; the Project only proposed the removal of one (1) tree and includes a

planting plan for 56 more tree (this is not including any plantings that will be required in the 50' Railroad Forest setback). Furthermore, the Project does not substantially obstruct, interrupt, or detract from a valued focal and/or panoramic vista from a public road, trails within an adopted County or State trail system, scenic vista or highway, or recreational area. Additionally, the Project as designed would also not result in an inconsistency with any goals, standards, or policies related to visual resources as given in the General Plan. For the above reasons, it was determined that the Project would not result in potentially significant impacts on visual resources in the Sebastopol community.

Overall, while affecting the visual character of the Project area on a short-term basis, Project construction activities would not substantially alter or degrade the existing visual character or quality of the Project site and surrounding area, for the following reasons: 1) views of construction would be limited in duration and locations; 2) the Project site appearance would be typical of construction sites in urban areas; and 3) construction fencing would be placed along the periphery of the Project site to screen much of the construction from view at street and bike path level.

The Planning Commission is recommending approval.

Tentative Map:

A "Tentative Map" is a map prepared for the purpose of showing the design and improvements for a parcel that is proposed to be subdivided, and the existing conditions in and around it. Tentative Maps are governed by both the City's Subdivision Ordinance (SMC Chapter 16) and the State Subdivision Map Act (SMA). A Tentative Map is similar to a preliminary design in that the approving body utilizes the tentative map for review, and often adds Conditions of Approval to be incorporated into the final document (in this case, the Final Map).

While the Tentative Map is reviewed by Planning Commission and approved by City Council, the Final Map requires only the approval of the City Council (after review for compliance by the City Engineer and Planning Department). Unlike a Tentative Map, the review and approval of a Final Map has very little discretion; it is limited to consistency with the City and SMA requirements, and conformance with the Tentative Map. Therefore, it is important that any conditions or requirements pertaining to changes to the subdivision map be specified at the time of approval of the Tentative Map (to include, easement dedications, number of parcels proposed and size of parcels, restricted areas, etc.).

The applicant is requesting to divide 1.74 acres of land into 18 lots (with one common area lot, #19), ranging from 1,260 square feet to 1,801 square feet. The proposed lot configurations are designed to accommodate the townhomes units, open communal space and parking spaces. Access to the subdivision would be obtained from the existing Morris Street entrance at 6731 Sebastopol Avenue.

For this project, Planning Department Staff and the City Engineer reviewed the Tentative Map and found, with the proposed Conditions of Approval, it will meet the requirements of the City's Subdivision Ordinance and the State SMA.

As noted above, the Planning Commission's review discussed the potential for a public sidewalk and connection utilizing the sidewalk that connects from the Joe Rodota Trail across the front of the property and ends at the shared fence with Park Village. Although the Planning Commission did not feel it was necessary as a Condition of Approval, they felt this is an item the City Council may want to review. One suggestion was to create an irrevocable offer of easement to be required for this extended connector path, if Park Village is ever closed and the site becomes an extension of Tomodachi Park, with the condition that the City could only accept the offer if the area fully became a park (ie if Park Village was closed at a future date). While this is unlikely, the Irrevocable Offer means that the City would not have to decide whether to accept the easement at the time of Final Map, but could do so at a future date. Note, if the offer were to be accepted, it would become a City sidewalk and the City's responsibility.

Staff discussed this option with the applicant, and he is agreeable to such a condition. If the Council concurs, a condition could be added to the project:

The applicant shall provide an Irrevocable Offer of Dedication of the sidewalk extending from the Joe Rodota Trail to the east side of the property. The City shall only accept such an offer if the adjacent City-owned parcel were to become a park and the sidewalk were to connect into a City park.

CEQA- Initial Study/Mitigated Negative Declaration:

Pursuant to California Environmental Quality Act (CEQA) Guidelines §15063, staff prepared an Initial Study to assess the potential adverse environmental effects of the proposed Project. Staff identified potential cumulative adverse impacts associated with the construction of the Project in regards to the following; Aesthetics, Air Quality, Biological Resources, Cultural Resources, Hydrology/Water Quality, and Transportation/Traffic. However, mitigation measures were identified that would reduce these potential impacts to a less than significant level. These mitigation measures are included as Conditions of Approval.

The Mitigated Negative Declaration identified potentially significant impacts regarding aesthetics, biological resources, and hydrology/water quality issues related to the Environmental and Scenic Open Space designation and proximity to the Laguna de Santa Rosa, cultural resources, construction period air quality issues, and traffic. There are available feasible mitigation measures that would reduce these impacts below a level of significance and, as conditioned, which would be applied to the project. These include the following:

1. **Aesthetics** - *Aesthetics concerns are associated with any scenic vistas in relation to its proximity to the Laguna de Santa Rosa.*
 - a. Building height shall be limited to 2 stories.
 - b. Exterior colors shall be selected which blend with the natural surroundings. Final colors shall be approved by the Design Review Board.

- c. Plantings shall be included along the eastern property line, in addition to the proposed 6' fence, to provide additional screening.
- d. Construction fencing shall be placed along the periphery of the Project site on the north, east and west property lines to screen construction activity from view. The southern construction fencing shall run along the required 50 setback buffer from the Railroad Forest property and not from the property line.
- e. Any signage will be minimal and will meet the City's Sign Ordinance guidelines as set forth in SMC 17.120. Signage lighting shall be restricted to external illumination, if applicable.

2. Air Quality – *Air quality concerns are associated primarily with construction of the project.*

- a. Include basic measures to control dust and exhaust during construction. During any construction period ground disturbance, the applicant shall ensure that the project contractor implement measures to control dust and exhaust. Implementation of the measures recommended by BAAQMD and listed below would reduce the air quality impacts associated with grading and new construction to a less than significant level. The contractor shall implement the following best management practices that are required of all projects:
 - i. All haul trucks transporting soil, sand, and other loose material off-site shall be covered.
 - ii. All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per week. The use of dry power sweeping is prohibited.
 - iii. All vehicle speeds on unpaved roads shall be limited to 15 miles per hour (mph).
 - iv. All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.
 - v. Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure Title 13, Section 2485 of California Code of Regulations [CCR]). Clear signage shall be provided for construction workers at all access points.
 - vi. All construction equipment shall be maintained and properly tuned in accordance with manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation.
 - vii. Post a publicly visible sign with the telephone number and person to contact at the Lead Agency regarding dust complaints. This person shall respond and take corrective action within 48 hours. The Air District's phone number shall also be visible to ensure compliance with applicable regulations.

3. **Biological Resources** – *Biological Resources concerns are associated with any scenic vistas in relation to its proximity to the Laguna de Santa Rosa.*
- a. A 50-foot environmental setback from the south property line shall be maintained in perpetuity as a buffer to the environmental resources of the adjacent Railroad Forest property (APN 004-072-014). The open space area and this restriction shall be designated on the Final Map.
 - b. No residential lots, improvements (trails, picnic benches, etc.) or structures shall be permitted inside the 50' setback buffer area.
 - c. Maintenance of this 50' setback and buffer area shall include the following restrictions and requirements:
 - i. Submittal of a plan, to be incorporated into the CCR's for the project, for the maintenance and control of exotic plant species within this setback area. This plan shall be required with the Final Map application.
 - ii. The buffer area shall be replanted with native species appropriate to provide a buffer to the Railroad Forest area prior to final inspection.
 - iii. The removal and replanting plan shall be developed by, or reviewed and approved by, a qualified biologist and City Staff prior to issuance of a building permit.
 - iv. CCR's shall include provisions for post-construction maintenance and control of exotic plant species within this setback area.
 - v. CCR's shall include the prohibition of the development of walking paths, hardscapes, play structures, or accessory structures; or the placement of permanent fixtures or furniture within this buffer. Only restoration shall be permitted in the 50' buffer.
 - vi. Stormwater maintenance requirements in Applicant materials are required and shall be included in the CCR's.
 - d. During construction, heavy equipment undercarriages and tires be washed prior to entering the site in order to remove any invasive plant seeds.
 - i. Applicant shall submit a Construction Management Plan that includes the provision for cleaning of heavy equipment undercarriages and tires prior to entering the site in order to remove any invasive plant seeds.
 - e. A pre-construction survey of on-site trees and trees within the immediate vicinity as determined by a qualified biologist, shall be require if project disturbances occurs during the breeding season of the following special-status species: *Selaphorus sasin* (Allen's Hummingbird); *Botaurus lentiginosus* (American Bittern); *Pelecanus erythrorhynchos* (American White Pelican); *Nycticorax nycticorax* (Black-crowned Night-Heron); *Larus californicus* (Californian Gull); *Hydroprogne caspia* (Caspian Tern); *Accipiter cooperii* (Cooper's Hawk); *Phalacrocorax auratus* (Double-crested Cormorant); *Aquila chrysaetos* (Golden Eagle); *Ammodramus savannarum* (Grasshopper Sparrow); *Ardea herodias* (Great Blue Heron); *Ardea alba* (Great Egret); *Lanius ludovicianus* (Loggerhead Shrike); *Numenius americanus* (Long-billed Curlew); *Falco columbarius* (Merlin); *Picoides nuttallii* (Nuttall's Widoecjer); *Contopus cooperi* (Olive-sided Flycatcher); *Falco peregrinus* (Peregrine Falcon); *Passerculus sandwichensis* (Svannah Sparrow); *Accipiter striatus* (Sharp-shinned Hawk); *Asio*

flammeus (Short-eared Owl); Egretta thula (Snowy Egret); Chaetura vauxi (Vaux's Swift); Elanus leucurus (White-tailed Kite); Setophagapetechia (Yellow Warbler); Icteria virens (Yellow-breasted Chat). The survey shall be completed within 15 days prior to beginning construction during the breeding season, and shall be done by a qualified biologist. Surveys shall be conducted according to a protocol developed in consultation with the DFW. Any active nests discovered during the pre-construction survey shall be marked on a map, and appropriate construction-free setbacks shall be established where no construction activities shall be permitted until all young have fledged and are observed by the qualified biologist to be foraging independently of their parents.

- f. Construction mitigations shall include temporary fencing at the 50-foot setback, with no construction staging or travel permitted within this area.

4. Cultural Resources - *Cultural Resources concerns are associated with any potential archeological resources.*

- a. In the event that any unanticipated artifacts or cultural features are discovered during grading or underground excavations all work in the vicinity of the find shall be stopped until the discovery area can be evaluated by an archaeologist and appropriate actions are taken.

5. Transportation/Traffic- *Traffic concerns are associated with potential increase to traffic and the Morris Street/Sebastopol Avenue intersection, pedestrian and bicycle access to the site; and, emergency access to/from Park Village site.*

- a. Prior to final inspection, the Project shall restripe the southbound Morris Street approach to create two 15-foot lanes (one outbound and one inbound), and if needed, re-install the traffic signal detector position.
- b. Project shall be modified prior to submitting for Design Review to include a walkway/sidewalk connection between the Project site and the existing multi-purpose trail on the west side of the Project site.
- c. The locked access gate between the Village Park Mobile Home Park and the Davis Townhomes properties shall be unlocked and opened in case of emergencies to allow for emergency evacuation.

As mitigated by the IS/MND, and by additional COAs, the project is consistent with the City's General Plan and Zoning Ordinance. The Planning Commission reviewed the MND and recommended approval to the City Council. The Council will need to determine if they agree and, if so, adopt the Mitigated Negative Declaration.

Recommendation:

Staff recommends that the City Council receive a presentation from the applicant, conduct a public hearing and consider any public comments, deliberate, and approve the Use Permit for solely residential, the ESOS Visual Resource Analysis and the Tentative Map; adopt the Mitigated

Negative Declaration; and waive the first reading and introduce an Ordinance rezoning the Parcel.

Exhibits:

City Council Resolution Adopting the Mitigated Negative Declaration

City Council Resolution Approving the Use Permit, ESOS Visual Resource Analysis, and Tentative Map, and Exhibit A: Tentative Map and Exhibit B: Recommended Conditions of Approval

City Council Ordinance Rezoning the Parcel

Attachments:

1. Master Planning Application Form, Environmental Assessment and Application Materials, including: Policy Statement/Policy Statement Table, Project Description, Color Palate/Color rendering sample, Location, Aerial and Parcel Map, Cross Site Section, Lighting Plan and Cutout, Smart Vent Wet and Dry Flooring Info, Carport, Trash Enclosure and Mailbox Info, and Flood Plain Grading Summary and Initial Storm Water Management Plan
2. Initial Study / Mitigated Negative Declaration and attachments, including: Traffic Study and Cultural Resources Evaluation
3. Minutes from 10/23/2018 PC meeting
4. Minutes from 11/14/2018 DRB meeting
5. Draft Minutes from 4/9/2019 PC meeting
6. Planning Commission Resolution 19-03

Plan Submittal: Tentative Map, Grading Plan, Utility Plan, Floor Plan, Elevations, Landscape Plan

RESOLUTION NO. 6239-2019

RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SEBASTOPOL ADOPTING A
MITIGATED NEGATIVE DECLARATION (MND) AND ADOPTING FINDINGS PURSUANT TO
THE CALIFORNIA ENVIRONMENTAL QUALITY ACT,
FOR THE PROPERTY AT 6737 SEBASTOPOL AVENUE (APN 004-063-036)

WHEREAS, an application for a Tentative Subdivision Map, Use Permit to allow a fully residential project in a commercial zone; Planned Community Zoning designation; and, approval of an Environmental Scenic Open Space (ESOS) study modification and approval of ESOS requirements (the "Project"), was filed on August 15, 2018, by Dan Davis. This proposal consists of subdividing a vacant 1.74 acre parcel (APN 004-063-036) parcel, into 19 lots, to be developed with 18 townhomes and a common area lot. Parking will be provided via a surface parking lot on the common area lot.

WHEREAS, the project was the subject of an Initial Study and Mitigated Negative Declaration prepared in compliance with the California Environmental Quality Act (CEQA), which was circulated for public comment from March 7, 2019 and April 8, 2019, including the California State Clearinghouse and the Federated Indians of the Graton Rancheria, consistent with local and State CEQA requirements;

WHEREAS, no comments were received from any State, Tribal, or other responsible agency, nor any members of the public, and no tribal consultation pursuant to California Public Resources Code Section 21080.3.1 was requested by the Federated Indians of Graton Rancheria; and

WHEREAS, the Mitigated Negative Declaration identifies potentially significant impacts regarding aesthetics, biological resources, and hydrology/water quality issues related to the Environmental and Scenic Open Space designation and proximity to the Laguna de Santa Rosa, cultural resources, construction period air quality issues, and traffic as outlined in the Initial Study/Draft Mitigated Negative Declaration; and

WHEREAS, there are available feasible mitigation measures that would reduce these impacts below a level of significance and, as conditioned, which would be applied to the project, as described below:

1. *Aesthetics - Aesthetics concerns are associated with any scenic vistas in relation to its proximity to the Laguna de Santa Rosa.*
 - a. Building height shall be limited to 2 stories.
 - b. Exterior colors shall be selected which blend with the natural surroundings. Final colors shall be approved by the Design Review Board.
 - c. Plantings shall be included along the eastern property line, in addition to the proposed 6' fence, to provide additional screening.

- d. Construction fencing shall be placed along the periphery of the Project Site on the north, east and west property lines to screen construction activity from view. The southern construction fencing shall run along the required 50 setback buffer from the Railroad Forest property and not from the property line.
- e. Any signage will be minimal and will meet the City's Sign Ordinance guidelines as set forth in SMC 17.120. Signage lighting shall be restricted to external illumination, if applicable.

2. Air Quality – *Air quality concerns are associated primarily with construction of the project.*

- a. Include basic measures to control dust and exhaust during construction. During any construction period ground disturbance, the applicant shall ensure that the project contractor implement measures to control dust and exhaust. Implementation of the measures recommended by BAAQMD and listed below would reduce the air quality impacts associated with grading and new construction to a less than significant level. The contractor shall implement the following best management practices that are required of all projects:
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 - iii. All vehicle speeds on unpaved roads shall be limited to 15 miles per hour (mph).
 - iv. All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.
 - v. Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure Title 13, Section 2485 of California Code of Regulations [CCR]). Clear signage shall be provided for construction workers at all access points.
 - vi. All construction equipment shall be maintained and properly tuned in accordance with manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation.
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3. **Biological Resources** – *Biological Resources concerns are associated with any scenic vistas in relation to its proximity to the Laguna de Santa Rosa.*
- a. A 50-foot environmental setback from the south property line shall be maintained in perpetuity as a buffer to the environmental resources of the adjacent Railroad Forest property (APN 004-072-014). The open space area and this restriction shall be designated on the Final Map.
 - b. No residential lots, improvements (trails, picnic benches, etc.) or structures shall be permitted inside the 50' setback buffer area.
 - c. Maintenance of this 50' setback and buffer area shall include the following restrictions and requirements:
 - i. Submittal of a plan, to be incorporated into the CCR's for the project, for the maintenance and control of exotic plant species within this setback area. This plan shall be required with the Final Map application.
 - ii. The buffer area shall be replanted with native species appropriate to provide a buffer to the Railroad Forest area prior to final inspection.
 - iii. The removal and replanting plan shall be developed by, or reviewed and approved by, a qualified biologist and City Staff prior to issuance of a building permit.
 - iv. CCR's shall include provisions for post-construction maintenance and control of exotic plant species within this setback area.
 - v. CCR's shall include the prohibition of the development of walking paths, hardscapes, play structures, or accessory structures; or the placement of permanent fixtures or furniture within this buffer. Only restoration shall be permitted in the 50' buffer.
 - vi. Stormwater maintenance requirements in Applicant materials are required and shall be included in the CCR's.
 - d. During construction, heavy equipment undercarriages and tires shall be washed prior to entering the site in order to remove any invasive plant seeds.
 - i. Applicant shall submit a Construction Management Plan that includes the provision for cleaning of heavy equipment undercarriages and tires prior to entering the site in order to remove any invasive plant seeds.
 - e. A pre-construction survey of on-site trees and trees within the immediate vicinity as determined by a qualified biologist, shall be required if project disturbances occur during the breeding season of the following special-status species: *Selaphorus sasin (Allen's Hummingbird)*; *Botaurus lentiginosus (American Bittern)*; *Pelecanus erythrorhynchos (American White Pelican)*; *Nycticorax nycticorax (Black-crowned Night-Heron)*; *Larus californicus (Californian Gull)*; *Hydroprogne caspia (Caspian Tern)*; *Accipiter cooperii (Cooper's Hawk)*; *Phalacrocorax auratus (Double-crested Cormorant)*; *Aquila chrysaetos (Golden Eagle)*; *Ammodramus savannarum (Grasshopper Sparrow)*; *Ardea herodias (Great Blue Heron)*; *Ardea alba (Great Egret)*; *Lanius ludovicianus (Loggerhead Shrike)*; *Numenius americanus (Long-billed Curlew)*; *Falco columbarius (Merlin)*; *Picoides nuttallii (Nuttall's Woodpecker)*; *Contopus*

cooperi (Olive-sided Flycatcher); Falco peregrinus (Peregrine Falcon); Passerculus sandwichensis (Savannah Sparrow); Accipiter striatus (Sharp-shinned Hawk); Asio flammeus (Short-eared Owl); Egretta thula (Snowy Egret); Chaetura vauxi (Vaux's Swift); Elanus leucurus (White-tailed Kite); Setophagapetechia (Yellow Warbler); Icteria virens (Yellow-breasted Chat). The survey shall be completed within 15 days prior to beginning construction during the breeding season, and shall be done by a qualified biologist. Surveys shall be conducted according to a protocol developed in consultation with the DFW. Any active nests discovered during the pre-construction survey shall be marked on a map, and appropriate construction-free setbacks shall be established where no construction activities shall be permitted until all young have fledged and are observed by the qualified biologist to be foraging independently of their parents.

- f. Construction mitigations shall include temporary fencing at the 50-foot setback, with no construction staging or travel permitted within this area.
4. *Cultural Resources - Cultural Resources concerns are associated with any potential archeological resources.*
 - a. In the event that any unanticipated artifacts or cultural features are discovered during grading or underground excavations all work in the vicinity of the find shall be stopped until the discovery area can be evaluated by an archaeologist and appropriate actions are taken.
 5. *Transportation/Traffic- Traffic concerns are associated with potential increase to traffic and the Morris Street/Sebastopol Avenue intersection, pedestrian and bicycle access to the site; and, emergency access to/from Park Village site.*
 - a. Prior to final inspection, the Project shall restripe the southbound Morris Street approach to create two 15-foot lanes (one outbound and one inbound), and if needed, re-install the traffic signal detector position.
 - b. Project shall be modified prior to submitting for Design Review to include a walkway/sidewalk connection between the Project Site and the existing multi-purpose trail on the west side of the Project Site.
 - c. The locked access gate between the Park Village Mobile Home Park and the Davis Townhomes properties shall be unlocked and opened in case of emergencies to allow for emergency evacuation, and

WHEREAS, on March 13, 2018, the Planning Commission conducted a preliminary review with the Planning Commission to review the project; and

WHEREAS, on April 9, 2019, the Planning Commission held a duly noticed public hearing, received a staff report, accepted public testimony, and duly considered the application, including, but not limited to, the application materials, Mitigated Negative Declaration, staff report, and public comments, duly considered the application, and unanimously adopted PC Resolution 19-03 recommending approval of the Project to the City Council;

and

WHEREAS, on May 7, 2019, the City Council held a duly noticed public hearing, received a staff report, accepted public testimony, and duly considered the application.

NOW, THEREFORE, BE IT RESOLVED THAT, The City Council of the City of Sebastopol, California, does hereby adopt the Mitigated Negative Declaration and adopt findings pursuant to the California Environmental Quality Act as set forth above, with the mitigation measures set forth above and in the Mitigated Negative Declaration, which are hereby incorporated into the Project and made Conditions of the Project.

The above and foregoing Resolution was duly passed, approved and adopted at a meeting by the City Council on the 7th day of May, 2019, by the following vote:

VOTE:

Ayes: Councilmembers Carnacchi, Glass, Gurney and Vice Mayor Slayter
Noes: None
Abstain: None
Absent: Mayor Hinton

APPROVED: _____


Vice Mayor Patrick Slayter

ATTEST: _____



Mary Gourley, Assistant City Manager/City Clerk, MMC

APPROVED AS TO FORM: _____


Larry McLaughlin, City Attorney

RESOLUTION NO. 6240-2019

RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SEBASTOPOL APPROVING
A USE PERMIT FOR A SOLELY-RESIDENTIAL DEVELOPMENT IN A COMMERCIAL ZONE;
AN ENVIRONMENTAL AND SCENIC OPEN SPACE STUDY; AND
A TENTATIVE SUBDIVISION MAP
FOR THE PROPERTY AT 6737 SEBASTOPOL AVENUE
(APN 004-063-036)

WHEREAS, an application for a Tentative Subdivision Map, Use Permit to allow a fully residential project in a commercial zone; Planned Community Zoning designation; and, approval of an Environmental Scenic Open Space (ESOS) study modification and approval of ESOS requirements (the "Project"), was filed on August 15, 2018, by Dan Davis. This proposal consists of subdividing a vacant 1.74 acre parcel (APN 004-063-036) parcel, into 19 lots, to be developed with 18 townhomes and a common area lot. Parking will be provided via a surface parking lot on the common area lot; and

WHEREAS, with the approval of the Use Permit and ESOS Visual Resource Analysis, the proposed residential-only development will be consistent with the subject property's General Plan designation of Central Core and Zoning Designation of Downtown Core; and,

WHEREAS, the Project, as conditioned, is consistent with the Central Core Land Use designation of the General Plan and General Plan policies, in that:

- a) The proposal is consistent with Policy LU 1-7 as the site is located at the edge of the Downtown Core District and is surrounded on three sides by existing residential and commercial developments.
- b) The proposal is consistent with Policy LU 6-2 as it provides much needed housing located in close proximity to various community services and transit.
- c) The proposal is consistent Policy COS 12-9 as only one tree is slated for removal. Additionally, Condition of Approvals have been added in regards to the 50' Railroad Forest setback which require this area to be maintained in its natural state, and which requiring buffer plantings / mitigation planting in said 50' setback.
- d) The proposal is consistent with Policy COS 12-12 as the development is clustered in the center of the project site, with the townhomes arranged in the three clusters with open space located in the center, and around the perimeter of the property. The southern edge of the site will have the largest open space, with a 50' setback from the Railroad Forest property. As conditioned, an open space easement shall be required on this section of the property which would prevent

disturbance of this area with the exception of restoration and removal of invasive species.

- e) The proposal is consistent with Policy CD 1-7 as it an infill development which provides much needed housing located in close proximity to pedestrian and bicycle trails and has convenient access to local amenities and transit. The project includes the requirement to construct a bicycle/pedestrian connection to the existing connector trail located at the west side of the site.
- f) The proposal is consistent with Policy CD 3-4 as the project was subject to a Cultural Resource Evaluation which found that the site did not contain any known or evident cultural, archeological or historic resources. In the event that any unanticipated artifacts or cultural features are discovered during grading or underground excavations all work in the vicinity of the find shall be stopped until the discovery area can be evaluated by an archaeologist and appropriate actions are taken.
- g) The proposal is consistent with the City's Housing Element, in that the site is identified as a site in the Site Inventory as land suitable for residential development.
- h) The proposal is consistent with the City's Housing Element, in that, as conditioned, the proposal would meet the City's Inclusionary Housing requirements by providing on-site Inclusionary Unit(s) and providing housing opportunities at a variety of income levels.

WHEREAS, the development of the project as residential-only in a commercial zone is appropriate, in that the establishment, maintenance, and operation of the Use applied for, a solely residential development in a commercial zone, will not, under the circumstances of the particular case, be detrimental to the health, safety, peace, morals, comfort, or general welfare of persons residing or working in the area of such use or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the City; due to the following reasons:

- a) The Zoning Ordinance allows for a solely-residential development on a commercially zoned parcel, provided it does not detract from the intent of the commercial spaces on the street. The property is located behind existing commercial and the residential development to the northeast and east. No commercial frontages are shared with this property or would be disrupted by the use of this site as solely residential.
- b) The project site is unique as it is located on the edge of an urbanized area and is considered part of the City's Downtown but abuts an environmentally sensitive habitat.

- c) The project will result in the removal of 1 existing tree and result in the planting of an additional 50+ trees, as currently proposed.
- d) The proposed site is located within walking distance of existing commercial, recreational uses and will provide additional housing opportunities for locals.
- e) It is not well suited for commercial development but does appear to be an appropriate location for much needed housing.
- f) The proposed development is appropriate for the site.

WHEREAS, on March 13, 2018, the Planning Commission conducted a preliminary review with the Planning Commission to review the project; and

WHEREAS, on October 23, 2018, the Planning Commission conducted a duly-noticed Public Hearing, heard public testimony, deliberated, and approved a reduction of the Environmental Scenic Open Space (ESOS) study to require a Visual Analysis only, and approved a reduction in the ESOS buffer setback to be 50 feet; and

WHEREAS, the applicant made adjustments in the proposal based on community comments and Planning Commission review of the ESOS reduced setback request; and

WHEREAS, a Visual Analysis was prepared in compliance with the requirements of the ESOS overlay zoning and considering the preliminary review comments from the public comments and Planning Commission and Design Review Board reviews held on October 23, 2018 and November 14, 2018; and

WHEREAS, the applicant submitted a revised Tentative Map on December 19, 2018 for the subdivision of the 1.74 acre lot into 19 parcels, including 18 residential parcels for townhomes and a common area lot and other related improvements; and

WHEREAS, the Visual Resource Analysis (VRA) found that the Project will be consistent with the Zoning Ordinance ESOS Study requirements in that:

- a) The application proposes only two stories (as opposed to the 4 stories permitted in the district) and was designed with colors that would blend into the natural landscaping.
- b) Considerable open space and an extensive planting plan (including the addition of 50+ trees, along with other shrubs and vines) is included as part of the proposed development.
- c) The site is predominantly blocked from the main view corridor of Bodega Ave/Highway 12 by a residence and a mobile home park, as well as commercial and retail facilities; and

WHEREAS, the Visual Resource Analysis further found that the Project would not result in potentially significant impacts on visual resources in the Sebastopol community, due to the following:

- a) The Project would not result in the introduction of features that would significantly detract from or contrast with the visual character of the surrounding community by conflicting with visual elements or quality of an existing area (i.e., through conflicting style, size, coverage, scale, building materials, etc.) given that the proposed project is of a similar size and scale to the surrounding development.
- b) The Project would not result in the removal of or substantial adverse change to one or more features that contribute to the valued visual character or image of the Project area, including but not limited to designated landmarks, historic resources, trees, or rock outcroppings in that there are no designated landmarks, historic resources or rock outcroppings located on the property; the Project only proposed the removal of one (1) tree and includes a planting plan for 56 more tree (this is not including any plantings that will be required in the 50' Railroad Forest setback).
- c) The Project does not substantially obstruct, interrupt, or detract from a valued focal and/or panoramic vista from a public road, trails within an adopted County or State trail system, scenic vista or highway, or recreational area.
- d) The Project as designed and Conditioned would also not result in an inconsistency with any goals, standards, or policies related to visual resources as given in the General Plan.
- e) While affecting the visual character of the Project area on a short-term basis, Project construction activities would not substantially alter or degrade the existing visual character or quality of the Project Site and surrounding area, for the following reasons:
 - i. views of construction would be limited in duration and locations;
 - ii. the Project Site appearance would be typical of construction sites in urban areas;
 - iii. construction fencing would be placed along the periphery of the Project Site to screen much of the construction from view at street and bike path level.

WHEREAS, on April 9, 2019, the Planning Commission conducted a duly-noticed Public Hearing, received a staff report, heard public testimony and considered the applications for a Use Permit for a solely-residential development in a commercial zone; Visual Analysis for an ESOS overlay district; Tentative Subdivision Map, Planned Community Zoning designation, and CEQA Mitigated Negative Declaration, and unanimously adopted

PC Resolution 19-03 recommending approval of the Project to the City Council; and

WHEREAS, on May 7, 2019, the City Council held a duly noticed public hearing, received a staff report, accepted public testimony, and duly considered the application for a Use Permit for a solely-residential development in a commercial zone; Visual Analysis for an ESOS overlay district; Tentative Subdivision Map, Planned Community Zoning designation, and CEQA Mitigated Negative Declaration; and

WHEREAS, The City of Sebastopol City Council has adopted a Mitigated Negative Declaration (MND) for the "Davis Townhomes" Project (the "Project") located at 6737 Sebastopol Avenue in a separate Resolution based on the findings of fact of the Adopting of Resolution No. 6239-2019

NOW, THEREFORE, BE IT RESOLVED THAT, The City Council of the City of Sebastopol, California, does hereby Approve, based on the findings above and subject to the Conditions of Approval in Exhibit B:

1. A Use Permit to permit a solely-residential development in a commercial zone;
2. The ESOS Visual Resources Analysis; and
3. Approve the Tentative Map, included in Exhibit A, to subdivide a 1.74 acre parcel to create a 19 lot subdivision, to include 18 residential townhome lots and a common area parcel.

The above and foregoing Resolution was duly passed, approved and adopted at a meeting by the City Council on the 7th day of May, 2019, by the following vote:

VOTE:

Ayes: Councilmembers Carnacchi, Glass, Gurney and Vice Mayor Slayter
Noes: None
Abstain: None
Absent: Mayor Hinton

APPROVED: _____

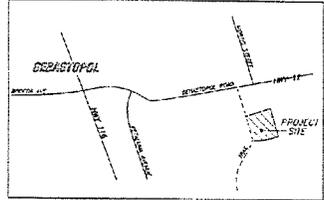
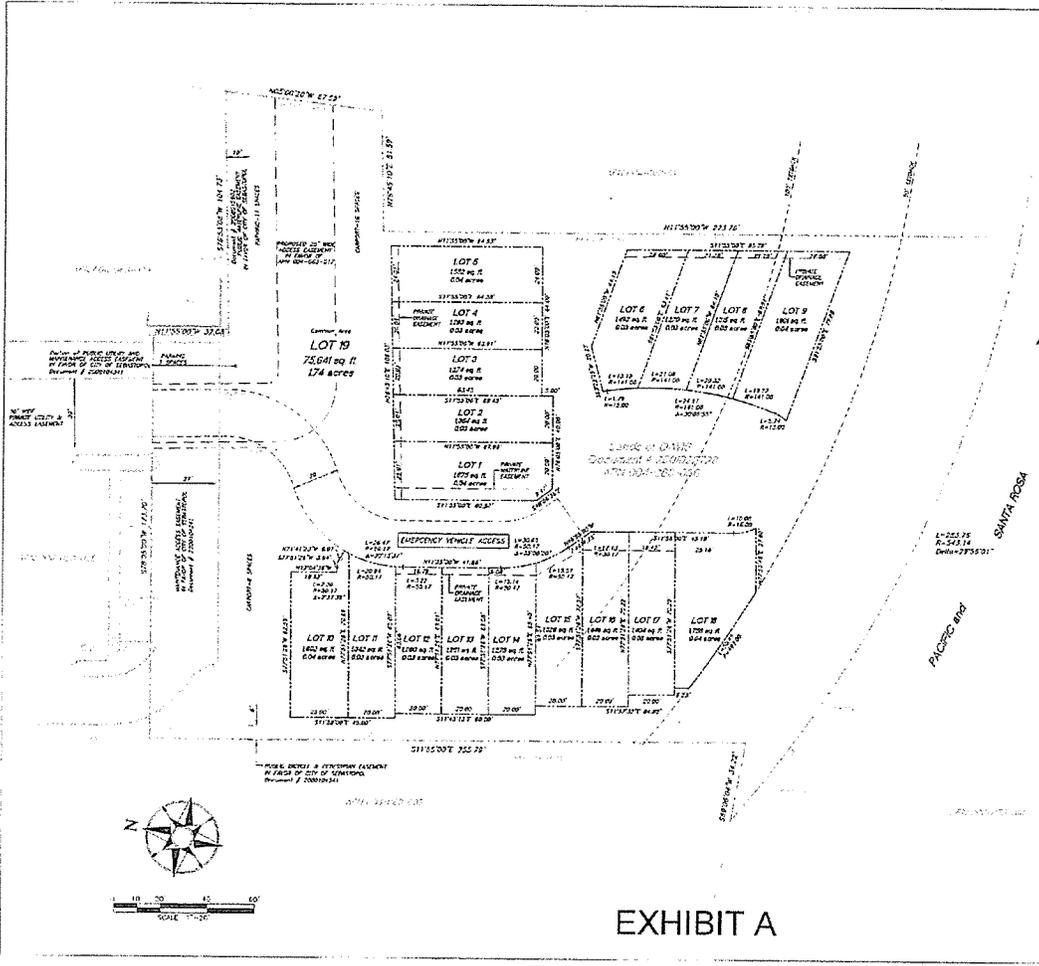

Vice Mayor Patrick Slayter

ATTEST: _____


Mary Gourley, Assistant City Manager/City Clerk, MMC

APPROVED AS TO FORM: _____


Larry McLaughlin, City Attorney



LOCATION MAP
TENTATIVE MAP
for
DAVIS TOWNHOMES
A PLANNED COMMUNITY

PROJECT DATA

APPLICANT AND PROPERTY OWNER:
DANIEL DAVIS
1051 TODD ROAD
SANTA ROSA, CA 95407

PROPERTY SURVEYOR:
BRAD & SUSANNE
LADD ASSOCIATES
1445 REGIONAL PUMP
SANTA ROSA, CA 95403

PROJECT ENGINEER:
ANDREW L. WILBURN
LADD ASSOCIATES
1445 REGIONAL PUMP
SANTA ROSA, CA 95403

EXISTING USE: UNDEVELOPED (AGRICULTURE)

PROPOSED USE: PLANNED CONDOMINIUM

TOTAL NUMBER OF LOTS: 18 LOTS

AVERAGE PLOT SIZE: 0.03 AC

AVERAGE PLOT SIZE: 0.11 AC

AVERAGE PLOT SIZE: 0.01 AC

EXISTING GROSS ACRES: 1.74 ACRES

SHEET INDEX

1. TENTATIVE MAP SHEET
2. GRADING, DRAINAGE and STORM WATER TREATMENT
3. UTILITY PLAN

LEGEND/ABBREVIATIONS

PROPOSED FENCE LINE	ACTUAL ELEVATION
EXISTING NEIGHBOR LINE	PROPOSED FLOOR
EXISTING EASEMENT (SEE 2200224-141)	PROPERTY
PERMITS LINE (TYPE NUMBER)	WATER METER
EDGE OF ADJACENT PARCELS	SEWER COVER CLEAN-OUT
EDGE OF ADJACENT PARCELS	ADJUST ELEVATION POLE
EDGE OF ADJACENT PARCELS	SEWER COVER POLE
EDGE OF ADJACENT PARCELS	FOR PERMITS
EDGE OF ADJACENT PARCELS	EXISTING

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DAVIS TOWNHOMES
A PLANNED COMMUNITY
TENTATIVE MAP
DANIEL DAVIS
1051 TODD ROAD, SANTA ROSA, CA, 95407

DATE:	05/17/11
BY:	DAVID
CHECKED:	DAVID
APPROVED:	DAVID
SCALE:	AS SHOWN
DATE:	05/17/11
BY:	DAVID
CHECKED:	DAVID
APPROVED:	DAVID
SCALE:	AS SHOWN
DATE:	05/17/11
BY:	DAVID
CHECKED:	DAVID
APPROVED:	DAVID
SCALE:	AS SHOWN

1 OF 3

EXHIBIT A

EXHIBIT B
FINAL CONDITIONS OF APPROVAL

Use Permit for solely-residential development in a commercial district, Planned Community Zoning Designation, Visual Resource Analysis Study Approval, Tentative Subdivision Map, and Adoption of a Mitigated Negative Declaration
6737 Sebastopol Avenue
APN 004-063-036, File 2018-82

Conditions of Approval:

1. Plans and elevations shall be in substantial conformance with plans prepared by Katherine Austin, and date-stamped received on December 19, 2018, and on file at the City of Sebastopol Planning Department, except as modified herein or by the Design Review Board. Any modifications to the plans shall be reviewed and approved by Staff prior to modification.
2. No additions to habitable space shall be allowed. Additions to exterior decks, porches, and patios, as well as accessory structures, shall be subject to the development standards included in Exhibit A Findings for Approval.
3. The Final Map submitted for review and approval shall be substantially consistent with the Tentative Map approval which is granted for 18 residential townhouse lots; common parcel areas for parking and vehicle egress, and pedestrian and bicycle access; common landscaped open space; emergency egress; and a restricted open space environmental buffer as indicated on the tentative map and these conditions.
4. The applicant shall provide an Irrevocable Offer of Dedication of the sidewalk extending from the Joe Rodota Trail to the east side of the property.
5. Aesthetics
 - a. Building height shall be limited to 2 stories.
 - b. Exterior colors shall be selected which blend with the natural surroundings. Final colors shall be approved by the Design Review Board. Any future modification of exterior colors shall require approval of the Planning Director prior to the application of such colors.
 - c. Plantings shall be included along the eastern property line, in addition to the proposed 6' fence, to provide additional screening.
 - d. Construction fencing shall be placed along the periphery of the Project Site on the north, east and west property lines to screen construction activity from view. The southern construction fencing shall run along the required 50 setback buffer from the Railroad Forest property and not from the property line.
 - e. Any signage will be minimal and will meet the City's Sign Ordinance guidelines as set forth in SMC 17.120. Signage lighting shall be restricted to external illumination, if applicable.
6. Air Quality
 - a. Include basic measures to control dust and exhaust during construction. During any construction period ground disturbance, the applicant shall ensure that the project contractor implement measures to control dust and exhaust. Implementation of the measures recommended by BAAQMD and listed below would reduce the air quality

impacts associated with grading and new construction to a less than significant level. The contractor shall implement the following best management practices that are required of all projects:

- i. All haul trucks transporting soil, sand, and other loose material off-site shall be covered.
- ii. All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per week. The use of dry power sweeping is prohibited.
- iii. All vehicle speeds on unpaved roads shall be limited to 15 miles per hour (mph).
- iv. All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.
- v. Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure Title 13, Section 2485 of California Code of Regulations [CCR]). Clear signage shall be provided for construction workers at all access points.
- vi. All construction equipment shall be maintained and properly tuned in accordance with manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation.
- vii. Post a publicly visible sign with the telephone number and person to contact at the Lead Agency regarding dust complaints. This person shall respond and take corrective action within 48 hours. The Air District's phone number shall also be visible to ensure compliance with applicable regulations.

7. Biological Resources

- a. A 50-foot environmental setback from the south property line shall be maintained in perpetuity as a buffer to the environmental resources of the adjacent Railroad Forest property (APN 004-072-014). The open space area and this restriction shall be designated on the Final Map.
- b. No residential lots or structures shall be permitted inside the 50' setback buffer area. Passive recreation uses (pedestrian trails, benches, etc.) may be allowed with a Use Permit approved by the Planning Director.
- c. Maintenance of this 50' setback and buffer area shall include the following restrictions and requirements:
 - i. Submittal of a plan, to be incorporated into the CCR's for the project, for the maintenance and control of exotic plant species within this setback area. This plan shall be required with the Final Map application.
 - ii. The buffer area shall be replanted with native species appropriate to provide a buffer to the Railroad Forest area prior to final inspection.
 - iii. The removal and replanting plan shall be developed by, or reviewed and approved by, a qualified biologist and City Staff prior to issuance of a building permit.
 - iv. CCR's shall include provisions for post-construction maintenance and control of exotic plant species within this setback area.
 - v. CCR's shall include the prohibition of the development of walking paths, hardscapes, play structures, or accessory structures; or the placement of permanent fixtures or furniture within this buffer. Only restoration shall be permitted in the 50' buffer.

- vi. Stormwater maintenance requirements in Applicant materials are required and shall be included in the CCR's.
- d. During construction, heavy equipment undercarriages and tires be washed prior to entering the site in order to remove any invasive plant seeds.
 - i. Applicant shall submit a Construction Management Plan that includes the provision for cleaning of heavy equipment undercarriages and tires prior to entering the site in order to remove any invasive plant seeds.
- e. A pre-construction survey of on-site trees and trees within the immediate vicinity as determined by a qualified biologist, shall be required if project disturbances occur during the breeding season of the following special-status species: *Selaphorus sasin* (Allen's Hummingbird); *Botaurus lentiginosus* (American Bittern); *Pelecanus erythrorhynchos* (American White Pelican); *Nycticorax nycticorax* (Black-crowned Night-Heron); *Larus californicus* (Californian Gull); *Hydroprogne caspia* (Caspian Tern); *Accipiter cooperii* (Cooper's Hawk); *Phalacrocorax auratus* (Double-crested Cormorant); *Aquila chrysaetos* (Golden Eagle); *Ammodramus savannarum* (Grasshopper Sparrow); *Ardea herodias* (Great Blue Heron); *Ardea alba* (Great Egret); *Lanius ludovicianus* (Loggerhead Shrike); *Numenius americanus* (Long-billed Curlew); *Falco columbarius* (Merlin); *Picoides nuttallii* (Nuttall's Woodpecker); *Contopus cooperi* (Olive-sided Flycatcher); *Falco peregrinus* (Peregrine Falcon); *Passerculus sandwichensis* (Savannah Sparrow); *Accipiter striatus* (Sharp-shinned Hawk); *Asio flammeus* (Short-eared Owl); *Egretta thula* (Snowy Egret); *Chaetura vauxi* (Vaux's Swift); *Elanus leucurus* (White-tailed Kite); *Setophaga petechia* (Yellow Warbler); *Icteria virens* (Yellow-breasted Chat). The survey shall be completed within 15 days prior to beginning construction during the breeding season, and shall be done by a qualified biologist. Surveys shall be conducted according to a protocol developed in consultation with the DFW. Any active nests discovered during the pre-construction survey shall be marked on a map, and appropriate construction-free setbacks shall be established where no construction activities shall be permitted until all young have fledged and are observed by the qualified biologist to be foraging independently of their parents.
- f. Construction mitigations shall include temporary fencing at the 50-foot setback, with no construction staging or travel permitted within this area.

8. Cultural Resources

- a. In the event that any unanticipated artifacts or cultural features are discovered during grading or underground excavations all work in the vicinity of the find shall be stopped until the discovery area can be evaluated by an archaeologist and appropriate actions are taken.

9. Transportation/Traffic

- a. Prior to final inspection, the Project shall restripe the southbound Morris Street approach to create two 15-foot lanes (one outbound and one inbound), and if needed, re-install the traffic signal detector position.
- b. Project shall be modified prior to submitting for Design Review to include a walkway/sidewalk connection between the Project Site and the existing multi-purpose trail on the west side of the Project Site.
- c. The locked access gate between the Village Park Mobile Home Park and the Davis Townhomes properties shall be unlocked and opened in case of emergencies to allow for emergency evacuation.

10. The project lighting shall use downcast (“dark sky compliant”) lighting and conscientious placement of any proposed lighting. The detailed lighting plan shall be reviewed by the Design Review Board for compliance this requirement.
11. The applicant shall provide documentation as needed for compliance with the environmental mitigations listed in these conditions and the Project’s Mitigated Negative Declaration.
12. Inclusionary Housing
 - a. The applicant shall fulfill the Inclusionary Housing requirements by providing the Inclusionary Units on-site. The number of units shall be in compliance with SMC 17.250.050(A).
 - b. Inclusionary Units shall be constructed at the same time as the market rate units.
 - c. The exterior appearance of the Inclusionary Units shall be the same as the market rate units in exterior materials and finish. This condition shall be maintained as an on-going condition.
 - d. Prior to approval of the Final Map by City Council, the applicant shall file a plan, for review and approval by the City Council, outlining how the Project will meet the Inclusionary Housing requirements listed in SMC 17.250. These requirements include, but are not limited to, number of units and rate of affordability; distribution and location of the units within the development; any requests for alternative amenities or modification to the size of the units.
 - e. Prior to approval of the Final Map by City Council, the applicant shall file a plan for review and approval by the City Council, of the on-going affordability administration through the Housing Land Trust of Sonoma County.
 - f. The Final Map shall bear a note indicating whether compliance with this section must be met prior to issuance of a building permit for each lot created by such map, and, as applicable, shall designate which lots are required to be developed with inclusionary units.
 - g. Prior to recording of the Final Map, the City Council shall approve an Inclusionary Housing Agreement governing and encumbering the project, as well as the form of an Affordable Housing Agreement and “Regulatory Agreement” governing and encumbering each of the Inclusionary Units. The Inclusionary Housing Agreement shall be executed by and between the City and Developer and recorded concurrently with the recording of a Final Map.
13. Prior to approval of a Final Map, final CCR’s (Codes, Covenants and Restrictions) shall be submitted for review and approval by the City Attorney, Planning Department, and City Engineer that implement the project as presented in the Project application materials and these conditions of approval This shall include ongoing obligations of the homeowners associated to maintain improvements within the subdivision.
14. The development’s CCRs shall include details regarding the maintenance of common and/or private open space located on the project site. This shall include a prohibition of the use of non-biodegradable and toxic chemicals in maintenance of both common and private open space areas.
15. The development’s open spaces (including the 50’ buffer from the Railroad Forest) shall be maintained by the property owner, not by the City. Maintainance shall be consistent with the City’s adopted Laguna Wetlants Preserve Restoration and Management Plan.

16. The project site includes protected trees intended to remain. Protective measures are required for these trees. All final tree protection measures shall be submitted for review and approval by the City Arborist prior to issuance of Improvement Plans.
17. A Tree Removal permit is required for the one tree proposed for removal.
18. Design Review approval is required by the Design Review Board for the design of the units, carports, site features, landscaping, and other amenities.
19. A Flood Plain Development Permit shall be required prior to any construction, and the Project shall comply with the City's Floodplain Ordinance (SMC 15.16).
20. The project shall be subject to impact fees as adopted by Council.
21. The City of Sebastopol and its agents, officers and employees shall be defended, indemnified, and held harmless from any claim, action or proceedings against the City, or its agents, officers and employees to attach, set aside, void, or annul the approval of this application or the environmental determination which accompanies it, or which otherwise arises out of or in connection with the City's action on this application, including but not limited to, damages, costs, expenses, attorney's fees, or expert witness fees.
22. The Planning Director shall interpret applicable requirements in the event of any redundancy or conflict in conditions of approval.
23. The Tentative Map shall expire 24 months after its approval or conditional approval unless an extension is approved as provided in SMC 16.28.100 and in accordance with the State Subdivision Map Act.
24. All other approvals than the Tentative Map shall be valid for three years, except that the applicant may request a one (1) year extension of this approval from the Planning Director, pursuant to Section 17.250.050 of the Zoning Ordinance.

Public Works/ Engineering Department:

25. Submittals for Engineering Plan Check shall be made at the Public Works Department. Plan Check Deposit shall be paid at the time of submittal. Call (707) 823-2151 for information.
26. Any exceptions or variances from these conditions will require the written approval of the City Engineer or approval of the City Council if required by City Code.

PRIOR TO APPROVAL OF THE SITE IMPROVEMENT PLANS, THE FOLLOWING CONDITIONS SHALL BE SATISFIED:

Final Map

27. A Final Map and prepared by a licensed surveyor or civil engineer, shall be prepared and submitted for the review and approval of the City Engineer. The map shall conform to the requirements of the Subdivision Map Act and local ordinances. Upon recording of the map, the subdivision is valid.
28. All property corners of lots within the subdivision shall be monumented with no less than 3' long by 1/2" diameter galvanized steel pipe imbedded no less than 24" into the earth, except

as expressly permitted in writing by the City Engineer.

29. The following notes shall appear on the Local Agency sheet of the Final Map:
 - a. "Building Permits shall be subject to payment of development fees in effect at the time of permit issuance."
 - b. "A 20 foot setback for fences greater than 3 feet in height is required on all street frontages and corner lots if required by the City Engineer."
30. The Final Map shall state:
 - a. The assessor's parcel number
 - b. Total area of land being subdivided (in acres)
 - c. Total number of lots being created
31. Developer shall either complete the required construction prior to recordation of the map or enter into an Improvement Agreement and post security with the City of Sebastopol prior to the filing of the Final Map, agreeing to complete the required construction within 24 months after the filing of the map. The Improvement Agreement shall be recorded with the map.
32. The applicant shall transmit by certified mail a copy of the conditionally approved Tentative Map together with a copy of Section 66436 of the State Subdivision Map Act to each public entity or public utility that is an easement holder of record. Written compliance shall be submitted to the City of Sebastopol.
33. The applicant shall execute a covenant running with the land on behalf of itself and its successors, heirs, and assigns agreeing to annex this subdivision into the existing City of Sebastopol Lighting Assessment District.
34. The developer shall submit CC&Rs for the project, which provide for the maintenance of the private facilities, including, but not limited to, the private streets and utilities.
35. The Access Easement in favor of APN 004-063-017 shall be dedicated on the Final Map and via a separate easement deed.
36. The developer shall dedicate a Public Utilities Easement for the onsite public waterlines from the west end of the Morris St right of way, up to and including the fire hydrants and water meters, and the public line that extends east to serve APN 004-063-017.

Improvement Plans – General

37. Improvement Plans prepared by a Registered Civil Engineer shall be submitted for the review and approval of the City Engineer showing grading, paving, utilities and drainage. The improvements plans shall include street and utility information including all concrete curb and gutter, sidewalk, striping and signing, paving, water lines and sewer lines, erosion control and any necessary transitions for the portion of the public street fronting the development. All improvements shall be in accordance with the City of Sebastopol Standard Improvement Details. Improvement Plans shall include a Storm Water Pollution Prevention Plan including winterization and erosion protection.
38. The improvement plans for work in the State right of way shall also be submitted to Caltrans for Encroachment Permit review. The developer shall obtain an Encroachment Permit for the work within the State right of way prior to approval of the improvement plans by the City. The

developer's contractor shall obtain an Encroachment Permit to perform the work in the State right of way prior to beginning that work.

39. The improvement plans must be evaluated by an arborist to assess the impact of the development on any existing trees and develop a site specific Tree Protection Plan. Improvement Plans shall include the location and size of all existing trees to be removed, and trees to remain. Trees on adjacent property which overhang the project boundary shall be afforded equal protection. Improvement plans shall show all measures identified in the Tree Protection Plan as needed, to protect trees during construction.
40. The project shall include post-construction stormwater BMPs in accordance with the City's Low Impact Development manual and Section 15.78 of the Municipal Code.
41. The following notes shall appear on the improvement plan cover sheet:
"During construction, the Developer shall be responsible for controlling noise, odors, dust and debris to minimize impacts on surrounding properties and streets."

Improvement Plans – Specifics

42. Morris St: Any failed portions of Morris St shall be removed and replaced up to the intersection at Sebastopol Ave/HWY 12.
43. Interior Drive Aisles: The developer shall construct the drive aisle over the Access Easement in favor of APN 004-063-017 with a minimum of 3 inches of asphalt over a minimum of 12 inches of aggregate base.
44. Emergency Vehicle Access: The surface of the Emergency Vehicle Access shall be constructed to the satisfaction of the Fire Marshall.

Soils

45. The applicant shall submit to the City of Sebastopol for review and approval, a detailed Soils Report certified by a Civil Engineer registered in the State of California and qualified to perform soils work. The report shall include a minimum of geotechnical investigation with regard to liquefaction, expansive soils, and seismic safety. The report shall also include pavement recommendations based on anticipated subgrade soils and traffic loads. The grading and improvement plans shall incorporate the recommendations of the approved Soils Report.

Undergrounding

46. During construction all utility distribution facilities on site shall be placed underground, except surface-mounted transformers, pedestal mounted terminal boxes, meter cabinets, and fire hydrants. Appropriate easements shall be provided to facilitate these installations.

Streets, Traffic & Circulation

47. No pervious paving or stamped concrete shall be installed in the existing or future public right of way.
48. Any additional proposed pavement removal and re-paving will be subject to the review and approval of the City Engineer.

Grading

49. The applicant shall submit to the City of Sebastopol for review and approval, a grading plan prepared by a Registered Civil Engineer; shall obtain a Grading Permit; and shall post sufficient surety guaranteeing completion.
50. The grading plan shall clearly show all existing survey monuments and property corners and shall state that they shall be protected and preserved.
51. The grading plan shall clearly show areas of possible soil contamination, along with the appropriate steps to deal with contaminated soils.
52. Both temporary and permanent erosion control plans shall be submitted for review and approval along with the grading plan. Permanent erosion control measures shall include hydroseeding of all graded slopes within 60 days of completion of grading.
53. If the site will require import or export of dirt, the applicant shall submit in writing the proposed haul routes for the trucks and equipment. The haul routes must be approved by the City prior to import/export work commencing.

Storm Drain

54. The applicant shall submit to the City of Sebastopol for review and approval, drainage plans, hydrologic, and hydraulic calculations prepared by a Registered Civil Engineer. The drainage plans and calculations shall indicate the following conditions before and after development:
 - a. Quantities of water, water flow rates, drainage areas and patterns and drainage courses. Hydrology shall be per current Sonoma County Water Agency Standards.
 - b. Project drainage shall be designed using the 10-year storm average flow and 100 year peak flow.
55. No drainage may discharge across sidewalks.
56. Any proposed bioswales must be wholly contained outside of the existing or proposed public right of way.
57. All storm drain inlets shall be permanently marked using a permanent polyurethane marker with the legend, "No Dumping – Drains To Creek."
58. The applicant shall demonstrate for each building pad to the satisfaction of the City of Sebastopol as follows:
 - a. Feasible access during a 10-year frequency storm.

Water

59. The developer shall install new domestic, irrigation and fire service laterals to serve the new building. All water mains shall be sized to provide adequate fire flows to the buildings. All water services shall be provided with backflow prevention devices in accordance with State and City standards.
60. New water laterals shall be constructed in accord with City Standards. Meter locations shall be subject to approval by the Sebastopol Public Works Department. The improvement plans shall show water services to each lot.

61. Fire protection shall be in accord with the requirements of Sebastopol Fire Department. With the submittal of the improvement plans, calculations shall be provided to the City and the Sebastopol Fire Department to ensure that adequate water pressures are available to supply hydrant flows and sprinkler flows.
62. New water mains and fire hydrants shall be constructed and functional prior to the issuance of the building permit for any above-ground structures.
63. All hydrants shall be covered with bags indicating that the hydrant is not active until flow tests are completed by the City and the hydrants are approved.
64. All aboveground backflow hardware shall be screened with an architectural screen compatible with the adjacent building.

Wastewater (sanitary sewer)

65. A sanitary sewer application shall be submitted to the Building Department for review and approval. Discharge permits for individual uses shall be subject to the requirements of the City of Santa Rosa Utilities Department, Environmental Compliance Division, for Sewer Use Permits.
66. The sewer main in Morris St east of the existing manhole shall be private, and shall be so noted on the improvement plans.

Miscellaneous

67. The improvement plans shall include detailed landscape construction drawings for work proposed in the public right of way.
68. Any trees planted within 10 feet of a public street curb shall include a root barrier acceptable to the City Engineer and the City Arborist.
69. The improvement plans shall include an onsite signing and striping plan which clearly delineates traffic control and parking restriction requirements.

PRIOR TO CONSTRUCTION, THE FOLLOWING CONDITIONS SHALL APPLY

70. No construction shall be initiated until the Improvement Plans have been approved by the City, all applicable fees have been paid, an encroachment permit and/or grading permit has been issued and a project schedule has been submitted to the City Engineer and a pre-construction conference has been held with the City Engineer or his designee.
71. Developer shall secure encroachment permits from the City and from Caltrans prior to performing any work within the City or State right of way or constructing a City facility within a City easement.
72. Applicant must file a **Notice of Intent To Comply With the Terms of General Permit to Discharge Storm Water Associated with Construction Activity** (NOI) with the State of California Water Resources Control Board, and obtain a permit, prior to commencement of any construction activity.

DURING CONSTRUCTION, THE FOLLOWING CONDITIONS SHALL APPLY:

73. All construction shall conform to the City Standard Details and Specifications dated July, 1998, all City Ordinances and State Map Act and the approved plans.
74. The developer shall complete all water and wastewater improvements, including pressure and bacterial testing and raising manholes and cleanouts to grade prior to connection of any buildings to the City water or wastewater systems.
75. All tree protection fencing must be installed and inspected prior to commencement of grading operations. Fencing shall be maintained throughout the construction period.
76. If any hazardous waste is encountered during the construction of this project, all work shall be immediately stopped and the Sonoma County Environmental Health Department, the Fire Department, the Police Department, and the City Inspector shall be notified immediately. Work shall not proceed until clearance has been issued by all of these agencies.
77. Prior to placing of asphalt, all underground utilities shall be installed and service connections stubbed out behind the sidewalk. Public utilities, Cable TV, sanitary sewers, and water lines, shall be installed in a manner which will not disturb the street pavement, curb, gutter and sidewalk, when future service connections or extensions are made.
78. Prior to placing the final lift of asphalt, all sanitary sewer lines shall be video inspected at the expense of the contractor/developer. All video tapes shall be submitted to the City. If any inadequacies are found, they shall be repaired prior to the placement of the final lift of asphalt.
79. The Contractor shall be responsible to provide erosion and pollution control in accordance with the approved plans and permits.
80. The developer shall keep adjoining public streets free and clean of project dirt, mud, materials, and debris during the construction period, as is found necessary by the City Engineer.
81. Where soil or geologic conditions encountered in grading operations are different from that anticipated in the soil and/or geologic investigation report, or where such conditions warrant changes to the recommendations contained in the original soil investigation, a revised soil or geologic report shall be submitted for approval by the City Engineer. It shall be accompanied by an engineering and geological opinion as to the safety of the site from hazards of land slippage, erosion, settlement, and seismic activity.
82. Hours of work for both public improvements and private improvements shall be limited to the hours of 7 a.m. to 7 p.m. Monday through Saturday. Work on Sunday will only be permitted with written permission from the City. Violation of these working hours shall be deemed an infraction and upon conviction thereof, shall be punishable as prescribed by law.
83. Throughout the construction of the project, dust control shall be maintained to the satisfaction of the City and the contractor shall be responsible to implement reasonable measure to cure any problems that may occur.

84. If the existing public streets are damaged during construction, the contractor/developer shall be responsible for repair at no cost to the City.

85. If, during construction, the contractor damages any existing facilities on the neighboring properties (i.e. fences, gates, landscaping, walls, etc.) contractor shall be responsible to replace all damaged facilities.

PRIOR TO OCCUPANCY, THE FOLLOWING CONDITIONS SHALL BE SATISFIED:

86. Prior to acceptance of improvements or occupancy of building, existing curb, gutter and sidewalk to remain shall be inspected by the Public Works Superintendent. Any curb, gutter and sidewalk which is not in accord with City standards or is damaged before or during construction, shall be replaced.

87. All streets shall be paved, all public utilities installed and all signage relating to traffic control (stop signs, etc.) shall be installed.

88. All improvements shown in the improvement plans for any individual parcel deemed necessary for the health, safety and welfare of the occupant and general public shall be completed prior to occupancy of that parcel.

89. The civil engineer/land surveyor shall file Elevation Certificates for the dwellings in the subdivision.

PRIOR TO ACCEPTANCE OF PUBLIC IMPROVEMENTS, THE FOLLOWING CONDITIONS SHALL BE SATISFIED:

90. Sufficient surety guaranteeing the public improvements for a period of one year shall be provided.

91. A complete set of **As-Built** or Record, improvement plans on the standard size sheets will be certified by the Civil Engineer and returned to the City Engineer's office prior to final acceptance of the public improvement. In addition, the plans shall be submitted on a CD-ROM in pdf format. These plans shall show all constructive changes from the original plans including substantial changes in the size, alignment, grades, etc. during construction, and any existing utilities that were unknown on the original plans but discovered during construction. The Contractor shall pay a fee for having the improvements put into the City Base Map.

Introduction and Waiving of First Reading

*City of Sebastopol
Ordinance No. 1124*

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF SEBASTOPOL
REZONING 6737 SEBASTOPOL AVENUE (APN 004-063-036) FROM DOWNTOWN CORE
TO A PLANNED COMMUNITY (PC) ZONING DISTRICT

THE CITY COUNCIL OF THE CITY OF SEBASTOPOL does hereby ordain as follows:

SECTION 1. The City of Sebastopol City Council has adopted a Mitigated Negative Declaration (MND) for the “Davis Townhomes” Project (the “Project”) located at 6737 Sebastopol Avenue in a separate Resolution based on the findings of fact of the Adopting of Resolution No. 6239-2019.

SECTION 2. The City of Sebastopol City Council finds that the rezoning is consistent with the General Plan and the Municipal Code based on the following facts:

1. The proposal is consistent with Policy LU 1-7 as the site is located at the edge of the Downtown Core District and is surrounded on three sides by existing residential and commercial developments.
2. The proposal is consistent with Policy LU 6-2 as it provides much needed housing located in close proximity to various community services and transit.
3. As Conditioned, the proposal is consistent Policy COS 12-9 as only one tree is slated for removal. Additionally, Condition of Approvals have been added in regards to the 50’ Railroad Forest setback which require this area to be maintained in its natural state, and which requiring buffer plantings / mitigation planting in said 50’ setback.
4. As Conditioned, the proposal is consistent with Policy COS 12-12 as the development is clustered in the center of the project site, with the townhomes arranged in the three clusters with open space located in the center, and around the perimeter of the property. The southern edge of the site will have the largest open space, with a 50’ setback from the Railroad Forest property. A Condition of Approval has been added to require an open space easement on this section of the property which would prevent disturbance of this area with the exception of restoration and removal of invasive species.
5. As Conditioned, the proposal is consistent with Policy CD 1-7 as it is an infill development which provides much needed housing located in close proximity to pedestrian and bicycle trails and has convenient access to local amenities and transit. The project includes the requirement to construct a bicycle/pedestrian connection to the existing connector trail located at the west side of the site.

6. As Conditioned, the proposal is consistent with Policy CD 3-4 as the project was subject to a Cultural Resource Evaluation which found that the site did not contain any known or evident cultural, archeological or historic resources. A Condition of Approval was added to require that, in the event that any unanticipated artifacts or cultural features are discovered during grading or underground excavations all work in the vicinity of the find shall be stopped until the discovery area can be evaluated by an archaeologist and appropriate actions are taken.
7. The proposal is consistent with the City's Housing Element, in that the site is identified as a site in the Site Inventory as land suitable for residential development.
8. The proposal is consistent with the City's Housing Element, in that, as conditioned, the proposal would meet the City's Inclusionary Housing requirements by providing on-site Inclusionary Unit(s) and providing housing opportunities at a variety of income levels.

The project is consistent with the provisions of the Zoning Ordinance, in that:

9. The establishment, maintenance, and operation of the Use applied for, a solely residential development in a commercial zone, will not, under the circumstances of the particular case, be detrimental to the health, safety, peace, morals, comfort, or general welfare of persons residing or working in the area of such use or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the City; due to the following reasons:
 - a. The Zoning Ordinance allows for a solely-residential development on a commercially zoned parcel, provided it does not detract from the intent of the commercial spaces on the street. The property is located behind existing commercial and the residential development to the northeast and east. No commercial frontages are shared with this property or would be disrupted by the use of this site as solely residential.
 - b. The project site is unique as it is located on the edge of an urbanized area and is considered part of the City's Downtown but abuts an environmentally sensitive habitat.
 - c. The project will result in the removal of 1 existing tree and result in the planting of an additional 50+ trees, as currently proposed.
 - d. The proposed site is located within walking distance of existing commercial, recreational uses and will provide additional housing opportunities for locals.
 - e. It is not well suited for commercial development but does appear to be an appropriate location for much needed housing.
 - f. The proposed development is appropriate for the site.
10. With approval of a Use Permit, and adoption of an Ordinance to rezone the parcel from Downtown Core to a Planned Community zoning district, the project will be consistent with the subject property's General Plan Designation of Central Core and Zoning Designation of Downtown Core. The Project is consistent with the requirements of a Planned Community zoning district in that:
 - a. The Project provides appropriate buffering, including fencing, landscaping and open space, between the project and the surrounding areas.
 - b. The parcel meets the minimum 12,000 square foot lot size.

- c. The Project provides amenities on site to include landscaping and parking amenities for the residential units.
- d. The Project provides more than 10 percent of the gross site area in open space and community open space areas for passive or active recreation.
- e. With the approval of an Ordinance for a Planned Community the reduced FAR and lot sizes will be consistent with the Zoning Ordinance.
- f. The Development Standards for the project are as laid out in the following tables and in the application materials in the Planning File 2018-82.

Allowed Uses Table:

Allowed Uses	Downtown Core Requirements	Planned Community Proposed
Permanent residential uses that are allowed in the R7 District when not part of a mixed-use development	Conditional Use Permit	Conditional Use Permit
Nonresidential uses except for office uses	35,000 sf	Home office uses, subject to SMC 17.210
Common Area Accessory buildings	-	Conditional Use Permit
Residential Accessory buildings	-	Permitted, subject to 17.08.030, but limited 80 sq. ft. in size

Development Standards Table:

Development Standards	Downtown Core Requirements	Planned Community Proposed
Minimum lot area	6,000	1,260 – 1,801 sq. ft.
Maximum building height: Buildings and other facilities	40 ft., 3 stories/ 50 ft. 4 stories (1)	2 stories, and 40 ft. However per plans ~30 ft. (total may vary due to flood plain restrictions).
Front yard	0 ft.	5 ft.
Interior side yard (except for structures sharing a common wall)	0 ft.	5 ft. ¹
Rear yard, Main building	0 ft.	5 ft.
Rear yard, Main building when abutting residential district	20 ft.	5 ft.
Maximum residential density	1 DU/1,743 SF lot area (CD) 1 DU/ 1,000 SF lot area (RM-H)	18 residential units on 75,794.4 SF lot = 1 DU/4,210.8 SF lot
Maximum Floor Area Ratio (FAR), not including residential except for purposes of calculating minimum FAR	Minimum (new buildings); 1.0 Maximum; 2.5	Net FAR 0.39 FAR ²
Minimum residential density	1 DU/ 3,600 SF lot area	Net Density: 18 residential units on 54,103 SF lot = 1 DU/3,005.72 SF lot

Minimum usable open space	50 sq. ft. per DU	280 sq. ft. per DU in rear yard + front and rear porches and common area
Parking - Residential	2 parking spaces per 2 or 3 bedroom units. 18 units x 2 = 36	2 spaces per unit (36 spaces), one of which is in a carport, and 2 visitor spaces. 38 spaces total (8 spaces to be provided for Electric Vehicles ³)
Bicycle Parking Spaces	20% required vehicle parking requirement. 36 x 0.20 = 8 bicycle parking spaces.	Hook in rear closet space for a bike for each unit, 18 bike spaces.

¹ Lot 18 which has an angled rear property line: angled lot line shall be considered a side yard.

² No additions to habitable space shall be permitted.

³ Vehicle parking to comply with Ordinance 1111 (Section 17.110.040 Electrical Vehicles).

SECTION 3. The City of Sebastopol City Council hereby modifies the Zoning Map of the City of Sebastopol to re-zone 6737 Sebastopol Avenue (APN 004-063-036) from “Downtown Core” to the “Planned Community (PC)” Zoning designation and subject to the Allowed Uses and Development Standards as described above.

APPROVED FOR FIRST READING, WAIVING OF FURTHER READING AND INTRODUCTION by the City Council of the City of Sebastopol, California, at a regular meeting of the City Council held on this 7th day of May, 2019.

SCHEDULED FOR SECOND READING, WAIVING OF FURTHER READING, AND ADOPTION OF ORDINANCE on the 21st day of May 2019.

VOTE:

Ayes: Councilmembers Carnacchi, Glass, Gurney and Vice Mayor Slayter
 Noes: None
 Abstain: None
 Absent: Mayor Hinton

APPROVED: _____
 Vice Mayor Patrick Slayter

ATTEST:

 Mary Gourley, Assistant City Manager/City Clerk, MMC

APPROVED AS TO FORM: _____

Larry McLaughlin, City Attorney

LEGEND / ABBREVIATIONS

EXISTING FEATURES SHOWN 50% SCREENED	
	BOUNDARY LINE
	CONTOUR LINE
	FENCE LINE (TYPE VARIES)
	EDGE OF PAVEMENT
	TREE DRIPLINE
	BUILDING LINE
	STORM DRAIN
	6" SANITARY SWER
	SS LATERAL
	GRASS SWALE
	WATERLINE
	PERVIOUS PAVEMENT
	NEW CONCRETE
AC	ASPHALTIC CONCRETE
INV	INVERT
WM	WATER METER
SSCO	SANITARY SEWER CLEAN-OUT
CONC	CONCRETE
JP	JOINT SERVICE POLE
IP	IRON PIPE MONUMENT
EL	ELEVATION
	FIRE HYDRANT
TC	TOP OF CURB
TP	TOP OF PAVEMENT
EP	EDGE OF PAVEMENT
EG	EXISTING GRADE
FG	FINISHED GRADE
FL	FLOW LINE
INV	INVERT
(E)	EXISTING
C	CONCRETE
B.R.A.	BIO RETENTION AREA
	DRAINAGE FLOW
SSMH	SANITARY SEWER MANHOLE
DI	DROP INLET
TOG	TOP OF GRATE

TREE ABBREVIATIONS

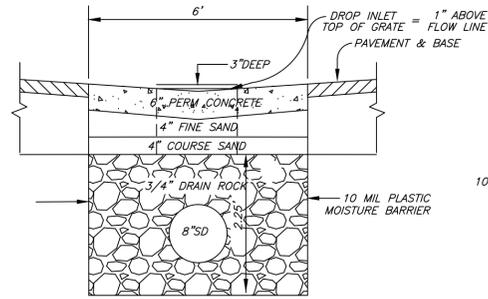
30" LVOK/40DL	30" DIAMETER LIVE OAK WITH 40' DIAMETER DRIPLINE
LVOK	LIVE OAK
WTOK	WHITE OAK
MAPL	MAPLE
WIL0	WILLOW

NOTES

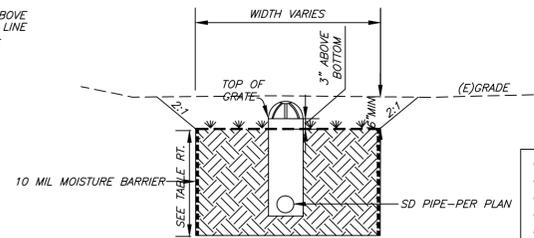
BOUNDARY LINES / LOT LINES SHOWN ON THIS MAP DERIVED FROM FOUND MONUMENTS AND RECORD DATA

BENCH MARK
SURVEY CONTROL POINT #5 3/8" REBAR TAGGED "PLS 5520"
ELEVATION=76.78 NAVD 88 (BY OPUS SOLUTION)

PAVEMENT TO MATCH (E)GRADE TO EAST FENCE TO BE REMOVED TO PROVIDE INGRESS & EGRESS TO APN 004-063-017



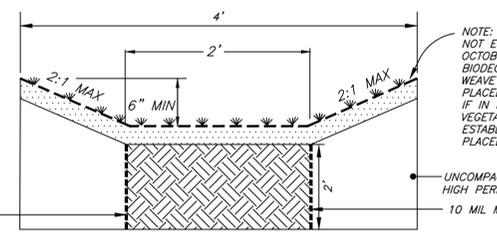
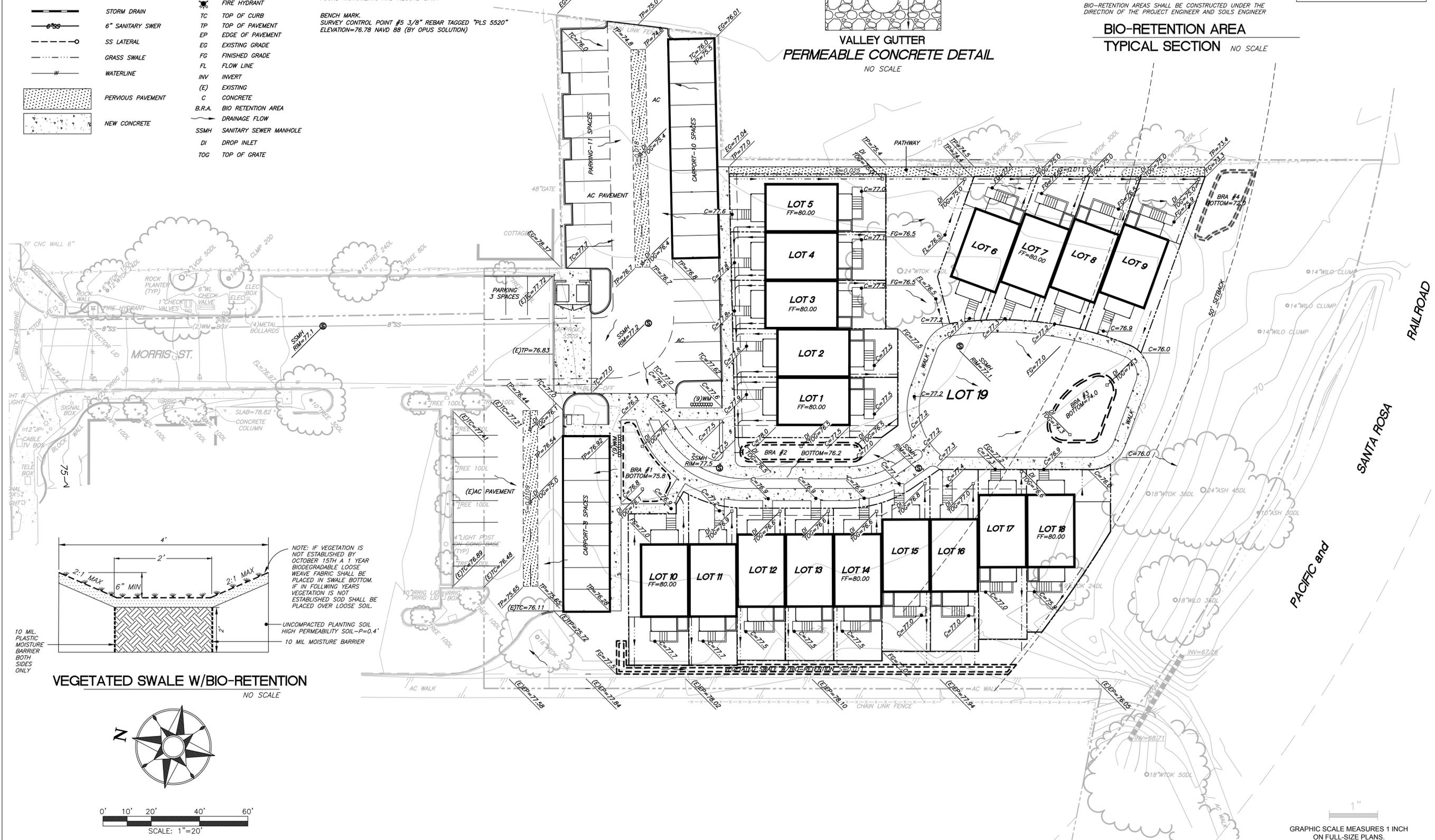
VALLEY GUTTER PERMEABLE CONCRETE DETAIL
NO SCALE



BIO-RETENTION AREA TYPICAL SECTION
NO SCALE

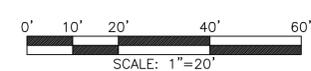
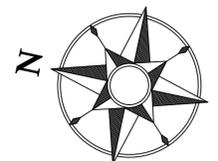
SOIL DEPTH TABLE

BMP ID.	SOIL DEPTH	SF
BRA#1	2.0'	400
BRA#2	2.0'	284
BRA#3	1.5'	450
BRA#4	2.0'	300

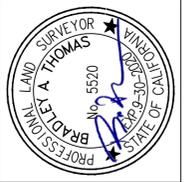


VEGETATED SWALE W/BIO-RETENTION
NO SCALE

NOTE: IF VEGETATION IS NOT ESTABLISHED BY OCTOBER 15TH A 1 YEAR BIODEGRADABLE LOOSE WEAVE FABRIC SHALL BE PLACED IN SWALE BOTTOM. IF IN FOLLOWING YEARS VEGETATION IS NOT ESTABLISHED SOD SHALL BE PLACED OVER LOOSE SOIL.



GRAPHIC SCALE MEASURES 1 INCH ON FULL-SIZE PLANS.



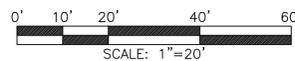
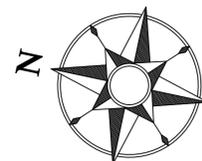
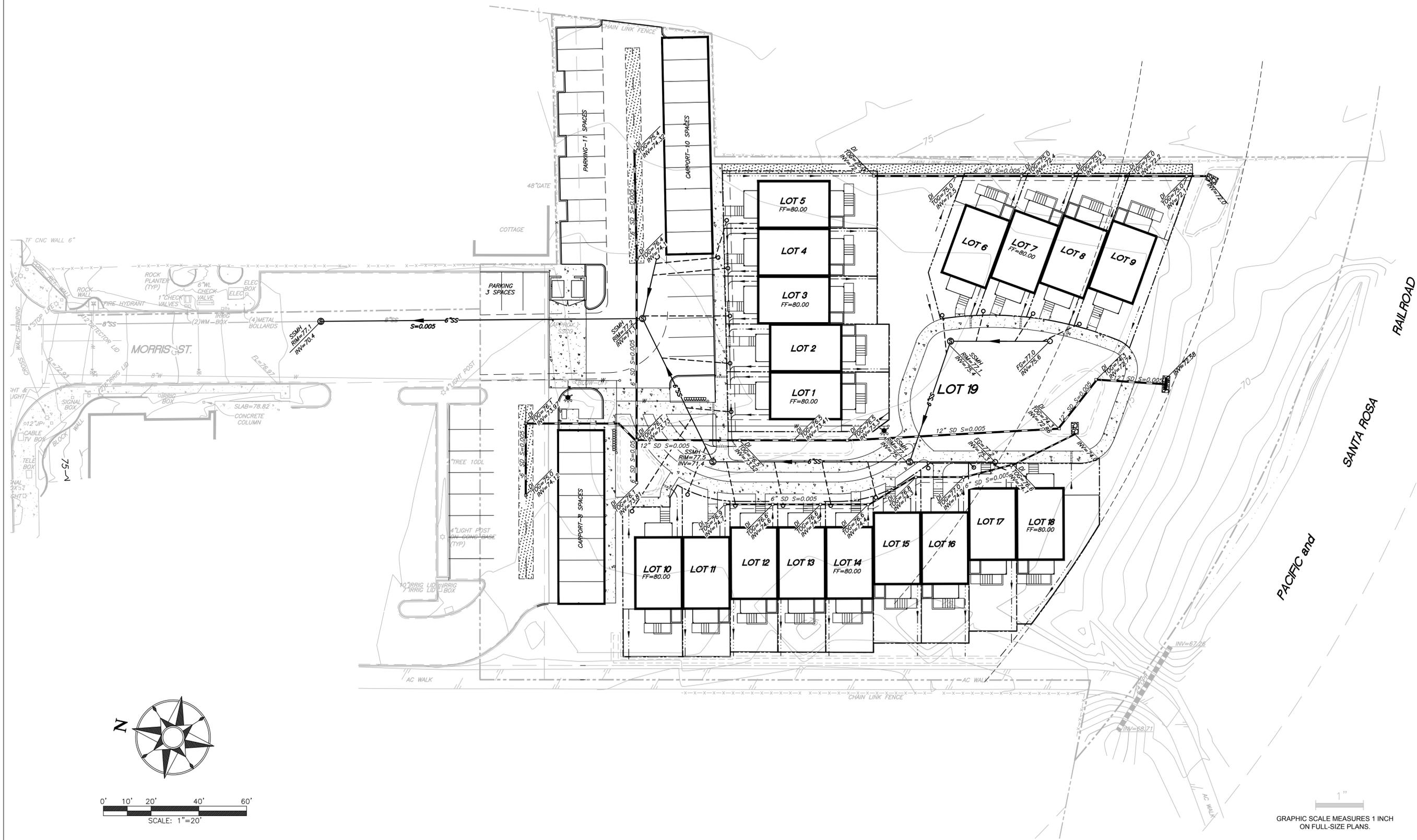
LACO
EUREKA • UKIAH • SANTA ROSA
1-800-515-5054 www.lacoassociates.com

NO.	HISTORY / REVISION	BY	CHK.	DATE

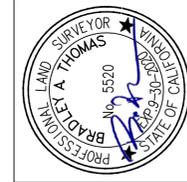
DAVIS TOWNHOMES
A PLANNED COMMUNITY
GRADING, DRAINAGE and STORM WATER TREATMENT

DANIEL DAVIS
1051 TODD ROAD, SANTA ROSA, CA. 95407

DRAWN	GG
CHECK	KD
APPROVED	BAT
DATE	12-18-2018
JOB NUMBER	9054.00
SHEET	2 OF 3



1"
GRAPHIC SCALE MEASURES 1 INCH
ON FULL-SIZE PLANS.



LACO
EUREKA • UKIAH • SANTA ROSA
1-800-515-5054 www.lacoassociates.com

NO.	HISTORY / REVISION	BY / CHK.	DATE

DAVIS TOWNHOMES
A PLANNED COMMUNITY
UTILITY PLAN
DANIEL DAVIS
1051 TODD ROAD, SANTA ROSA, CA. 95407

DRAWN	GG
CHECK	KD
APPROVED	BAT
DATE	12-18-2018
JOB NUMBER	9054.00
SHEET	



Lot 1 Lot 2 Lot 3 Lot 4 Lot 5

REAR ELEVATIONS



Lot 5 Lot 4 Lot 3 Lot 2 Lot 1

FRONT ELEVATIONS



Side Elevation Lot 5



Side Elevation Lot 1



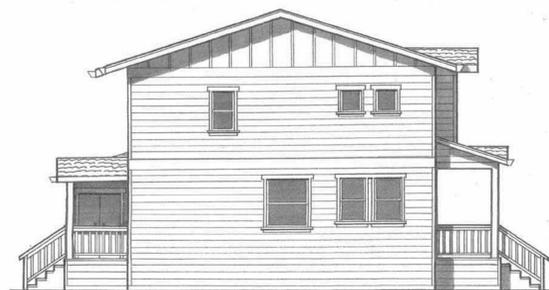
Lot 6 Lot 7 Lot 8 Lot 9

REAR ELEVATIONS



Lot 9 Lot 8 Lot 7 Lot 6

FRONT ELEVATIONS



Side Elevation Lot 9



Side Elevation Lot 6



REVISIONS	BY

Katherine Austin, AIA
 179 SE Rice Way 524 So. Main Street
 Bend, OR 97702 Sebastopol, CA 95472
 (707) 529-5565
 kaustin@pacbell.net

DAVIS TOWN HOMES
 Morris St. Sebastopol, CA

Lots 10-18
 Rendered Elevations
 Front, Rear and Sides

Date 1.30.18
 Scale 1/8" = 1'-0"
 Drawn KA
 Job
 Sheet **A2.0**
 Of Sheets



Lot 18 Lot 17 Lot 16 Lot 15 Lot 14 Lot 13 Lot 12 Lot 11 Lot 10

FRONT ELEVATIONS



Side Elevation Lot 18



Side Elevation Lot 10



Lot 10 Lot 11 Lot 12 Lot 13 Lot 14 Lot 15 Lot 16 Lot 17 Lot 18

REAR ELEVATIONS



REVISIONS	BY

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 Bend, OR 97702
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 kaustin@pacbell.net

DAVIS TOWN HOMES
 Morris St. Sebastopol, CA

Lots 1-5 and 6-9
 Rendered Elevations
 Front, Rear and Sides

Date 1.30.18

Scale 1/8" = 1'-0"

Drawn KA

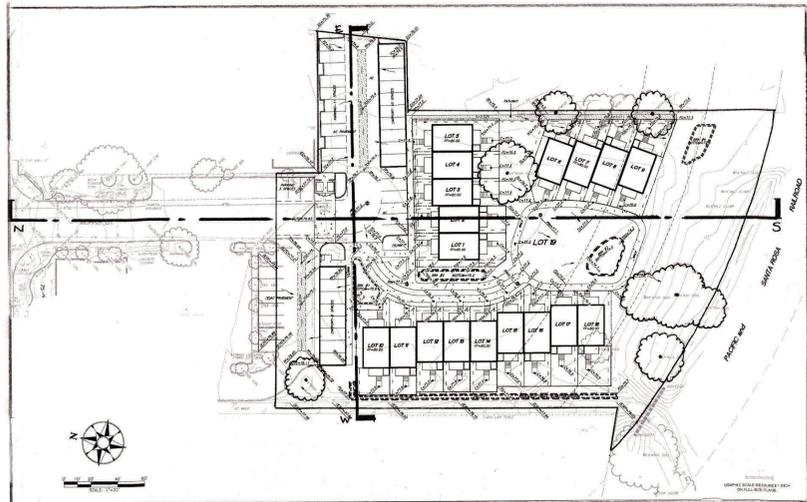
Job

Sheet

A2.1

Of Sheets

REVISIONS	BY



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 (707) 529-5565 kaustin@pacbell.net

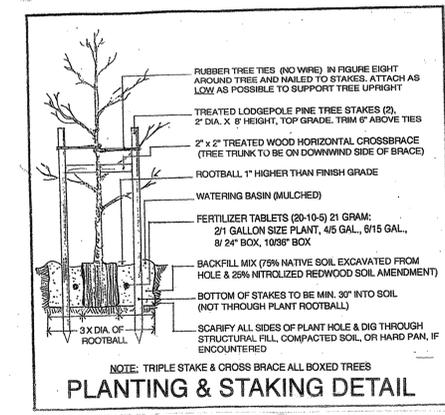
DAVIS TOWN HOMES
 Morris St. Sebastopol, CA

Cross Site Sections
 North South & East West

Date	9.30.2018
Scale	1" = 20'
Drawn	KA
Job	
Sheet	A2.2
Of	Sheets

PLANT LEGEND

ABBR.	PLANT SPECIES (Botanical & Common Names)	NO.	SIZE	NOTES	WATER USE
TREES					
AP	<i>Acer platanoides</i> 'Crimson Sentry' Upright Redleaf Norway Maple	11	15 Gal.	Standard	Moderate
BN	<i>Betula nigra</i> 'Heritage' Heritage River Birch	6	15 Gal.	Natural Form	High
CC	<i>Cinnamomum camphora</i> Camphor	3	24" Box	Natural Form	Moderate
LH	<i>Lagerstroemia hybrid</i> 'Tuscarora' Hybrid Crape Myrtle	2	24" Box	Standard	Low
MG	<i>Magnolia grandiflora</i> 'Russell' or 'Little Gem' Southern Magnolia	2	24" Box	Standard	Moderate
PC	<i>Pistacia chinensis</i> 'Keith Davey' Male Chinese Pistache	3	24" Box	Standard	Low
QA	<i>Prunus caroliniana</i> 'Thundercloud' Purpleleaf Flowering Plum	9	24" Box	Standard	Low
PT	<i>Quercus agrifolia</i> Coast Live Oak	7	24" Box	Natural Form	Very Low
TL	<i>Tristania laurina</i> Littletree Myrtle	12	15 Gal.	Standard	Moderate
SHRUBS, PERENNIALS					
BD	<i>Buddleia 'Dwarf'</i> Dwarf Butterfly Bush	11"	15 Gal.		Low
CJ	<i>Ceanothus Julia Phelps</i> CA Lilac	3	5 Gal.		Low
CN	<i>Cornelia sasanqua</i> 'Yuletide' Yuletide Camellia	9	5 Gal.		Moderate
CS	<i>Cercis occidentalis</i> Spreading Camellia	3	5 Gal.		Moderate
CO	<i>Cercis occidentalis</i> Cercis occidentalis	6	15 Gal.		Low
CP	Western Redbud <i>Calceolola pulchella</i>	4	5 Gal.		Moderate
DB	<i>Diospyros bicolor</i> Pink Breath of Heaven	31	5 Gal.		Low
DV	<i>Dodonaea viscosa</i> 'Purpurea' Yellow Fartree Lily	3	15 Gal.	Shrub Form	Low
HB	Purple Hopseed Hebe 'Coed'	10	5 Gal.		Moderate
HS	Coed Hebe Hydrangea 'Shooting Stars'	8	5 Gal.		Moderate
JC	Shooting Stars Hydrangea Juniperus chinensis 'Skyrocket'	5	5 Gal.		Low
LC	Skyrocket Juniper Lorocostium chinensis 'Sizzling Pink' or 'Razzeberry'	12	5 Gal.		Low
MS	Purpleleaf Chinese Fringe Bush Miscanthus sinensis 'Cabaret'	4	5 Gal.		Moderate
ND	Caravel Chinese Silver Grass Nandina domestica 'Fire Power'	13	5 Gal.		Low
OG	Fire Power Heavenly Bamboo Oemanshus heterophyllus 'Goshiki'	7	5 Gal.		Moderate
PG	Goshiki False Holly Podocarpus elongatus 'Ice Blue'	4	15 Gal.		Moderate
PL	Blue Yellow Wood Plumbago capensis 'Imperial Blue' or 'Royal Blue'	9	5 Gal.		Low
PV	Cape Plumbago Variety Pittosporum tobira 'Variegata'	15	5 Gal.		Low
RI	Cape Plumbago Variety Rhinodaphne umbellata 'Minor'	14	5 Gal.		Low
RR	Compact Indian Hawthorn Rosa Flower Carpet 'Single Red' or 'Scarlet'	6	5 Gal.		Moderate
RW	Red Flower Carpet Rose (no other varieties) Rosa Floribunda 'Iceberg'	5	5 Gal.		Moderate
SH	White Iceberg Rose Sotya heterophyllus	16	5 Gal.		Low
SG	Australian Blue Star Creeper Salvia greggii 'Hot Lips'	11	5 Gal.	Avail. at Cal Flora Nur.	Low
WW	Hot Lips Salvia Westringia fruticosa 'Morning Light'	9	5 Gal.		Low
VINES					
CR	<i>Campsis radicans</i> Trumpet Creeper	4	5 Gal.	Train on Fence	Low
HH	<i>Hedera Helix</i> 'Necrolepoint' or 'Hahn's' English Ivy	2	5 Gal.	Train on Wall	Moderate
HV	<i>Hardenbergia violacea</i> 'Happy Wanderer' Happy Wanderer Vine	8	5 Gal.	Train on Fence	Moderate
JN	Jasmine Jasminum nitidum	7	5 Gal.	Train on Fence	Low
GROUND COVERS					
	<i>Arcostaphylos uva-ursi</i> 'Point Reyes' Point Reyes Manzanita	As Req'd.	1 Gal. @ 30" c.c.		Low
	<i>Carex divisa</i> (Turf/Grass)	As Req'd.	4" Pots @ 18" c.c.		Low
	Berkeley Sedge <i>Colymbastis diandra</i> 'Coral Beauty'	As Req'd.	1 Gal. @ 30" c.c.		Low
	Spreading Cotoneaster <i>Erigeron kanvinskianus</i>	As Req'd.	1 Gal. @ 24" c.c.		Low
	Santa Barbara Daisy <i>Festuca rubra</i> 'No Mow'	Solid	Sod		Low
	No Mow Creeping Red Fescue <i>Kniphofia 'Lips Masc.'</i> 'Vanilla' OR 'Ember Glow'	As Req'd.	4" Pots @ 18" c.c. (3 Rows)		Low
	Dwarf Hybrid Red Hot Pokers <i>Lantana montevidensis</i>	As Req'd.	1 Gal. @ 24" c.c.		Low
	Purple Lantana <i>Liriodendron 'Silver Dragon'</i>	As Req'd.	4" Pots @ 12" c.c.		Moderate
	Silver Dragon Lily Turf <i>Lavandula angustifolia</i> 'Hidcot'	As Req'd.	4" Pots @ 18" c.c. (3 Rows)		High
	Hidcot Lavender (low growing) Lawn	Solid	Sod		High
	Dwarf Tall Fescue (Named Form) & Perennial Rye Mix	As Req'd.	1 Gal. @ 30" c.c.		Low
	Muhlenbergia capillaris 'Regal Mist' Pink Muffy Grass	As Req'd.	1 Gal. @ 18" c.c.		Moderate
	<i>Trachypogonum jasmimoides</i> Star Jasmine	As Req'd.	1 Gal. @ 18" c.c.		Low
	<i>Coreopsis auriculata</i> 'Sierra Sunser' Sierra Sunset G.C. Daisy	As Req'd.	4" Pots @ 18" c.c. - Avail. Emerissa Nursery		Low

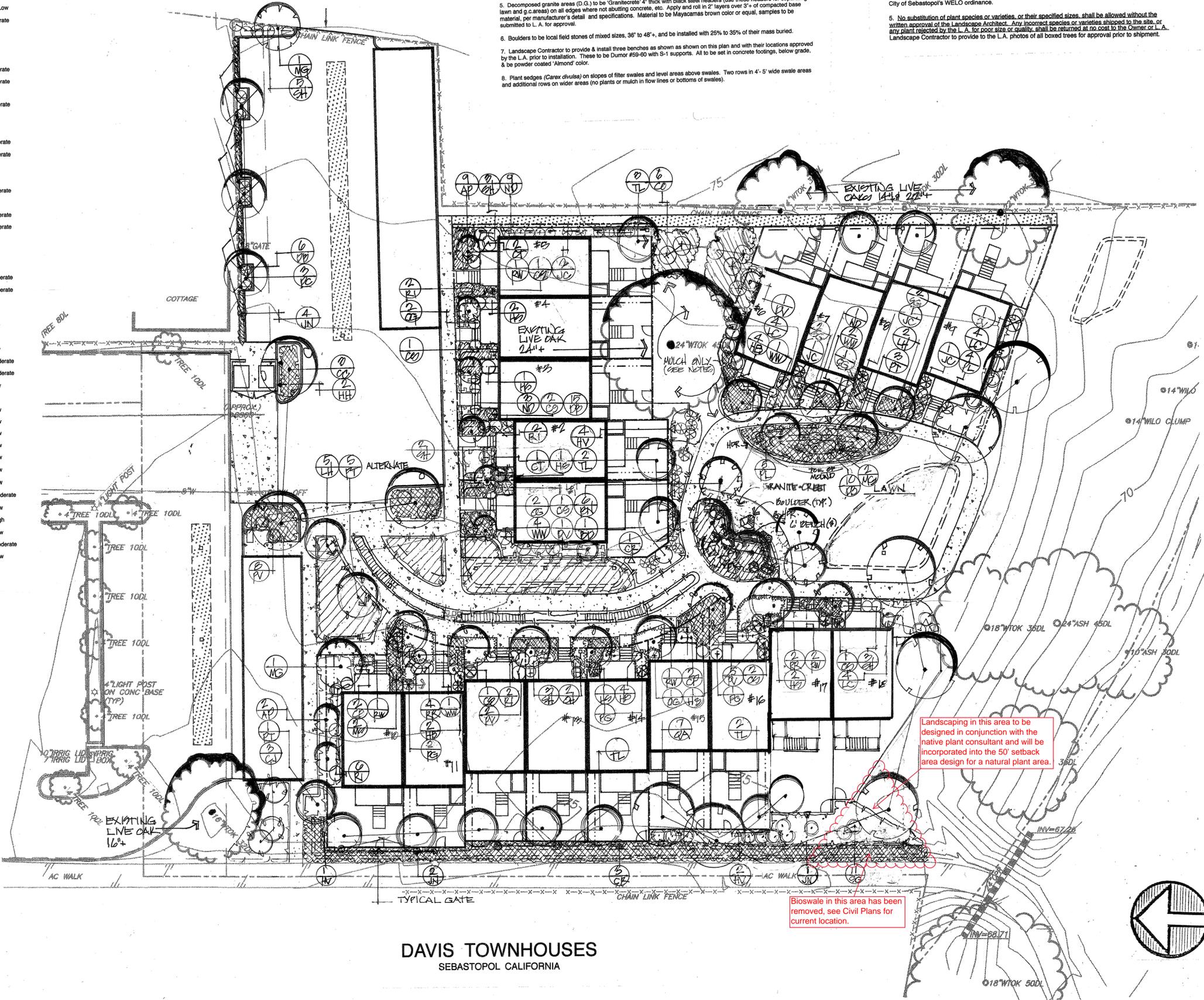


PLANTING NOTES

- All new trees planted within 6' of a sidewalk, curb, paving, etc. shall have root barriers (18" deep) installed along the solid object for 5' in each direction centered on the tree's trunk.
- Plant quantities and square footages are approximate only & Landscape Contractor is responsible to provide & install all plantings as on shown.
- Five, 6' long wires to radiate out on fences from base of vines secured to metal eyes screwed into fence boards. Tie vines onto wires.
- Land Contractor to provide photos of all boxed trees for approval by Landscape Architect prior to ordering. All plants shall be inspected by the L.A. prior to any planting operations and any not acceptable shall be returned at no cost to the Owner or L.A.
- Decomposed granite areas (D.G.) to be 'Granitecrete' 4" thick with black steel headers (use these headers for separating lawn and g.c. areas) on all edges where not abutting concrete, etc. Apply and roll in 2" layers over 3" of compacted base material, per manufacturer's detail and specifications. Material to be Mayscamas brown color or equal, samples to be submitted to L.A. for approval.
- Boulders to be local field stones of mixed sizes, 36" to 48"+, and be installed with 25% to 35% of their mass buried.
- Landscape Contractor to provide & install three benches as shown on this plan and with their locations approved by the L.A. prior to installation. These to be Duroc #69-60 with S-1 supports. All to be set in concrete footings, below grade, & be powder coated 'Almond' color.
- Plant edges (*Carex divisa*) on slopes of filter swales and level areas above swales. Two rows in 4'-5' wide swale areas and additional rows on wider areas (no plants or mulch in low lines or bottoms of swales).

GENERAL LANDSCAPE NOTES

- A minimum of 8" non-mechanically compacted topsoil shall be available for water absorption and root growth in all planted areas.
- A minimum 3" layer of mulch (see Planting Specifications) shall be applied on all exposed soil surfaces of planting areas except in NoMow grass areas.
- Upon completion of the installation & all requirements of the WELO ordinance have been met, the contractor shall submit to the Engineering Development Services Inspector a completed and signed WELO Schedule A, 'Certificate of Completion' signed by both Landscape Architect & Landscape Contractor.
- The Certificate of Completion shall be accompanied by an irrigation audit (by an independent Irrigation Auditor), an irrigation schedule and a maintenance schedule, provided by the Landscape Contractor, as described in the City of Sebastopol's WELO ordinance.
- No substitution of plant species or varieties, or their specified sizes, shall be allowed without the written approval of the Landscape Architect. Any incorrect species or varieties shipped to the site, or any plant rejected by the L.A. for poor size or quality, shall be returned at no cost to the Owner or L.A. Landscape Contractor to provide to the L.A. photos of all boxed trees for approval prior to shipment.



Bioswale in this area has been removed, see Civil Plans for current location.

DAVIS TOWNHOUSES
SEBASTOPOL CALIFORNIA

INITIAL DRAWING DATE:	12-2-18
APPROVED BY:	
DESCRIPTION:	
DATE:	
NO.:	

PREPARED FOR:
DANIEL DAVIS
1051 TODD ROAD
SANTA ROSA, CA. 95407
707 975 0794

PROJECT DESCRIPTION:
DAVIS TOWNHOUSES
SEBASTOPOL CALIFORNIA
ATN 00-003-006

SHEET DESCRIPTION:
LANDSCAPE PLANTING PLAN

DESIGNED BY:
PARKER SMITH
LANDSCAPE ARCHITECTURE
CALICENSED LANDSCAPE ARCHITECT #1284

1945 PINER ROAD #25, SANTA ROSA, CA 95403 (707) 477-7502

SCALE: 1/8" = 1'

DRAWN BY: RS

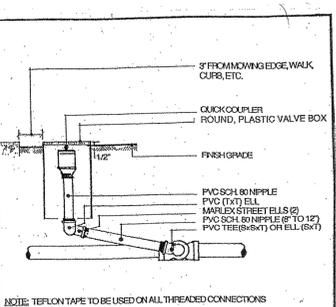
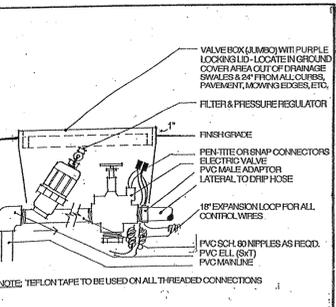
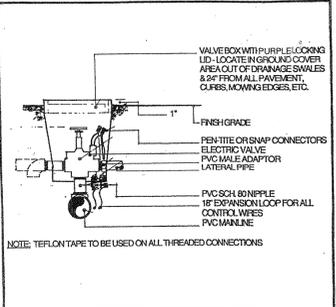
CHECKED BY: RS

FILE No.: 17/19

SHEET No. 3 OF 3

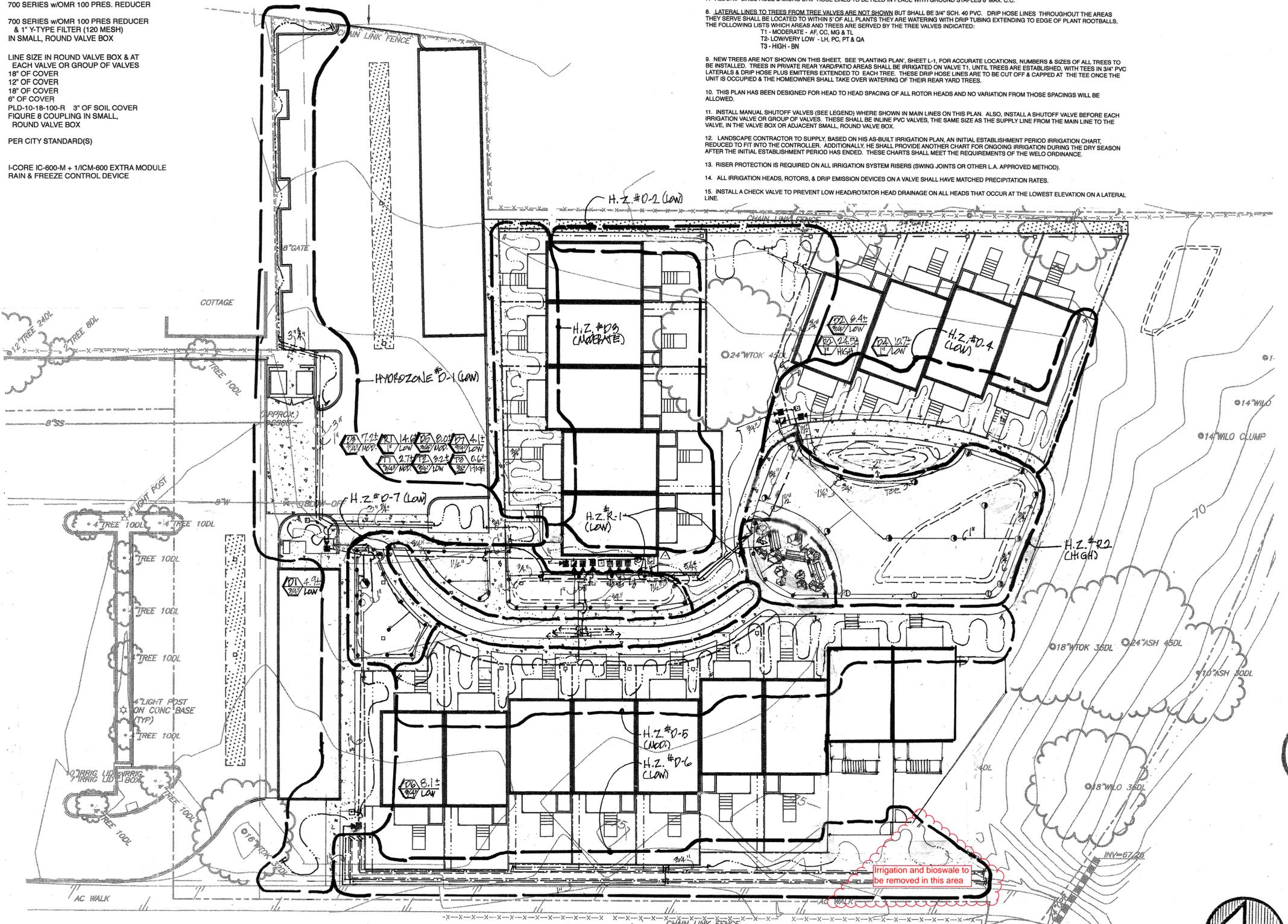
IRRIGATION LEGEND

SYMBOL	EQUIPMENT	MANUF'R.	MODEL NO.(S)
●	POP-UP HEAD / MP ROTATOR	HUNTER	PROS-06 & MP1000/ADJ. ARC & 8'-15" RADIUS NOZZLES
○	POP-UP HEAD / MP ROTATOR	HUNTER	PROS-06 & MP2000w/ADJ. ARC & 18" RADIUS NOZZLE
⊙	POP-UP HEAD / MP ROTATOR	HUNTER	PROS-06 & MP3000 w/ADJ. ARC & 30" RADIUS NOZZLE
⊠	ROTATOR HEAD CONTROL VALVES	IRRITROL or HUNTER	700 SERIES w/OMR 100 PRES. REDUCER
⊡	DRIP CONTROL VALVES	IRRITROL or HUNTER	700 SERIES w/OMR 100 PRES REDUCER & 1" Y-TYPE FILTER (120 MESH)
⊞	QUICK COUPLER VALVE 3/4"	HUNTER OR EQ.	IN SMALL, ROUND VALVE BOX
●	MANUAL SHUT-OFF BALL VALVE	PVC	LINE SIZE IN ROUND VALVE BOX & AT EACH VALVE OR GROUP OF VALVES
---	MAIN LINE	PVC SCH. 40	18" OF COVER
---	LATERAL LINE	PVC CL. 200	12" OF COVER
---	SLEEVE	PVC SCH. 40	18" OF COVER
---	DRIP HOSE LINE	5/8" POLYHOSE	6" OF COVER
---	MICRO DRIP HOSE - 18" SPACING	HUNTER	PLD-10-18-100-R 3" OF SOIL COVER
---	DRIP HOSE END		FIGURE 8 COUPLING IN SMALL, ROUND VALVE BOX
⬢	P.O.C. SUBMETER, HI-FLOW SENSOR, RED. PRES. BACKFLOW DEVICE - 1 1/2"		PER CITY STANDARD(S)
⬢	CONTROLLER, 12 STATIONS SOLAR SYNC - WIRELESS	HUNTER	I-CORE IC-600-M + 1/CM-600 EXTRA MODULE
⬢	CONTROLLER STATION / G.P.M. WATER USE PLANT TYPE	HUNTER	RAIN & FREEZE CONTROL DEVICE



IRRIGATION NOTES

- IRRIGATION SYSTEM IS DESIGNED TO OPERATE ON 55 TO 95 P.S.I. AT POINT OF CONNECTION. MAXIMUM G.P.M. IS 25± PER CIRCUIT. ADJUST VALVES PRESSURE REGULATORS TO PROVIDE OPTIMUM OPERATING PRESSURE AT THE ROTATOR HEADS (40 psi) AND AT THE DRIP EMITTERS (20 psi), PER MANUFACTURER'S RECOMMENDATIONS.
- INSTALL WIRELESS WEATHER STATION ON FASCIA OF BUILDING & ADJUST SENSOR TO TURN OFF SYSTEM IN MODERATE TO HEAVY RAINFALL. NOT LIGHT RAINFALL. PAINT ABOVE GROUND COMPONENTS TO MATCH COLOR OF BUILDING.
- SEE CIVIL ENGINEER'S PLANS FOR SPECIFIC LOCATIONS OF UNDERGROUND UTILITY, STORM DRAIN LINES, ETC. THAT OCCUR WITHIN LANDSCAPE AREAS & CONFIRM THE DEPTH OF THESE WITH GENERAL CONTRACTOR PRIOR TO TRENCHING. ALL TRENCHING LIMITED TO 6" DEPTH IN DRIPLINE OF EXISTING OAK TREE. PAINT CONTROLS & ABOVE GRADE CONDUITS TO IT THE SAME COLOR AS BUILDING.
- PIPES SHOWN IN PAVED AREAS TO BE LOCATED IN ADJACENT LANDSCAPE AREAS IN JOINT TRENCHES.
- SLEEVES ARE SIZED FOR LINES SHOWN PLUS LATERALS TO IRRIGATE TREES ON THEIR SEPARATE VALVE.
- ALL DRIP LINES HOSE & MICRO DRIP HOSE LINES TO BE HELD IN PLACE WITH GROUND STAPLES @ MAX. C.C.
- LATERAL LINES TO TREES FROM TREE VALVES ARE NOT SHOWN BUT SHALL BE 3/4" SCH. 40 PVC. DRIP HOSE LINES THROUGHOUT THE AREAS THEY SERVE SHALL BE LOCATED TO WITHIN 5' OF ALL PLANTS THEY ARE WATERING WITH DRIP TUBING EXTENDING TO EDGE OF PLANT ROOTBALLS. THE FOLLOWING LISTS WHICH AREAS AND TREES ARE SERVED BY THE TREE VALVES INDICATED:
T1 - MODERATE - AF, CC, MG & TL
T2 - LOW/VERY LOW - LH, PC, PT & QA
T3 - HIGH - BN
- NEW TREES ARE NOT SHOWN ON THIS SHEET. SEE 'PLANTING PLAN', SHEET L-1, FOR ACCURATE LOCATIONS, NUMBERS & SIZES OF ALL TREES TO BE INSTALLED. TREES IN PRIVATE REAR YARD/PATIO AREAS SHALL BE IRRIGATED ON VALVE T1, UNTIL TREES ARE ESTABLISHED. WITH TREES IN 3/4" PVC LATERALS & DRIP HOSE PLUS EMITTERS EXTENDED TO EACH TREE. THESE DRIP HOSE LINES ARE TO BE CUT OFF & CAPPED AT THE TREE ONCE THE UNIT IS OCCUPIED & THE HOMEOWNER SHALL TAKE OVER WATERING OF THEIR REAR YARD TREES.
- THIS PLAN HAS BEEN DESIGNED FOR HEAD TO HEAD SPACING OF ALL ROTOR HEADS AND NO VARIATION FROM THOSE SPACINGS WILL BE ALLOWED.
- INSTALL MANUAL SHUTOFF VALVES (SEE LEGEND) WHERE SHOWN IN MAIN LINES ON THIS PLAN. ALSO, INSTALL A SHUTOFF VALVE BEFORE EACH IRRIGATION VALVE OR GROUP OF VALVES. THESE SHALL BE IN LINE PVC VALVES, THE SAME SIZE AS THE SUPPLY LINE FROM THE MAIN LINE TO THE VALVE, IN THE VALVE BOX OR ADJACENT SMALL, ROUND VALVE BOX.
- LANDSCAPE CONTRACTOR TO SUPPLY, BASED ON HIS AS-BUILT IRRIGATION PLAN, AN INITIAL ESTABLISHMENT PERIOD IRRIGATION CHART, REDUCED TO FIT INTO THE CONTROLLER. ADDITIONALLY, HE SHALL PROVIDE ANOTHER CHART FOR ONGOING IRRIGATION DURING THE DRY SEASON AFTER THE INITIAL ESTABLISHMENT PERIOD HAS ENDED. THESE CHARTS SHALL MEET THE REQUIREMENTS OF THE WELO ORDINANCE.
- RISER PROTECTION IS REQUIRED ON ALL IRRIGATION SYSTEM RISERS (SWING JOINTS OR OTHER L.A. APPROVED METHOD).
- ALL IRRIGATION HEADS, ROTORS, & DRIP EMISSION DEVICES ON A VALVE SHALL HAVE MATCHED PRECIPITATION RATES.
- INSTALL A CHECK VALVE TO PREVENT LOW HEAD/ROTATOR HEAD DRAINAGE ON ALL HEADS THAT OCCUR AT THE LOWEST ELEVATION ON A LATERAL LINE.



INITIAL DRAWING DATE:	12-2-18
APPROVED BY:	
DESCRIPTION:	
DATE:	
NO.:	

PREPARED FOR:

DANIEL DAVIS
1051 TODD ROAD
SANTA ROSA, CA. 95407
707 975 0794

PROJECT DESCRIPTION:

DAVIS TOWNHOUSES
SEBASTOPOL CALIFORNIA
APN 004-063-036

SHEET DESCRIPTION:

LANDSCAPE IRRIGATION PLAN

SCALE: 1/16" = 1'

DRAWN BY: PS

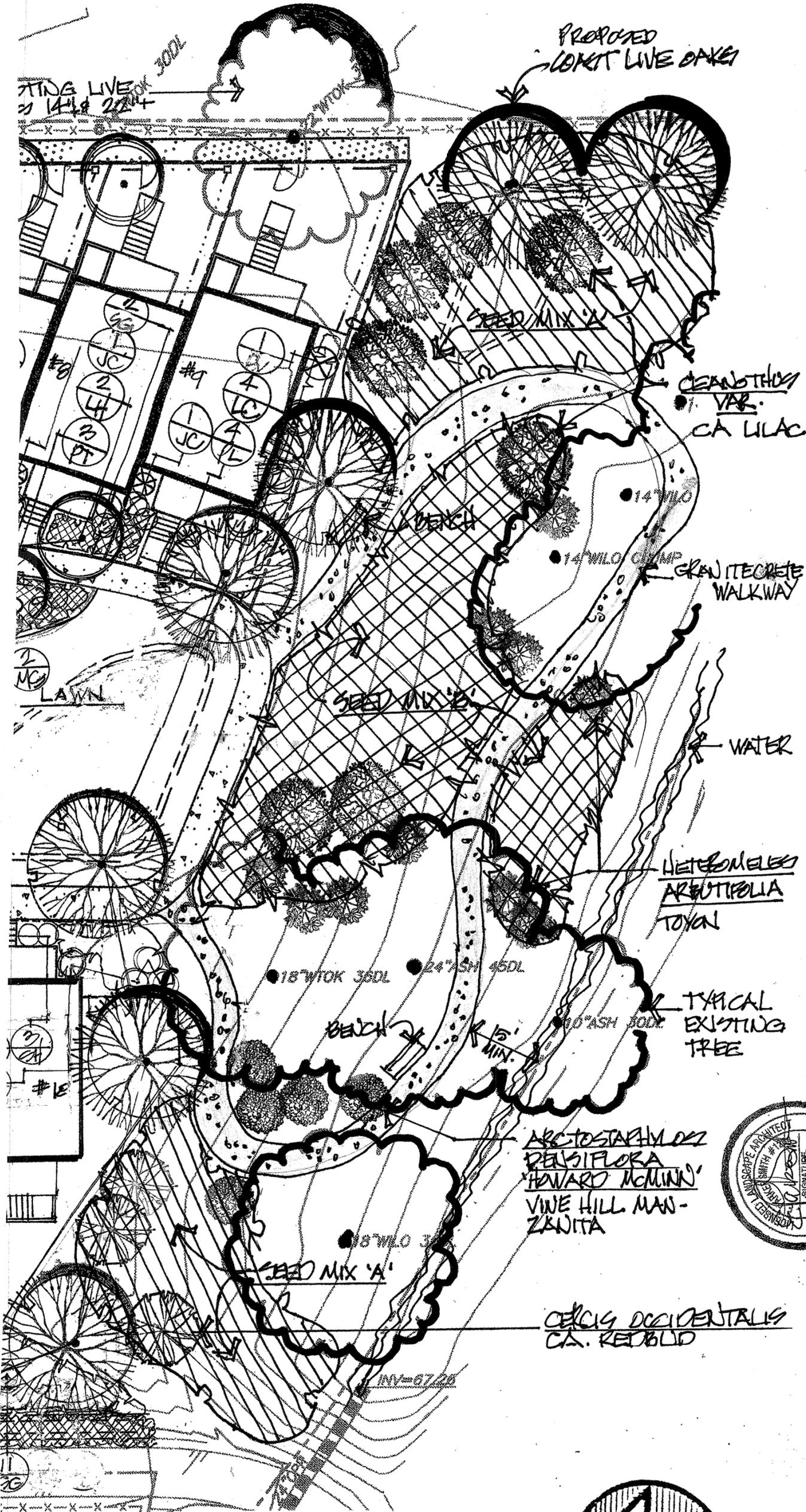
CHECKED BY: PS

FILE NO.: 17/19

SHEET No. **L-2** OF **3**

PARKER SMITH
LANDSCAPE ARCHITECTURE
CA LICENSED LANDSCAPE ARCHITECT #1284
1945 PINER ROAD #25, SANTA ROSA, CA 95403 (707) 477-7502

DAVIS TOWNHOUSES
SEBASTOPOL CALIFORNIA



DANIEL DAVIS
 1051 TODD ROAD
 SANTA ROSA, CA. 95407
 707 975 0794

PREPARED FOR:

DAVIS TOWNHOUSES

SEBASTOPOL CALIFORNIA
 APN 004-063-036

SHEET DESCRIPTION:

PRELIMINARY LANDSCAPE PLAN
 NATIVE PLANTINGS ON SLOPE AREA

PARKER SMITH
 LANDSCAPE ARCHITECTURE
 CALICENCED LANDSCAPE ARCHITECT #1264

1945 PINER ROAD #25, SANTA ROSA, CA 95403 (707) 477-7502

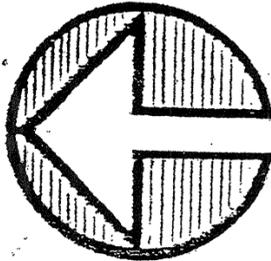


ARCTOSTAPHYLOS
 PENNIFLORA
 'HOWARD McMINN'
 VINE HILL MAN-
 ZAVITA

CERCIS OCCIDENTALIS
 CA. REDBUD

NOTE

BOTH SEED MIXES TO CONTAIN THREE
 NATIVE FESCUE LOW, CLUMBIING GRASSES.
 MIX 'A' TO HAVE WHITE, PINK & BLUE
 NATIVE WILDFLOWERS & MIX 'B' TO HAVE
 ORANGE & YELLOW NATIVE WILDFLOWERS.



SCALE:	1/8" = 1'
DRAWN BY:	RS
CHECKED BY:	RS
FILE No.:	17/19
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SL-1	1

4-24-19